



TRAFFIC AND MOBILITY COMMISSION

Agenda

Aug. 18, 2022, 4 p.m.

Council Chamber
1200 Carlsbad Village Drive
Carlsbad, CA 92008

Welcome to Your Traffic and Mobility Commission Special Meeting

We welcome your interest and involvement in the city’s legislative process. This agenda includes information about topics coming before the Traffic and Mobility Commission and the action recommended by city staff. You can read about each topic in the staff reports, which are available on the city website and in the Office of the City Clerk. The Minutes Clerk is also available to answer any questions you have about Traffic and Mobility Commission meeting procedures.

How to watch In Person



Traffic & Mobility Commission
Meetings take place at City Hall,
1200 Carlsbad Village Drive

Online



Watch the livestream and replay past meetings
on the city website,
carlsbadca.gov/residents/communication/city-tv-channel

How to participate

If you would like to provide comments to the Traffic and Mobility Commission, please:

- Fill out a speaker request form, located in the foyer.
 - Submit the form to the Minutes Clerk before the item begins.
 - When it’s your turn, the Chair will call your name and invite you to the podium.
 - Speakers have three minutes, unless the presiding officer (usually the Chair) changes that time.
 - You may not give your time to another person, but groups can select a single speaker as long as three other members of your group are present. Group representatives have 10 minutes unless that time is changed by the presiding officer or the Commission.
- **In writing:** Email comments to traffic@carlsbadca.gov. Comments received by 2 p.m. the day of the meeting will be shared with the Traffic and Mobility Commission prior to the meeting. When e-mailing comments, please identify in the subject line the agenda item to which your comments relate. All comments received will be included as part of the official record.

Written comments will not be read out loud.

Reasonable Accommodations

Persons with a disability may request an agenda packet in appropriate alternative formats as require by the Americans with Disabilities Act of 1990. Reasonable accommodations and auxiliary aids will be provided to effectively allow participation in the meeting. Please contact the City Manager’s Office at 760-434-2821 (voice), 711 (free relay service for TTY users), 760-720-9461 (fax) or manager@carlsbadca.gov by noon on the Thursday before the meeting to make arrangements.

More information about Traffic and Mobility Commission meeting procedures can be found at the end of this agenda and in the Carlsbad Municipal Code chapter 1.20.

CERTIFICATE OF POSTING

I do hereby certify that a copy of the foregoing agenda was posted at the Council Chamber at

4:05 pm on Friday

08/12/2022
Date

Eliane Paiva
Signature

CALL TO ORDER:

ROLL CALL:

PLEDGE OF ALLEGIANCE:

APPROVAL OF MINUTES:

Minutes of the Regular Meeting held July 5, 2022.

PUBLIC COMMENT: *In conformance with the Brown Act, no Commission action can occur on items presented during Public Comment. A total of 15 minutes is provided so members of the public can address the Commissioners on items that are not listed on the Agenda. Speakers are limited to three (3) minutes each. If you desire to speak during Public Comment, fill out a **SPEAKER CARD** and submit it to the Minutes Clerk. If there are more than five (5) speakers, the remaining speakers will be heard at the end of the agenda just prior to Commissioners Reports. When you are called to speak, please come forward to the podium and state your name.*

CONSENT CALENDAR: *The items listed under Consent Calendar are considered routine and will be enacted by one motion as listed below. There will be no separate discussion on these items prior to the time the Commission votes on the motion unless members of the Traffic and Mobility Commission, the Liaison or the public request specific items be discussed and/or removed from the Consent Calendar for separate action. A request from the public to discuss an item must be submitted to the Minutes Clerk in writing prior to Traffic and Mobility consideration of the Consent Calendar.*

1. **AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS PROJECT - FINAL DRAFT:** Support staff's recommendation to implement the improvement plans for Segment 2 of the Avenida Encinas Coastal Rail Trail and Pedestrian Improvements, Capital Improvement Program Project No. 6004 (Staff Contact: Brandon Miles and Hossein Ajideh, Public Works)
2. **INTERSECTION IMPROVEMENTS AT MELROSE DRIVE AND PALOMAR AIRPORT ROAD:** Support staff's recommendation to approve the plans and specifications of the intersection improvements at Melrose Drive and Palomar Airport Road, Capital Improvement Project No. 6034. (Staff Contact: Brandon Miles and Hossein Ajideh, Public Works)
3. **REVISE PRIMA FACIE SPEED LIMITS FOR GATEWAY ROAD AND CADENCIA STREET:** Support staff's recommendation to City Council to revise prima facie speed limits on:
 - a) Gateway Road between El Camino Real and El Fuerte Street, from 40 miles per hour to 35 miles per hour
 - b) Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street, from 40 miles per hour to 35 miles per hour(Staff Contact: Miriam Jim and John Kim, Public Works)

DEPARTMENTAL REPORTS:

4. **AMEND THE CARLSBAD MUNICIPAL CODE BY ADDING SECTION 10.40.302 TO RESTRICT OVERNIGHT PARKING ON SURFSIDE LANE AND ISLAND WAY** – Receive staff's recommendation that City Council introduce an ordinance amending the Carlsbad Municipal Code by adding Section 10.40.302 to restrict overnight parking from 11 p.m. to 5 a.m. on both sides of

Surfside Lane, north of Island Way and both sides of Island Way, east of Carlsbad Boulevard.
(Staff Contact: Scott Meritt and Allegra Frost, Police Department and City Attorney)

Staff's Recommendation: Support staff's recommendation

5. POLICE REPORT REGARDING TRAFFIC & MOBILITY-RELATED MATTERS DURING THE MONTH OF JULY 2022 – Receive the report on Traffic & Mobility related matters during the month of July 2022. (Staff Contact: Lieutenant Jason Jackowski, Police Department)

Staff's Recommendation: Receive the report.

6. UPDATE ON THE SOUTH CARLSBAD BOULEVARD CLIMATE ADAPTATION PROJECT: Receive an update on the traffic impact analysis and roadway design options studies for the grant-funded South Carlsbad Boulevard Climate Adaptation Project. (Staff Contact: Tom Frank and Katie Hentrich, Public Works)

Staff's Recommendation: Receive a presentation

TRAFFIC AND MOBILITY COMMISSION COMMENTS:

PUBLIC COMMENTS: Continuation of the Public Comments

This portion of the agenda is set aside for continuation of public comments, if necessary, due to exceeding the total time allotted in the first public comments section. When you are called to speak, please come forward to the podium and state your name. The remainder of the categories are for reporting purposes. In conformance with the Brown Act, no public testimony and no Commission action can occur on these items.

ADJOURNMENT:

ABOUT THE TRAFFIC AND MOBILITY COMMISSION AGENDA

Per Carlsbad Municipal Code 2.28.050 the Traffic and Mobility Commission is an advisory commission to the City Council directed to study all matters referred to it concerning traffic safety and pedestrian safety and to make written recommendations to the city council regarding traffic and pedestrian safety within the city.

The Ralph M. Brown Act (commonly referred to as the "Brown Act") governs open meetings for local government bodies in the state of California. The Brown Act guarantees the public's right to attend and participate in meetings of local legislative bodies and is contained in section 54950 et seq. of the Government Code. The Brown Act also includes requirements for the distribution of agendas for these public meetings.

TRAFFIC AND MOBILITY COMMISSION PROCEDURE

For each item on the agenda, City of Carlsbad staff will present a report to the Traffic and Mobility Commission. The Traffic and Mobility Commissioners may then ask clarifying questions about the staff report in advance of the public outreach. The audience will then be asked for comment. The Traffic and Mobility Commission is interested to hear all persons wishing to give testimony. Please file a "**Request to Speak**" form to speak on items listed on the agenda. Persons not desiring to speak but, wishing to be recorded as proponents or protesters of record, may do so by submitting their name and address to the minutes clerk. If anyone wishes to question a Traffic and Mobility Commission recommendation, they may contact the Traffic and Mobility Division at 1635 Faraday Avenue, between the hours of 7:30 a. m. and 5:30 a.m., Monday

through Thursday, and 8 a.m. to 5 p.m. on Friday.

Visual materials should be submitted to the Transportation Department at 1635 Faraday Avenue no later than noon the Friday before the meeting. Please label all materials with the agenda item number you are representing. All materials exhibited to the Traffic and Mobility Commission during the meeting (slides, maps, photos, etc.) are part of the public record and must be kept by the Traffic and Mobility Division for at least 60 days after final action on the matter. Your materials will be returned upon written request. **Video clips cannot be accommodated.**

The Public Comment portion of the Agenda allows community members to speak on items that are not on the agenda. If you desire to speak about an item not listed on the agenda, a **“Request to Speak”** form should be filed with the Minutes Clerk. In conformance with the Brown Act, no action can occur on items presented during Public Comment.

Any agenda related writings or documents provided to the majority of the Traffic and Mobility Commission after distribution of the Agenda packet will be available for public inspection at the Transportation Department located at 1635 Faraday Avenue, Carlsbad, CA 92008. In addition, a binder containing all agenda related writings and documents will be held by the Minutes Clerk at each Traffic and Mobility Commission meeting and available for public review.

Those wishing to speak to the Traffic and Mobility Commission are asked to come forward, speak into the microphone, and give their name and address for the taped record. A time limit of three minutes is allotted to each speaker. Members of the public are asked to observe order at this meeting and to conduct themselves in a courteous and respectful manner.



TRAFFIC AND MOBILITY COMMISSION

Minutes

Council Chambers
1200 Carlsbad Village Drive
Carlsbad, CA 92008

July 5, 2022, 4:00 p.m.

CALL TO ORDER: 4 p.m.

ROLL CALL: Perez, Linke, Penseyres, Coelho and Newlands
Absent: Proulx and Fowler

PLEDGE OF ALLEGIANCE: Chair Perez led the Pledge of Allegiance

APPROVAL OF MINUTES:

Minutes of the Regular Meeting held May 2, 2022.

Minutes of the Regular Meeting held June 6, 2022

Motion by Vice-Chair Linke, seconded by Commissioner Coelho to approve the minutes for the May 2, and the June 6, 2022, meeting as presented. Motion carried, 5/0/0/2 (Absent: Commissioner Proulx and Commissioner Fowler))

PUBLIC COMMENT:

Lynda Daniels requested that staff install a sign telling pedestrians to stay to the right on the multipurpose bike lane between Oceanside and Carlsbad before you get to the traffic circle. She also asks that the city put more effort into maintaining this bike lane.

Scott Hansen, a Representative of the San Diego Mountain Biking Association brought forward the information that the San Diego Mountain Biking Association strongly disagrees with one aspect of the e-bike ordinance. The statement about dismounting the regulated mobility device where the trail width is less than five feet, or a pedestrian or equine is within a distance of 50 feet from the regulated mobility device. This will create trail user conflicts. There are a lot of great programs for reducing trail user conflict and the San Diego Mountain Bike Association would like to partner with Carlsbad to find a solution to this problem.

CONSENT CALENDAR: None

DEPARTMENTAL REPORTS:

1. POLICE REPORT REGARDING TRAFFIC & MOBILITY RELATED MATTERS DURING THE MONTH OF JUNE 2022 – (Staff Contact: Lieutenant Jason Jackowski, Police Department)

Staff's Recommendation: Receive a presentation

Lieutenant Jackowski presented the report and reviewed a PowerPoint presentation (on file in the Office of the City Clerk)

Commissioner Coelho inquired about the bicycle officer who is working full time for e-bike enforcement and if this position will continue after summer season.

Lieutenant Jackowski replied that the E-bike officer will return to his normal position as a school resource officer when school starts up again in mid-August. We will evaluate the situation to see if this is a full time position that is needed.

Commissioner Penseyres asked about the sign located southbound on coast highway that states that bicycle enforcement is ahead. What type of enforcement was being done in this location? Also, why this location?

Lieutenant Jackowski said it was all types of bicycle enforcement and was to highlight what the e-bike enforcement officer had been doing. He is writing citations and providing education. This sign gets moved around a lot and is usually somewhere on Carlsbad Boulevard.

Vice-Chair Linke asked about the 40 citations that were issued, and what portion are opting for the educational program?

Lieutenant Jackowski said he does not have that information, but the education program was not running the entire time that the citations were given. It is currently up and running now.

Chair Perez asked if the E-bike officer was using an E-bike himself. Also, is there any funding that is out there right now that we could look into in order to continue this program and build up an E-bike police force?

Lieutenant Jackowski replied that the police officer was enforcing the law using an E-bike himself. They did utilize the OTS grant to put together details where they have three or four officers do e-bike enforcement on a weekend instead of just one officer.

Chair Perez inquired about the mobile golf carts if they are safe and regulated to carry infant car seat?

Lieutenant Jackowski replied that he would have to research this but if the vehicle is lawful to be operated on the roadway and they had an underage child in the vehicle then the child would have to be in a child seat per law.

- CARLSBAD BOULEVARD RESTRIPIING BETWEEN MANZANO DRIVE AND ISLAND WAY, BIKE LANE IMPROVEMENTS AT FIVE LOCATIONS ON CARLSBAD BOULEVARD AND THREE SUSTAINABLE MOBILITY PLAN BIKE ENHANCEMENT PROJECTS – Receive a presentation and provide direction to staff regarding Carlsbad Boulevard Restriping between Manzano Drive and Island Way, bike lane improvements at five locations on Carlsbad Boulevard (CIP Project No. 6096), and Three Sustainable Mobility Plan Bike Enhancement Projects - La Costa Avenue at the intersections of Piraeus; Saxony and El Camino Real, (CIP Project No. 6101); Jefferson Street between Las Flores Drive and I-5 Overpass, (CIP Project No. 6100); and Paseo Del Norte and Cannon Road intersection, (CIP Project No. 6095). (Staff Contact: John Kim and Miriam Jim, Public Works)

Staff's Recommendation: Receive a presentation and provide input to staff

PUBLIC COMMENT:

Jack Kubota thanked the Commission for their services and requested that staff consider enhancing the safety of the bicyclists along Carlsbad Boulevard and Avenida Encinas.

Commissioner Newlands asked about the picture of the bike lane on Carlsbad Boulevard which shows pedestrians with no sidewalk. Will this stay this way?

City Traffic Engineer Kim responded that this is an interim project and Transportation Director Tom Frank will present a more permanent solution that will address the multi-modal issues in more detail. This is a striping only project that we can implement quickly.

Commissioner Newlands asked about the lane reduction on Solamar Drive and Island Way, the table showed at peak time this would decrease the service for vehicles and then later it said there will be no real effect. Which is the case?

City Traffic Engineer Kim responded that they have implemented a lane reduction in the past in this location for bad weather events and other issues. Based on the data from those incidents they don't think the volume will be an issue with the lane reduction.

Senior Engineer Jim added that the little to no impact of the lane reduction is specific to the signalized intersections and the area between the two intersections. The number of lanes at the intersection itself will remain two lanes.

Commissioner Coelho inquired about the timeframe for completing the striping project.

City Traffic Engineer Kim responded that the restriping project will be done within the 12-month period of when it begins construction.

Commissioner Penseyres inquired about westbound Cannon Road at Paseo Del Norte and whether we may use a bike box in this location. This would help cyclists line up with the sharrows in the middle lane. They could get a head start with the bike box and this could be a great enhancement.

City Traffic Engineer Kim responded that this could be easily incorporated but they need to research when you would implement a bike box. We wouldn't want to put one in where usage would not be high. We want vehicles to respect the box where there is high level of activity of cyclists in that location.

Vice-Chair Linke brought up area of concern regarding the dashed green bike lanes. Where there was a dedicated right turn lane to the right, we would make the solid green bike lane to the left of that. If there was a conflict through an intersection, we would dash the line through the conflict zones. In the past we have declined the recommendation to put the dashed green line where there is a shared right turn and bike lane because drivers might be reluctant to enter there. There are many instances in all these projects where we are changing the unwritten rule and he wants to use whatever is safest.

Vice-Chair Linke also inquired about the elimination of parking on Jefferson Street and asked if many people use those spaces. Is there an alternative to this parking?

Senior Engineer Jim responded that there are more parking demands on the west side of Jefferson Street than the east side. Based on observations there will not be a large issue with parking. Public notice will be given before the project is implemented.

Vice-Chair Linke asked if there is a target speed limit that we are looking to get to on southbound Carlsbad Boulevard. The current speed limit is 50 mph, and he inquired if this speed limit go down with reducing the travel lanes.

City Traffic Engineer Kim responded that they don't have a target speed limit but typically with a significant siding and striping project we will conduct a speed survey. If the speed survey shows that the speed limit needs to be reduced, we will propose an adjustment.

Vice-Chair Linke brought up that the traffic study was done in 2021 during Covid-19, where the traffic count would be lower. The pre-covid number was a lot higher. If we eliminate a lane we could run into a lot of issues if traffic volume resumes to where it was pre-covid.

City Traffic Engineer Kim responded that they are aware that the level of service analysis pre-covid was higher. Our recommendation will stay the same for this project. The observation of this closure was pre-covid along this stretch and we didn't see any traffic operation issues during these closures.

Motion by commissioner Coelho, seconded by Commissioner Newlands to include the following comments:

- Staff shall provide notification to residents on Jefferson Street north of Las Flores informing them of the removal of on-street parking on the east side of Jefferson Street
- Staff shall ensure that the traffic study for the improvements on Carlsbad Boulevard include analysis of pre-COVID traffic conditions
- After implementation of the proposed improvements on Cannon Road at Paseo Del Norte, staff shall review the project for potential implementation of a bike box at the westbound approach of Cannon Road
- After implementation of the proposed improvements on Carlsbad Boulevard staff shall evaluate options to reconfigure the parking lot north of Island Way to accommodate the maximum number of vehicles

Motion carried: 5/0/0/2 (Absent: Commissioner Proulx and Commissioner Fowler)

3. PRELIMINARY CONCEPTUAL DESIGN OF THE SOUTH CARLSBAD BOULEVARD CLIMATE ADAPTATION PROJECT

– Receive a report from staff regarding the Preliminary Conceptual Design of the South Carlsbad Climate Adaptation Project and the results of the public outreach effort; and,

1. Receive public comments
2. Discuss and provide comments to staff on the three design options identified in the presentation
3. Consider supporting staff's recommendation to City Council to approve Option 2 as the preferred option for the preliminary conceptual design of the South Carlsbad Boulevard Climate Adaptation Project (Staff Contact: Tom Frank and Katie Hentrich, Public Works)

Staff's Recommendation: Receive the report and support staff's recommendation to City Council to approve Option 2 as the preferred option for the preliminary conceptual design of the South Carlsbad Boulevard Adaptation Project.

Transportation Director Frank and Senior Program Manager Hentrich presented the report and reviewed a PowerPoint presentation (on file in the Office of the City Clerk)

PUBLIC COMMENT:

Bill Canepa of the Hilton Garden Inn on Solamar Drive feels the process is rushed and asked for the City to take more time to publicly vet the project. He also expressed concern about the roundabout at Solamar Drive and concerned about construction issues. He asked with the decision matrix is there consideration for adjacent property owners. He suggested that staff gather more resident's input before going in front of City Council.

Jim Davidson, the president of the Solamar Homeowners Association shared Mr. Canepa's concerns and expressed concern about the roundabout at Solamar Drive. The intersection serves both the hotel driveway and the senior citizens who live on Solamar Drive. He looks forward to continuing conversations to do the right thing to do at the "Solamar Driveway".

Commissioner Newlands inquired about Option 3, and if this option would enhance the issue that we heard from the public speakers regarding the roundabout?

Transportation Director Frank responded that Option 3 is a viable option and that the proposed improvements are outside of the coastal hazard zone. The reason why this option did not exceed Option 2 was because of the other benefits of the roundabout at Solamar Drive. A roundabout provides additional pedestrian pathways on the southerly side of the intersection but also on the northern side. It also provides shorter crossing distances for the travel lanes. The roundabout also that drivers slow down while going through the intersection. With Option 3 there would also be a pedestrian signal at the intersection. Option 3 is a viable option but did not rank as high as Option 2.

Commissioner Coelho asked about when the public input on these options were collected.

Transportation Director Frank replied that there was an extensive public outreach process and that was shown in exhibit number one. That input was included in the key issues in the options analyses.

Commissioner Coelho inquired if it was noted how many people are taking the left turn into Solamar Drive.

Transportation Director Frank replied that staff do not have the traffic data study at this time.

Commissioner Coelho inquired about the approximate timeline between now and when the first shovel would hit the ground.

Transportation Director Frank responded that what is driving the schedule is the deadline for the completion of the study, which is February of 2023. We do not have a timeline for when this would go to a final design or environmental studies. This will depend on the City Council future action.

Commissioner Penseyres comments about the 24 hour counts for the vehicles. He thinks that we should note that the bicycle counts are 13 hour counts instead of 24 hours. He has observed that there are a huge number of e-bikes now that ride on Carlsbad Blvd. after dark. There are as many bicycles now as vehicles. He suggests that we should extend the bicycle count to 24 hours.

Commissioner Penseyres inquired if the sharrows could be used at the beginning, middle and end of the roundabout, could they be added all through the roundabout. He expressed problems when bicyclist

ride to the right and cars pass them in the roundabout and this should be addressed during the design of the roundabout. The level of service for motorists is different for traffic lights versus roundabouts. For example, a traffic light intersection will not fail until the average delay is 80 seconds. A roundabout will fail with an average delay of 55 seconds. This is important as they help people understand that we are not always comparing apples to apples when talking about level of service.

Commissioner Penseyres supports the proposed wide Class I and mentioned that we should address induced demand on Carlsbad Boulevard before we go in front of City Council.

Vice-Chair Linke asked if the design of this project will influence the remainder of the project on Carlsbad Boulevard. He commented that the city should have an uniform appearance on Carlsbad Boulevard.

Transportation Director Frank responded that this design is only for this segment of Carlsbad Boulevard. Any future segments will be a separate project with their own public outreach effort.

Vice-Chair Linke supports roundabouts and has some concerns about roundabouts. Bicyclists have to merge into the roundabout with vehicle traffic and is not sure about the comfort level of pedestrians and bicyclists. He wants to make sure we provide clear information to cyclists regarding the need to merge or use the sidewalks to bypass the roundabout instead. He likes the concept of roundabouts and improving traffic flow by using roundabouts. He also commented on the vehicle volumes and concern regarding capacity of the roadway. He believes a formal capacity study needs to be done and that he doesn't feel they can support an option without the formal study. Is it possible to have this study conducted, and then come back to the commission before going to City Council?

Transportation Director Frank responded that cyclists will have the option to enter the roundabout along with vehicle traffic or exit via the ramps and to use the sidewalk area which in the roundabouts will actually be a shared pathway. For this segment of the sidewalk between the two ramps it will be a shared pathway which will be much wider than a sidewalk. For option number 2, in this cross-section there will be a buffered bike lane on the outside which is used for high-speed bicyclists and e-bikes. The lower speed cyclists and pedestrians on skateboards, with strollers, etc. will use the class 1 bike lane. We need both of these spaces in this area to accommodate both types of transport. We don't want to mix the high-speed cyclists with families. All of the options will provide increased capacity, but the single lane roundabouts are the most efficient. There is a lot of data on single lane roundabouts and that data shows that they can easily handle 20,000 – 25,000 average daily trips. With the option number 2 implementing roundabouts, it might create more capacity than what we actually have today.

Vice-Chair Linke asked if we can see these options again after the most recent traffic study is completed. He also supports active transportation improvements along Carlsbad Blvd. considering the high demand along the coastline, and he is also concerned about congestion.

Transportation Director Frank replied that there will not be enough time for the traffic study to come back to the commission before we need to present our options to City Council to complete the study. However, the 30 percent conceptual design will include the traffic study and that will come back in front of the commission.

Chair Perez expressed his concerns regarding construction impacts to adjacent businesses and emergency services.

Transportation Director Frank replied that the city street designs always address the impact on emergency services and that our emergency services departments will be reviewing the options and ultimately the 30% design in detail.

Chair Perez inquired if it is possible to create an advisory committee specifically for this project? This is a big project that will reshape Carlsbad. The advisory committees have been enormously beneficial in the past.

Transportation Director Frank responded that there may be a timing issue on what we can do before we complete this study.

Commissioner Coelho asked about the timeline and reiterates that the goal is to create a schematic design. Can we still make changes off of this concept based on comments from City Council and other commissions? The entire Carlsbad Boulevard segment should be looked at holistically and it should flow.

Transportation Director Frank replied that as we get into further development of the design, we can address any concerns that come up and bring it back to the commission. There is some leeway moving forward with the conceptual design.

Commissioner Coelho asked why staff is leaning toward option number 2 instead of option number 3.

Transportation Director Frank responded that it was based on the rank showing on the staff report. Both option 2 and option 3 were very close in ranking.

Commissioner Coelho inquired if the proposed change from Option 2 to Option 3 mentioned in the staff report is due to erosion control.

Transportation Director Frank answered yes.

Vice-Chair Linke asked if this project will go to a public vote since the budget will exceed the \$1M Prop H limit?

Transportation Director Frank said that it is too early to address this issue, but if it needs to go for a vote than it will go to a vote. This issue would be determined at a later date.

Vice-Chair Linke said that he is not ready to vote in any of the options due to the lack of a traffic impact analysis study and the points here are very subjective and arbitrary. He expressed that the congestion factor should possibly have more weight in rating of the options.

Commissioner Coelho asked about why we are working on the roadway and intersections that are outside the coastal hazard zone.

Transportation Director Frank replied that we need to re-align the road and the intersection because the entire existing southbound vehicle travel lane is within the coastal hazard zone within the study limits.

Motion by Vice-Chair Linke, seconded by Commissioner Newlands to include the following comments:

- Recommended that sharrows be provided within the inscribed circle of the roundabouts
- The traffic impact analysis results should be provided to the Traffic & Mobility Commission for review prior to going to City Council
- The shared use paths around each roundabout shall be constructed of concrete and not decomposed granite
- Recommended that City Council form an advisory committee for further review of the South Carlsbad Boulevard project

Motion carried: 5/0/0/2 (Absent: Commissioner Proulx and Commissioner Fowler)

Commissioner Penseyres moved to approve staff's recommendation. No second received and motion did not pass.

Commissioner Coelho commented that he supports the project however he feels the commission need to review a traffic study associated with the options. He also commented that he rides on Carlsbad Blvd on the north part of the city and he much prefers the improvements and not having to wait at traffic signals.

Motion by Vice-Chair Linke, seconded by Commissioner Coelho requesting that a Special Traffic & Mobility Commission meeting shall be convened on Aug. 18, 2022, to review the results of the traffic impact analysis and alternatives analysis. Motion carried: 5/0/0/2 (Absent: Commissioner Proulx and Commissioner Fowler)

4. UPDATE ON TAMARACK AVENUE AND VALLEY STREET PEDESTRIAN SIGNAL PROJECT – Received presentation regarding the Tamarack Avenue and Valley Street Pedestrian Signal Project and provide input to staff. (Staff Contact: John Kim and Lindy Pham, Public Works)

Staff's Recommendation: Support staff's recommendations.

City Traffic Engineer Kim and Associate Engineer Pham presented the report and reviewed a PowerPoint presentation (on file in the office of the City Clerk)

PUBLIC COMMENT:

Tim Morgan requested that staff stop the project because of the many issues and address the speed problems. He has gained 100% consensus of all residents who live around the area of Valley Street and Tamarack Avenue. He requested more community involvement to look for alternatives.

Jason Oziel commented on the lack of public input on the design of Tamarack Avenue and Valley Street crosswalk improvement. Please stop this project and start addressing the speed issue.

Jean-Marc Dumouchez commented on the speed problem on Tamarack Avenue and suggested that staff consider a roundabout on Tamarack Avenue and James Street.

Jim Young commented on the design implementation of the crosswalk of Tamarack Avenue and Valley Street that in his opinion is over engineered and does not solve the speeding problems.

Michael Zepeda commented on the lack of public input on this project and requested that staff consider reevaluating the hybrid signal installation at the crosswalk of Tamarack Avenue and Valley Street.

Commissioner Newlands mentioned that he went out and looked at the site and agrees with the public commenters and their suggestions.

Commissioner Penseyres commented that he has been at this location many times as a cyclist and does not see any issues with the intersection itself.

Vice-Chair Linke commented that he also went to the site and met with some of the public commenters. The three issues he noticed are the Bulb-Outs, the diameter of the signal post and the third is making an exception to the residential traffic calming program to get what the residents want. How can we reduce the amount of encroachment that cars have to take to get around the Bulb-Outs?

City Traffic Engineer Kim said that staff will look at the curb extension. The rubbing itself on the curb does not cause concern as sometimes drivers don't pay attention and rub their tires on that. Staff can look specifically at the Valley Street to Tamarack Avenue turn to make sure that larger vehicles such as the school bus have room to make the turn without entering into the opposing lane.

Vice-Chair Linke asked if the residents have to submit an application for the residential traffic management program?

City Traffic Engineer Kim responded that staff will move forward with the traffic calming study.

Vice-Chair Linke mentioned that a raised crosswalk has been suggested as well as a roundabout. He is also concerned about the diameter of the signal pole and the lack of communication that he heard from the residents.

City Traffic Engineer Kim responded that the diameter of the signal is determined by the length of the mast arms that are going to be placed on the signal pole. The Communication department post information on the website informing the residents of the coming up project, and letters were sent to addresses in that area. Construction Management and Inspection Department notified us that the door knockers were given to all nearby houses as well. Staff will communicate better during the design phase.

Chair Perez asked if there were any communications to the community about the agenda item for this meeting? Also, were there any changes on the construction plans or blueprints regarding the design of the signal poles.

City Traffic Engineer Kim responded yes, the public were notified that this agenda item would be heard at tonight's meeting. There were not any changes on the blueprints regarding the signal poles.

Vice-Chair Linke made the recommendation to City Council that they direct staff to expedite a traffic calming program around that intersection to include looking at Bulb-Outs and Hawk signal.

Commissioner Coelho expanded on the recommendation to include not just that intersection, but up the hill as well.

City Traffic Engineer Kim said that the traffic calming process works best where there are single family homes fronting the street. He recommended the portion of Tamarack Avenue on either side of Valley Street where single-family homes are present. So, from Skyline Drive to Adams Street.

Motion by Vice-Chair Linke, seconded by Commissioner Coelho to include the following comments:

- Recommended to City Council that they direct staff to expedite a residential traffic calming program on Tamarack Avenue from Skyline Drive to Adams Street.
- And to address the resident's concerns on the newly installed Bulb-Outs at the intersection of Tamarack Avenue and Valley Street.

Motion carried: 5/0/0/2 (Absent: Commissioner Proulx and Commissioner Fowler)

5. TRAFFIC AND MOBILITY COMMISSION FISCAL YEAR 2022-23 WORKPLAN– Review and approve the final Fiscal Year 2022-23 Traffic and Mobility Commission Workplan and appoint a commissioner to represent the Traffic and Mobility Commission at the City Council meeting where the Work Plan will be presented for City Council consideration. (Staff Contact: Nathan Schmidt, Public Works)

Staff's Recommendation: Approve the Work Plan and appoint one commissioner to represent the Traffic & Mobility Commission at a City Council meeting.

Vice-Chair Linke asked if the communications plan was included and if there were any changes.

Transportation, Planning and Mobility Manager Schmidt replied that there were no changes to the communication plan.

Motion by Vice-Chair Linke, seconded by Commissioner Coelho to approve the FY 2022-23 Traffic & Mobility Commission Work Plan and nominated Chair Brandon Perez to represent the commission at the City Council meeting on Aug. 16, 2022. Motion carried: 5/0/0/2 (Absent: Commissioner Proulx and Commissioner Fowler)

CITY TRAFFIC ENGINEER COMMENTS:

Transportation, Planning and Mobility Manager Schmidt commented that the August Traffic and Mobility Commission meeting is undetermined at this time, but they will let the commission know once it is confirmed whether we need to cancel or postpone the meeting due to the remodel of the Council Chambers.

TRAFFIC AND MOBILITY COMMISSION COMMENTS:

Commissioner Coelho commented that the appointments of many of the commissioners will expire at the end of the year. He suggested that at the next meeting they should discuss a different rotation schedule so that the appointments of six commissioners don't end at the same time.

Commissioner Coelho asked about public comments on non-agenda items. How are those filtered out? City Traffic Engineer Kim responded that all public comments that are addressed to the commission are forwarded to the commission whether it is an agenda item or not.

Transportation Director Frank said that staff follow up with email communication if they are given an email address. We can include the comments in City Traffic Engineer Kim's report when they are relevant.

Commissioner Coelho inquired about Plum Tree and the public comments received in that area.

City Traffic Engineer Kim responded that staff continue to work with the requests. There are some studies that are outstanding and need to be performed when school is back in session. We will determine the speed issue once the speed data is confirmed.

Commissioner Coelho asked if staff seek out the businesses and residents who live in a very specific area like the Solamar residents and businesses.

Transportation Director Frank said that they did specifically call and mee with the Hotel personnel and residents in that area. We regularly meet with effected property owners and stakeholders of projects.

Commissioner Penseyres inquired about the e-bike rider crash on the class one path. Is there any way that we can track e-bike rider crash on Class I paths?

City Traffic Engineer Kim responded that they will have to check with Carlsbad Police and report back.

Commissioner Penseyres gave an update on the Local Road Safety Plan, or LRSP, meeting from last week. He learned that the City of Carlsbad will not get a lot of the state funds available from this program because only painting for bike lanes and extra lighting is allowed for competing for these funds.

City Traffic Engineer Kim responded that staff will agendize an item on LRSP for a later T&MC meeting and that competing for funds is only one aspect of the LRSP program.

Vice-Chair Linke mentioned that we can use the data from the LRSP program to help guide us in identifying problem areas that need to be addressed.

Vice-Chair Linke commented that when someone contacts him about an issue, he first directs them to the Transportation Department and that if they are not satisfied with the answer given, they can contact him again and he will submit a question.

Vice-Chair Linke talked about the Growth Management Update Citizens Committee that last month was dedicated to water and sewer. Next month meeting will focus on mobility and discuss how to deal with performance standard.

Chair Perez praised city workers that he witnessed them helping the residents in need on the road.

ADJOURNMENT:

Chair Perez adjourned the Traffic & Mobility Commission Meeting on July 5, 2022, at 8:55 p.m.

Eliane Paiva, Minutes Clerk



Staff Report

Meeting Date: Aug. 18, 2022
To: Traffic and Mobility Commission
Staff Contact: Brandon Miles, Associate Engineer
brandon.miles@carsbadca.gov, 442-339-2745
Subject: Avenida Encinas Coastal Rail Trail and Pedestrian Improvements

Recommended Action

Support staff's recommendation to implement the improvement plans for Segment 2 of the Avenida Encinas Coastal Rail Trail and Pedestrian Improvements, Capital Improvement Program Project No. 6004.

Summary

In July 2017, Caltrans awarded the City of Carlsbad an Active Transportation Program, or ATP, grant by Caltrans to construct a portion of the Coastal Rail Trail, or CRT, along Avenida Encinas between Carlsbad Boulevard and Cannon Road. Construction of this portion of the CRT has been identified as a high priority by the local and regional community. The CRT is a planned regional bikeway that includes a 44-mile regional bikeway that connects six coastal cities throughout San Diego County.

On Dec. 7, 2021, with a recommendation from staff, the City Council authorized executing an agreement with NASLAND Engineering for engineering design and environmental services for the project.

On April 4, 2022, the Traffic and Mobility Commission received a presentation and provided input on the preliminary design of the project. At the same meeting, the commission voted unanimously to support staff's recommended bike lane classification, enhanced Class II bike lane, along Avenida Encinas

Following the April 4, 2022, Traffic and Mobility Commission meeting, staff discovered additional comments submitted by the public relating to the project. To provide those additional comments to the Traffic and Mobility Commission, staff placed an item on the June 6, 2022, Regular Meeting Agenda for the Commission's consideration.

At the June 6, 2022, meeting, the Commission discussed the additional comments that had been received as well as comments received in person at the meeting. Following consideration of all the comments received staff were directed to return to the Traffic and Mobility Commission with the final improvement plans for Segment 2 of the Avenida Encinas Coastal Rail Trail and Pedestrian Improvements.

Discussion

Staff have received input from Traffic and Mobility Commission and the public on the preliminary design plans. Staff directed NASLAND Engineering to update the improvement plans and environmental documents for Segment 2 of the project. Segment 2 is located between Poinsettia Transit Station and Carlsbad Boulevard as shown in Exhibit 1.

Improvements along the project corridor include the following features as identified in the improvement plans provided in Exhibit 2:

- Striping modifications
- Enhanced Class II bike lanes
- Enhanced intersection treatments including curb extensions, traffic calming measures, and high visibility crosswalks at the following intersections on Avenida Encinas:
 - Poinsettia Lane
 - Portage Way
 - Dory Lane
 - Carlsbad Boulevard
- Traffic calming with a road lane realignment between Poinsettia Lane and Windrose Circle that has been identified to reduce the level of service (LOS).
- Enhanced midblock pedestrian crossing at San Carlos Street and Avenida Encinas

As shown in the proposed improvements plans, provided in Exhibit 2, most of the roadway improvements will be implemented through roadway restriping. The roadway segment from Poinsettia Lane to Windrose Circle will be restriped from 5-vehicle lanes to 2-vehicle lanes. While traffic studies show the road lane realignment for this segment will result in a drop in Level of Service, from LOS C to LOS D, this segment will still operate at acceptable levels of service under the City's General Plan Mobility Element. Consistent with Mobility Element 3-P.15, staff will therefore bring this item to the City Council with a recommendation to approve the road lane re-alignment.

Next Steps

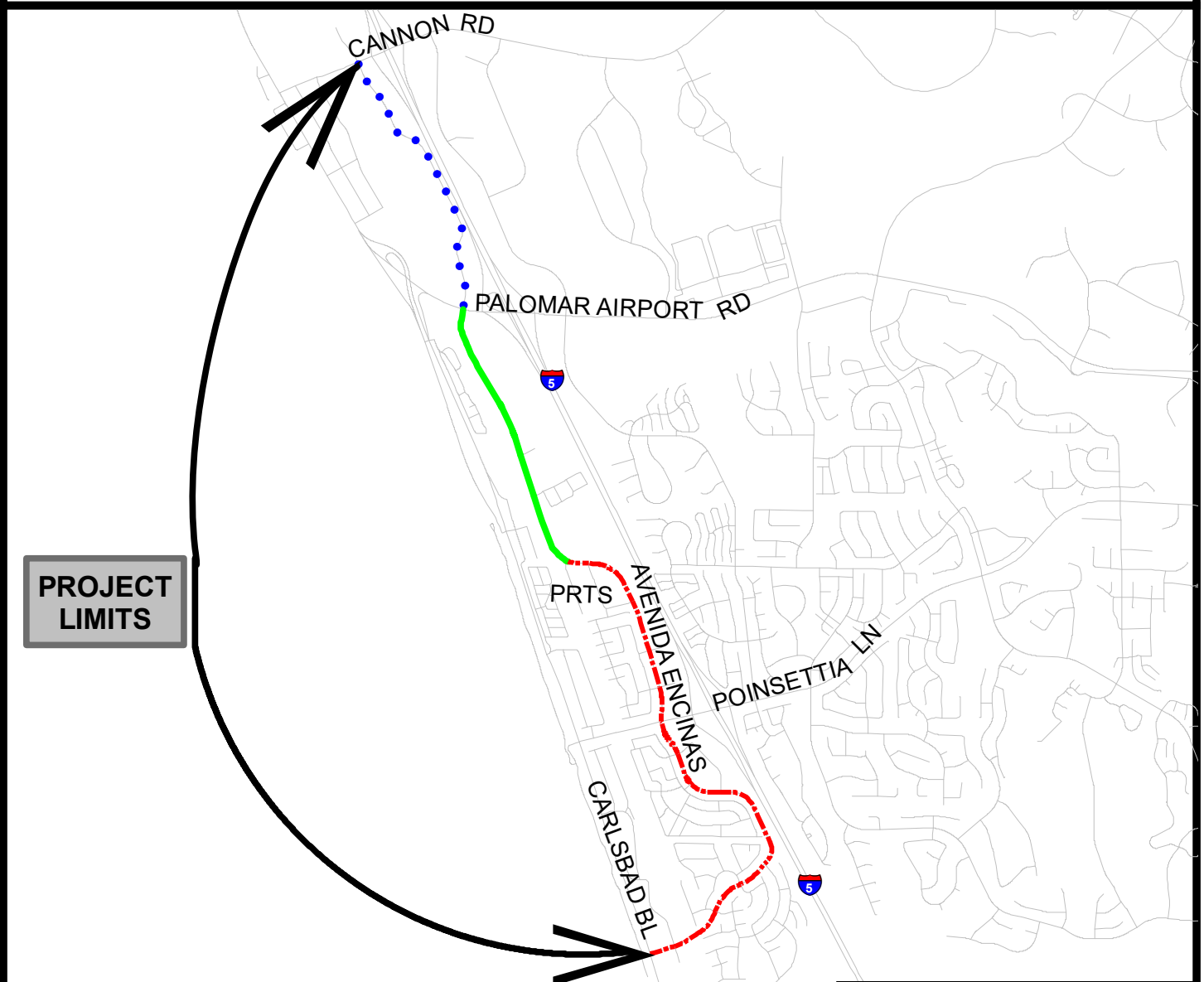
Following the Traffic and Mobility Commission's review, staff will finalize the construction plans for Segment 2 of the project and submit a construction allocation request to Caltrans for the ATP grant funding by Aug. 31, 2022. Thereafter, staff will bring a staff report to the City Council in late 2022 with a recommendation to approve the plans, specifications, and contract documents and authorize the City Clerk to advertise for construction bids for the project.

After the bidding process is complete, the project will be brought back to the City Council with a request to award a construction contract to the lowest responsive and responsible bidder. The construction is targeted to begin in mid-2023.

Exhibits

1. Project Location Map
2. Improvement Plans

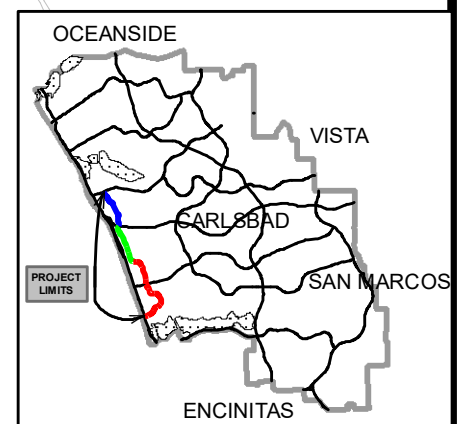
LOCATION MAP



PROJECT LIMITS

Legend

- Segment 1 - Completed
 - - - - Segment 2 - To be Completed by ATP and City Funds
 - Segment 3 - To be Completed by City-Only Funds
- PRTS - Poinsettia Regional Transit Station



PROJECT NAME
Avenida Encinas Coastal Rail Trail and Pedestrian Improvements

PROJECT NUMBER
6004

EXHIBIT
1

INDEX OF SHEETS

NO.	DESCRIPTION
1	TITLE SHEET
2-3	NOTES
4-14	IMPROVEMENT PLANS
14-19	DETAIL SHEETS
20-25	SIGNING AND STRIPING PLANS
26-29	TRAFFIC SIGNAL PLANS

SURVEY CONTROL STATEMENT:

HORIZONTAL CONTROL

HORIZONTAL CONTROL FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM, CCS 83, ZONE 6, EPOCH 1991.35 AND IS DETERMINED BY GPS MEASUREMENTS AT GPS STATION 133, GPS STATION 136 AND GPS STATION 138 PER THE CITY OF CARLSBAD SURVEY CONTROL NETWORK RECORD OF SURVEY 17271.

VERTICAL CONTROL

ELEVATIONS ARE RELATIVE TO THE NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29) PER THE CITY OF CARLSBAD SURVEY CONTROL NETWORK RECORD OF SURVEY 17271.

PROJECT CONTROL

STATION: "136"	STATION: "138"
N = 1985447.745	N = 1989695.304
E = 6234298.535	E = 6233260.661
EL = 54.061	EL = 74.150

CONVERGENCE FACTOR = -0° 35' 14.993"
 COMBINATION FACTOR = 0.999964460
 GROUND DISTANCE = GRID DISTANCE/COMBINATION FACTOR

RESULTANT COORDINATES AND ELEVATIONS ARE CLASSIFIED AS PROJECT CONTROL AND ARE TO BE USED FOR THIS PROJECT ONLY.

MONUMENT NOTE

THE CONTRACTOR SHALL OBTAIN THE SERVICES OF LICENSED LAND SURVEYOR FOR MONUMENT PRESERVATION INCLUDE PRE-CONSTRUCTION SURVEYS OF ALL RECORD MONUMENTS, RE-SETTING MONUMENT DISTURBED BY CONSTRUCTION AND FILING A CORNER RECORD WITH THE COUNTY OF SAN DIEGO.

IMPERVIOUS AREA

TOTAL DISTURBED AREA =
 (THIS AREA INCLUDES BUT IS NOT LIMITED TO OFF-SITE WORK INCLUDING PUBLIC IMPROVEMENTS AND TEMPORARY DISTURBANCE SUCH AS VEHICLE AND EQUIPMENT STAGING AREAS, CONSTRUCTION WORKER FOOT TRAFFIC, SOIL/GRAVEL PILES, UTILITY TRENCHES, BACKFILL CUTS, AND SLOPE KEYWAYS)

TOTAL EXISTING IMPERVIOUS AREA =
 TOTAL REPLACED IMPERVIOUS AREA =
 TOTAL PROPOSED IMPERVIOUS AREA =

STORM WATER BMP CATEGORY

STANDARD PROJECT

WORK TO BE DONE

THE IMPROVEMENT WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE FOLLOWING DOCUMENTS, CURRENT AT THE TIME OF CONSTRUCTION, AS DIRECTED BY THE CITY ENGINEER.

- CARLSBAD MUNICIPAL CODE
- CITY OF CARLSBAD ENGINEERING STANDARDS
- LATEST EDITION OF CALTRANS STANDARD PLANS
- LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS
- LATEST EDITION OF CALTRANS SPECIAL PROVISIONS
- LATEST EDITION OF CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CAMUTCD)
- THIS SET OF PLANS
- RESOLUTION NO. _____ DATED _____
- THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 2018 (GREEN BOOK).
- SOILS REPORT AND RECOMMENDATIONS BY: N/A
- THE SAN DIEGO AREA REGIONAL STANDARD DRAWINGS AND AS MAY BE MODIFIED BY THE CITY OF CARLSBAD STANDARDS
- WATER POLLUTION CONTROL PLAN PREPARED BY CONTRACTOR
- CALIFORNIA STORM WATER QUALITY ASSOCIATION BMP CONSTRUCTION HANDBOOK, CITY OF CARLSBAD BMP DESIGN MANUAL, AND CALTRANS CONSTRUCTION SITE BMP MANUAL

CITY OF CARLSBAD

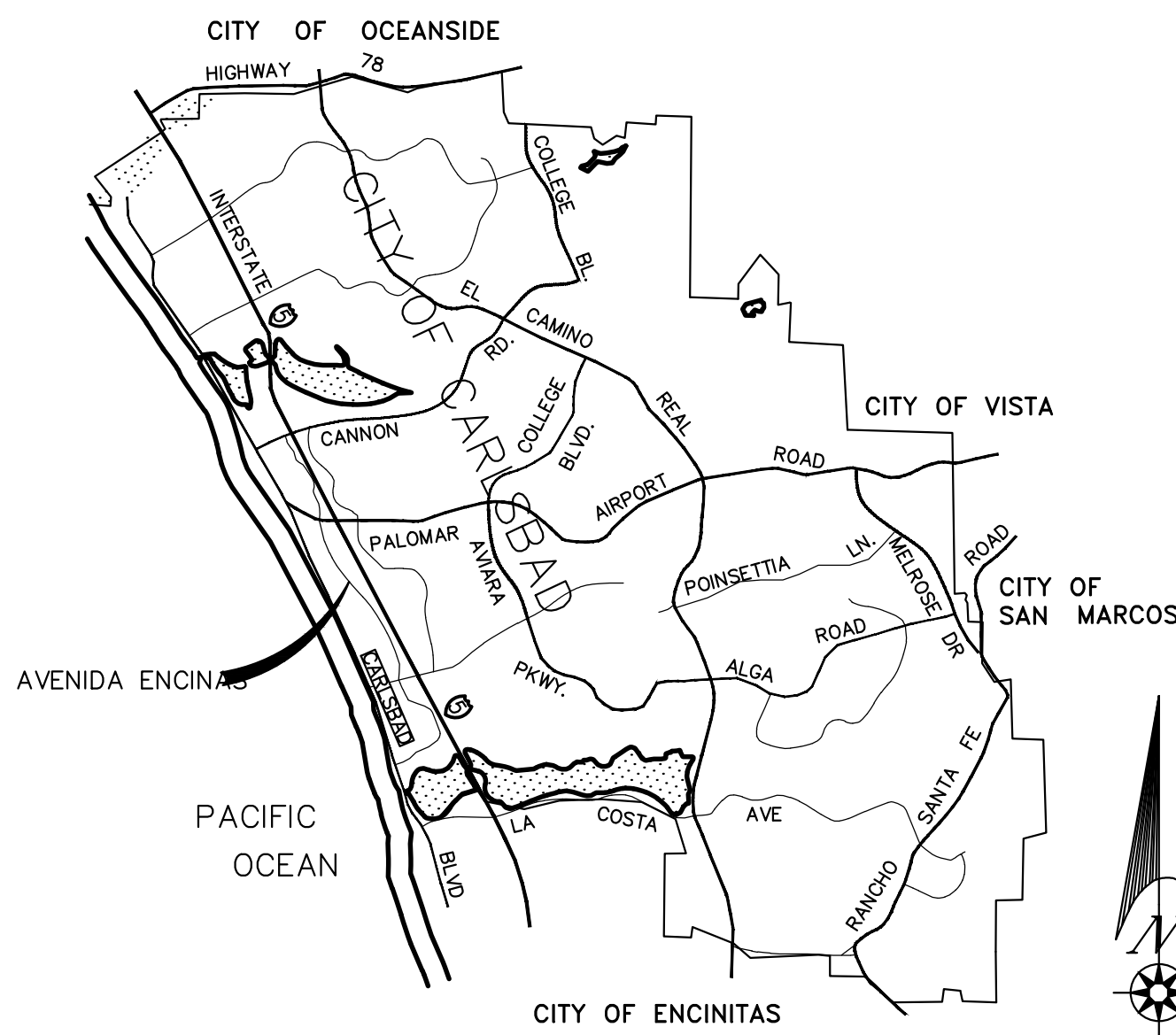
AVENIDA ENCINAS COASTAL RAIL TRAIL PROJECT AND PEDESTRIAN IMPROVEMENTS SEGMENT 2

PROJECT NO. 6004



**CITY OF CARLSBAD
CARLSBAD, CA**

- CITY COUNCIL**
 MATT HALL - MAYOR
 TERESA ACOSTA - COUNCIL MEMBER
 PRIYA BHAT-PATEL - COUNCIL MEMBER
 KEITH BLACKBURN - COUNCIL MEMBER
 PEDER NORBY - COUNCIL MEMBER
 SCOTT CHADWICK - CITY MANAGER



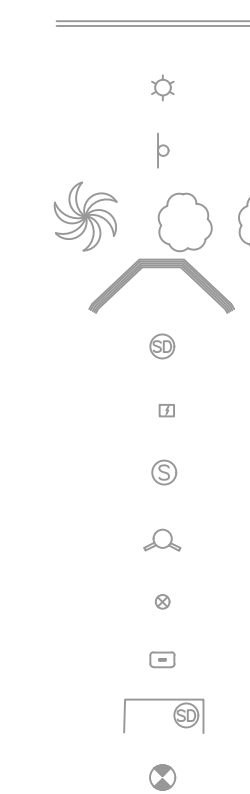
VICINITY MAP
N.T.S.

LEGEND

EXISTING CONDITIONS

- ITEM
- CURB AND GUTTER
- EXISTING STREET LIGHT
- EXISTING STREET SIGN
- EXISTING TREE/BUSH
- EXISTING CURB RAMP
- EXISTING STORM DRAIN MANHOLE
- EXISTING ELECTRICAL PULL BOX
- EXISTING SEWER MANHOLE
- EXISTING FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING WATER METER
- EXISTING STORM DRAIN INLET
- EXISTING SURVEY CONTROL POINT

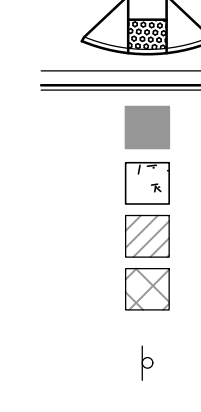
SYMBOL



IMPROVEMENT LEGEND:

DESCRIPTION	STD. DWG NO./DETAIL NO.
CURB RAMP	SDRSD G-28, G-28, G-31
6" CURB & GUTTER	SDRSD G-02
ASPHALT PAVING	
PCC SIDEWALK	SDRSD G-07
SLURRY SEAL	CAL TRANS 37-3
FULL DEPTH PAVEMENT REPLACEMENT	
RELOCATED SIGN AND POST	M-45
SAWCUT LINE	

SYMBOL



TRAFFIC IMPROVEMENT LEGEND:

EXISTING CURB	EXISTING TRAFFIC SIGNAL POLE
EXISTING GUTTER	PROPOSED TRAFFIC SIGNAL POLE
EXISTING SIDEWALK	PROPOSED TRAFFIC SIGNAL MAST ARM
EXISTING RIGHT-OF-WAY	EXISTING VEHICLE HEAD
EXISTING CENTERLINE	PROPOSED VEHICLE HEAD
EXISTING STRIPING	EXISTING PEDESTRIAN HEAD
PROPOSED STRIPING	PROPOSED PEDESTRIAN HEAD
EXISTING RED CURB	EXISTING EVP DETECTOR
PROPOSED RED CURB	PROPOSED EVP DETECTOR
PROPOSED GREEN BIKE LANE	EXISTING MAST ARM SIGN
MATCH EXISTING STRIPING	PROPOSED MAST ARM SIGN
EXISTING SIGN ON POST	EXISTING STREET NAME SIGN
EXISTING SIGN ON POLE	EXISTING TRAFFIC SIGNAL CONTROLLER CABINET
PROPOSED SIGN ON POST	PROPOSED TRAFFIC SIGNAL CONTROLLER CABINET
EXISTING PEDESTRIAN BARRICADE	EXISTING SERVICE PEDESTAL
PROPOSED PEDESTRIAN BARRICADE	PROPOSED SERVICE PEDESTAL
EXISTING SOLAR FLASHING BEACON WITH SIGN	EXISTING WIRELESS RADIO
EXISTING/NEW TRAFFIC SIGNAL	PROPOSED WIRELESS RADIO
EXISTING FIRE HYDRANT	EXISTING TRAFFIC MEASURING DEVICE
EXISTING CABLE TELEVISION	EXISTING ALPR
EXISTING ELECTRICAL	EXISTING DETECTION LOOP
EXISTING FIBER	PROPOSED DETECTION LOOP
EXISTING GAS	PROPOSED BIKE DETECTION LOOP
EXISTING RECYCLED WATER	EXISTING PULL BOX
EXISTING SEWER	PROPOSED PULL BOX
EXISTING TELECOMMUNICATIONS	PROPOSED FIBER SPLICE VAULT
EXISTING WATER	EXISTING CONDUIT
	PROPOSED CONDUIT
	EXISTING STREET LIGHT
	EXISTING ELECTRICAL TRANSFORMER

DECLARATION OF RESPONSIBLE CHARGE

I HEREBY DECLARE THAT I AM THE ENGINEER OF WORK FOR THIS PROJECT, THAT I HAVE EXERCISED RESPONSIBLE CHARGE OVER THE DESIGN OF THE PROJECT AS DEFINED IN SECTION 6703 OF THE BUSINESS AND PROFESSIONS CODE, AND THAT THE DESIGN IS CONSISTENT WITH CURRENT STANDARDS.

I UNDERSTAND THAT THE CHECK OF PROJECT DRAWINGS AND SPECIFICATIONS BY THE CITY OF CARLSBAD DOES NOT RELIEVE ME AS ENGINEER OF WORK, OF MY RESPONSIBILITIES FOR PROJECT DESIGN.

FIRM: NASLAND ENGINEERING
 ADDRESS: 4740 RUFFNER STREET
 CITY, ST.: SAN DIEGO, CA
 TELEPHONE: 858-292-7770
 BY: LAWRENCE THORNBURGH DATE: _____

R.C.E. NO.: 49795
 REGISTRATION EXPIRATION DATE: 6/30/2024



Civil Engineering
Surveying
Land Planning

T (858) 292-7770
 4740 Ruffner Street
 San Diego, CA 92111
 nasland.com



"AS BUILT"

P.E. _____ EXP. _____ DATE _____

REVIEWED BY: _____

INSPECTOR _____ DATE _____

STRUCTURAL (POST-CONSTRUCTION) BMP TABLE					
CITY TRAFFIC ENGINEER APPROVAL	TYPE	DESCRIPTION	OWNERSHIP	EXEMPT FROM HYDROMODIFICATION?	
				STANDARD STORMWATER REQUIREMENTS	YES (SEE SWMP FOR DOCUMENTATION)
BY: _____ DATE: _____				SHEET NO.	MAINTENANCE FREQUENCY

DATE	INITIAL	REVISION DESCRIPTION	DATE	INITIAL	DATE	INITIAL

SHEET 1 CITY OF CARLSBAD TRANSPORTATION DEPARTMENT SHEETS 29

TITLE SHEET FOR:
AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS

APPROVED: _____ HOSSEIN AJUDEH
 ENGINEERING MANAGER RCE 75991 EXP. 6/20/24 DATE _____

DWN BY: EE/DG CIP NO. _____ DRAWING NO. _____
 CHKD BY: DG 6004 DWG 536-6
 RVWD BY: LT

GENERAL NOTES

- THIS PLAN SUPERSEDES ALL OTHER PLANS PREVIOUSLY APPROVED BY THE CITY OF CARLSBAD REGARDING IMPROVEMENTS SHOWN ON THIS SET OF PLANS.
- APPROVAL OF THIS PLAN DOES NOT LESSEN OR WAIVE ANY PORTION OF THE CARLSBAD MUNICIPAL CODE, RESOLUTION OF CONDITIONAL APPROVAL, CITY STANDARDS OR OTHER ADDITIONAL DOCUMENTS LISTED HEREON AS THEY MAY PERTAIN TO THIS PROJECT. THE ENGINEER IN RESPONSIBLE CHARGE SHALL REVISE THESE PLANS WHEN NON--CONFORMANCE IS DISCOVERED.
- NO WORK SHALL BE COMMENCED UNTIL ALL PERMITS HAVE BEEN OBTAINED FROM THE CITY AND OTHER APPROPRIATE AGENCIES AT THE CONTRACTOR'S EXPENSE.
- ORIGINAL DRAWINGS SHALL BE REVISED TO REFLECT AS--BUILT CONDITIONS BY THE ENGINEER--OF--WORK PRIOR TO FINAL ACCEPTANCE OF THE WORK BY THE CITY.
- ACCESS FOR FIRE AND OTHER EMERGENCY VEHICLES SHALL BE MAINTAINED TO THE PROJECT SITE AT ALL TIMES DURING CONSTRUCTION.
- ALL INSPECTION REQUESTS OTHER THAN FOR PRECONSTRUCTION MEETING WILL BE MADE BY CALLING THE ENGINEERING 24--HOUR INSPECTION REQUEST LINE AT (760) 438--3891. INSPECTION REQUESTS MUST BE RECEIVED PRIOR TO 2:00 P.M. ON THE DAY BEFORE THE INSPECTION IS NEEDED. INSPECTION WILL BE MADE THE NEXT WORK DAY UNLESS YOU REQUEST OTHERWISE. REQUEST MADE AFTER 2:00 P.M. WILL BE SCHEDULE FOR TWO FULL WORK DAYS LATER.
- THE CONTRACTOR SHALL DESIGN, CONSTRUCT AND MAINTAIN ALL SAFETY DEVICES, INCLUDING SHORING, AND SHALL BE SOLELY RESPONSIBLE FOR CONFORMING TO ALL LOCAL, STATE AND FEDERAL SAFETY AND HEALTH STANDARDS, LAWS AND REGULATIONS.
- THE CONTRACTOR SHALL CONFORM TO LABOR CODE SECTION 6705 BY SUBMITTING A DETAIL PLAN SHOWING THE DESIGN OF SHORING, BRACING SLOPE OR OTHER PROVISIONS TO BE MADE OF WORKER PROTECTION FROM THE HAZARD OF CAVING GROUND DURING THE EXCAVATION OF SUCH TRENCH OR TRENCHES OR DURING THE PIPE INSTALLATION THEREIN. THIS PLAN MUST BE PREPARED FOR ALL TRENCHES FIVE FEET (5') OR MORE IN DEPTH PRIOR TO EXCAVATION. IF THE PLAN VARIES FROM THE SHORING SYSTEM STANDARDS ESTABLISHED BY THE CONSTRUCTION SAFETY ORDERS, TITLE 8 CALIFORNIA ADMINISTRATIVE CODE, THE PLAN SHALL BE PREPARED BY A REGISTERED ENGINEER AT THE CONTRACTORS EXPENSE. A COPY OF THE OSHA EXCAVATION PERMIT MUST BE SUBMITTED TO THE INSPECTOR PRIOR TO EXCAVATION.
- IF ANY ARCHAEOLOGICAL RESOURCES ARE DISCOVERED WITHIN ANY WORK AREA DURING CONSTRUCTION, OPERATIONS WILL CEASE IMMEDIATELY, AND THE CONTRACTOR WILL NOTIFY THE CITY. OPERATIONS WILL NOT RESTART UNTIL THE CONTRACTOR HAS RECEIVED WRITTEN AUTHORITY FROM THE CITY TO DO SO.
- ALL OFF--SITE HAUL ROUTES SHALL BE SUBMITTED BY THE CONTRACTOR TO THE CITY ENGINEER FOR APPROVAL TWO FULL WORKING DAYS PRIOR TO BEGINNING OF WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DEBRIS OR DAMAGE OCCURRING ALONG THE HAUL ROUTE OR ADJACENT STREETS AS A RESULT OF THE GRADING OPERATION.
- THE EXISTENCE AND LOCATION OF UTILITY STRUCTURES AND FACILITIES SHOWN ON THE CONSTRUCTION PLANS WERE OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS. ATTENTION IS CALLED TO THE POSSIBLE EXISTENCE OF OTHER UTILITY FACILITIES OR STRUCTURES NOT SHOWN OR IN A LOCATION DIFFERENT FROM THAT SHOWN ON THE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITIES SHOWN ON THE PLANS AND ANY OTHER EXISTING FACILITIES OR STRUCTURES NOT SHOWN.
- THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING FACILITIES (ABOVEGROUND AND UNDERGROUND) WITHIN THE PROJECT SITE SUFFICIENTLY AHEAD OF THE CONSTRUCTION TO PERMIT THE REVISIONS OF THE CONSTRUCTION PLANS IF IT IS FOUND THAT THE ACTUAL LOCATIONS ARE IN CONFLICT WITH THE PROPOSED WORK.
- THE CONTRACTOR SHALL NOTIFY AFFECTED UTILITY COMPANIES (SEE BELOW) AT LEAST TWO FULL WORKING DAYS PRIOR TO STARTING CONSTRUCTION NEAR THEIR FACILITIES AND SHALL COORDINATE WORK WITH A COMPANY REPRESENTATIVE.

UNDERGROUND SERVICE ALERT (DIG ALERT)	811
SD&E	(800)411--7343
AT&T	(800)892--0123
SPECTRUM CABLE	(760)438--7741
COX COMMUNICATIONS	(619)262--1122
CITY OF CARLSBAD(STREETS AND STORM DRAIN)	(442)339--2980
CITY OF CARLSBAD(SEWER, WATER & RECLAIMED WATER)	(442)339--2722
SAN DIEGUITO WATER DISTRICT	(760)633--2650
LEUCADIA WASTEWATER DISTRICT	(760)753--0155
VALLECITOS WATER DISTRICT	(760)744--0460
OLIVENHAIN WATER DISTRICT	(760)753--6466
BUENA SANITATION DISTRICT	(760)411--7343 X1330

- *AS APPROPRIATE
- IN ACCORDANCE WITH THE CITY STORM WATER STANDARDS ALL STORM DRAIN INLETS CONSTRUCTED BY THIS PLAN SHALL INCLUDE "STENCILS" BE ADDED TO PROHIBIT WASTEWATER DISCHARGE DOWNSTREAM. STENCILS SHALL BE ADDED TO THE SATISFACTION OF THE CITY.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF CARLSBAD CONSTRUCTION MANAGEMENT & INSPECTION DEPARTMENT 48 HOURS PRIOR TO STARTING WORK SO THAT INSPECTION MAY BE PROVIDED.

DISABLED ACCESS NOTES:

ALL GRADES SHOWN ON THESE PLANS WERE DESIGNED IN COMPLIANCE WITH THE 2019 CALIFORNIA BUILDING CODE (CBC) AND THE 2018 CALTRANS ADA COMPLIANCE HANDBOOK FOR ACCESSIBLE DESIGN (ADA) TO ALLOW FOR CONSTRUCTION TOLERANCES. IT IS THE CONTRACTORS' RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH THESE CODES. SHOULD A DESIGN QUESTION ARISE OR A FIELD CONDITION PRESENT ITSELF THAT IS DIFFERENT FROM THESE PLANS, WORK SHALL CEASE AND THE CIVIL ENGINEER BE NOTIFIED SO THAT AN ACCEPTABLE SOLUTION CAN BE DETERMINED. THE CONTRACTOR IS ADVISED TO CAREFULLY CHECK ALL PHASES OF WORK RELATING TO CBC AND ADA ACCESS FOR THIS PROJECT. CONSTRUCTION THAT EXCEEDS MAXIMUM OR MINIMUM DIMENSIONS AND SLOPES AS DEFINED BY CBC AND ADA ARE SUBJECT TO REJECTION AND MAY BE REQUIRED TO BE REMOVED AND REPLACED AT THE CONTRACTOR'S SOLE COST. SINCE THE CIVIL ENGINEER OR SURVEYOR CANNOT CONTROL THE EXACT METHODS OR MEANS USED BY THE CONTRACTOR OR THEIR SUB--CONTRACTORS DURING CONSTRUCTION, THE CIVIL ENGINEER ASSUMES NO RESPONSIBILITY FOR THE FINAL ACCEPTANCE OF ADA--RELATED ITEMS BY THE AGENCY HAVING JURISDICTION, ANY OTHER AUTHORITY, OR OTHER AFFECTED PARTIES.

STREET NOTES

- STREET LIGHTS SHALL BE INSTALLED AS SHOWN ON THESE PLANS, UNDERGROUND CONDUIT RUNS, SERVICE POINTS AND HAND--HOLES SHALL BE SHOWN ON THE "AS--BUILT" PLANS. NO DIRECT BURIAL STREET LIGHT FOUNDATIONS WILL BE ALLOWED. ALL LUMINAIRES SHALL BE "CUT OFF" TYPE.
- ALL UNDERGROUND UTILITIES AND LATERALS SHALL BE INSTALLED PRIOR TO CONSTRUCTION OF CURBS, CROSS GUTTERS OR SURFACING OF STREETS.
- WHERE AN EXISTING PIPE LINE IS TO BE ABANDONED IT SHALL BE REMOVED WITHIN TWENTY FEET OF BUILDING OR STREET AREAS AND REPLACED WITH PROPERLY COMPACTED SOILS. IN OTHER AREAS THE PIPE WILL BE PLUGGED WITH CONCRETE OR REMOVED AS APPROVED BY THE CITY ENGINEER.
- WHEELCHAIR RAMP SHALL BE INSTALLED PER REQUIREMENTS OF TITLE 24, CALIFORNIA ADMINISTRATIVE CODE.
- STREET TREES SHALL BE INSTALLED PER SECTION VI OF THE CITY OF CARLSBAD LANDSCAPE MANUAL.
- FIRE HYDRANT MARKERS SHALL BE PLACED IN THE STREET WITHIN THE LIMITS OF WORK OF THIS PLAN PER SDRS DWG M19.
- ALL SURVEY MONUMENTS AND POINTS THAT ARE DISTURBED BY THIS WORK SHALL BE REESTABLISHED, PERPETUATED, AND DOCUMENTED PER THE PROFESSIONAL LAND SURVEYORS ACT.
- TWO (2) SETS OF SURVEY (CUT) SHEETS SHALL BE PROVIDED TO THE CITY INSPECTOR.
- STREET LIGHTS SHALL BE 5,500 LUMEN (40 WATT) OR 13,700 PUPIL LUMEN (100 WATT) HIGH EFFICIENCY INDUCTION LIGHTS.

SEWER NOTES

- SEWER MAIN AND APPURTENANCES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "CITY OF CARLSBAD ENGINEERING STANDARDS (LATEST EDITION), VOLUME 1--GENERAL DESIGN STANDARDS, CHAPTER 6--DESIGN CRITERIA FOR GRAVITY SEWER LINES AND APPURTENANCES, AND VOLUME 3 -- STANDARD DRAWINGS AND NOTES AND THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION (GREEN BOOK).
- BEFORE ANY CONNECTION TO THE CITY'S EXISTING SYSTEM, A PERMIT SHALL BE OBTAINED FROM THE CITY. IT MUST BE SIGNED AND APPROVED BY THE CITY.
- BEFORE CONSTRUCTION BEGINS IN ANY PUBLIC RIGHT OF WAY, A CITY RIGHT OF WAY PERMIT SHALL BE REQUIRED.

WATER NOTES

- WATER & RECYCLED WATER MAIN AND APPURTENANCES SHALL BE CONSTRUCTED ACCORDANCE WITH THE "CITY OF CARLSBAD ENGINEERING STANDARDS" (LATEST EDITION) VOLUMES 2 & 3.
- BEFORE ANY CONNECTION OR SHUT DOWN OF VALVES ON EXISTING C.M.W.D. LINES, CONTRACTOR MUST SUBMIT A UTILITY REQUEST FORM (E--28) A MINIMUM OF 2 WEEKS PRIOR TO REQUESTED WORK BEING PERFORMED.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF CARLSBAD CONSTRUCTION MANAGEMENT & INSPECTION DEPARTMENT 48 HOURS PRIOR TO STARTING WORK SO THAT INSPECTION MAY BE PROVIDED.
- NO TREES OR STRUCTURES SHALL BE ALLOWED IN THE WATER LINE EASEMENT, ALL EXISTING TREES AND STRUCTURES SHALL BE REMOVED PRIOR TO COMMENCEMENT OF WORK -- ANY EXCEPTIONS SHALL REQUIRE WRITTEN PERMISSION FROM THE DISTRICT ENGINEER.
- ALL BURIED COPPER PIPING AND APPURTENANCES SHALL BE PROTECTED BY MEANS OF CATHODIC PROTECTION.
- THE EXACT HORIZONTAL AND VERTICAL ALIGNMENT AND TYPE OF MATERIALS OF THE FIRE LINE LOCATED BETWEEN THE DETECTOR CHECK VALVE AND BUILDING SHALL BE DETERMINED BY THE CITY OF CARLSBAD FIRE MARSHAL.
- ALL POTABLE WATER SERVICES SHALL BE 1" AND ALL METERS SHALL BE 1" (UNLESS OTHERWISE NOTED ON PLANS).
- ALL WATER METER BOXES SHALL BE 17" X 30" X 12" POLYMER CONCRETE AS MANUFACTURED BY ARMORCAST OR EQUAL.
- MINIMUM SPACING BETWEEN POTABLE WATER SERVICES AND SEWER LATERALS SHALL BE 10 FEET OR THE DDW SEPARATION REQUIREMENTS, WHICHEVER IS MORE STRINGENT.
- THE TOP OF WATER METER BOXES SHALL BE FLUSH WITH THE FINISHED SURFACE GRADE.
- CONSTRUCTION OF WATER MAINS AND RECYCLED WATER MAINS SHALL ADHERE TO THE "CRITERIA FOR THE SEPARATION OF WATER MAINS AND SANITARY SEWERS" PUBLISHED BY THE STATE OF CALIFORNIA'S DEPARTMENT OF HEALTH SERVICES.
- AXIAL DEFLECTION AT THE PIPE JOINTS IS NOT ALLOWED. THE USE OF A HIGH--DEFLECTION COUPLING AT A PIPE JOINT MAY BE PERMITTED BY THE INSPECTOR ON A CASE BY CASE BASIS NOT TO EXCEED 4 DEGREES TOTAL DEFLECTION PER COUPLING (2 DEGREES/EACH GASKET). USE CERTAINTeed "VINYL--IRON" H.D. FOR C909.
- BEFORE CONSTRUCTION BEGINS IN ANY PUBLIC RIGHT OF WAY, A CITY RIGHT OF WAY PERMIT SHALL BE REQUIRED.
- NO CUTTING OF ASBESTOS CEMENT PIPE (ACP) IS PERMITTED. CONTRACTOR SHALL REMOVE ACP TO NEAREST COUPLING AND INSTALL ACP X PVC ADAPTORS AND THE NECESSARY PVC SIZE AND CLASS PIPE TO INSTALL THE TEE AT THE STATION.
- PRIOR TO BACKFILL, INSTALL TRACER WIRE ON TOP OF PIPE AND SECURE IN PLACE WITH 2--INCH WIDE PLASTIC ADHESIVE TAPE AT MAXIMUM 10--FOOT INTERVALS. WIRE TO ENTER VALVE STANDS BY MEANS OF DRILLED HOLE IN PIPE STAND WALL JUST BELOW 4TBOX. WIRE IS NOT TO INTERFERE WITH VALVE NUT OPERATION. RUN TRACER WIRE CONTINUOUSLY ALONG PIPE AND TERMINATE IN ADJACENT VALVE BOXES FOR BURIED ASSEMBLIES OR BURIED VALVES. WHERE BURIED SPLICES OCCUR, USE AN ELECTRICAL EPOXY RESIN SUCH AS SCOTCHCAST, OR APPROVED EQUAL. PROVIDE 24 INCHES OF COILED WIRE AT ACCESS POINTS FOR ATTACHMENTS OF PIPE LOCATING EQUIPMENT. EACH INSTALLED RUN OF PIPE SHALL BE CAPABLE OF BEING LOCATED USING THE TRACER WIRE. PROTECT WIRE INSULATION FROM DAMAGE DURING INSTALLATION AND BACKFILLING. WIRE INSULATION THAT IS BROKEN, CUT, OR DAMAGED SHALL BE REPLACED. TRACER WIRE SHALL BE AWG NO. 8 STRANDED COPPER WIRE WITH HIGH MOLECULAR WEIGHT POLYETHYLENE (HMW/PE) INSULATION SPECIFICALLY DESIGNED FOR DIRECT BURIAL IN CORROSIVE SOIL OR WATER. POLYETHYLENE INSULATION SHALL CONFORM TO ASTM D 1248, TYPE 1, CLASS C. WIRES WITH CUT OR DAMAGED INSULATION ARE NOT ACCEPTABLE AND REPLACEMENT OF THE ENTIRE WIRE WHICH HAS BEEN DAMAGED WILL BE REQUIRED AT THE CONTRACTOR'S EXPENSE.

SIGNING AND STRIPING NOTES

- PREPARATION OF SIGNING AND STRIPING PLAN TO BE APPROVED BY THE CITY INSPECTOR. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING ALL APPROVED SIGNING AND STRIPING.
- SIGNING, STRIPING AND PAVEMENT MARKINGS SHALL CONFORM TO THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (FHWA'S MUTCD latest version, AS AMENDED FOR USE IN CALIFORNIA), CALTRANS STANDARD SPECIFICATIONS (LATEST VERSION), THESE PLANS AND THE SPECIAL PROVISIONS.
- ALL SIGNING AND STRIPING IS SUBJECT TO THE APPROVAL OF THE TRANSPORTATION DIRECTOR OR HIS REPRESENTATIVE, PRIOR TO INSTALLATION.
- ANY DEVIATION FROM THESE SIGNING AND STRIPING PLANS SHALL BE APPROVED BY THE ENGINEER OF WORK AND TRANSPORTATION DIRECTOR PRIOR TO ANY CHANGE IN THE FIELD.
- ALL SIGNING AND STRIPING SHALL BE REFLECTIVE PER CALTRANS SPECIFICATIONS. STRIPING SHALL BE REPAINTED TWO WEEKS AFTER INITIAL PAINTING. SIGNING SHALL USE ENCAPSULATED LENS REFLECTIVE SHEETING (HIGH INTENSITY OR EQUAL).
- EXACT LOCATION OF STRIPING AND STOP LIMIT LINES SHALL BE APPROVED BY THE TRANSPORTATION DIRECTOR OR THEIR REPRESENTATIVE PRIOR TO INSTALLATION.
- CONTRACTOR SHALL REMOVE ALL CONFLICTING PAINTED LINES, MARKINGS AND PAVEMENT LEGENDS BY GRINDING. DEBRIS SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR. NO PRESSURE WASHING OR GRADING DEBRIS SHALL ENTER THE GUTTER LINE OR STORM DRAIN SYSTEM.
- ALL PAVEMENT LEGENDS SHALL BE THE LATEST VERSION OF THE CALTRANS STENCILS.
- LIMIT LINES AND CROSSWALKS SHALL BE FIELD LOCATED. CROSSWALKS SHALL HAVE 10' INSIDE DIMENSION UNLESS OTHERWISE SPECIFIED.
- ALL CROSSWALKS, LIMIT LINES, STOP BARS, PAVEMENT ARROWS AND PAVEMENT LEGENDS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.
- FIRE HYDRANT PAVEMENT MARKERS SHALL CONFORM TO THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (FHWA'S MUTCD LATEST VERSION, AS AMENDED FOR USE IN CALIFORNIA) AND SAN DIEGO REGIONAL STANDARD DRAWING M--19.
- ALL MEDIAN NOSES AND FLARES SHALL BE PAINTED YELLOW.
- ALL SIGNS SHALL BE STANDARD SIZE AS SHOWN IN THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (FHWA'S MUTCD LATEST VERSION, AS AMENDED FOR USE IN CALIFORNIA) UNLESS OTHERWISE SPECIFIED.
- SIGN POSTS SHALL BE SQUARE PERFORATED STEEL TUBING WITH BREAKAWAY BASE PER SAN DIEGO REGIONAL STANDARD DRAWING M--45.
- WHEN A SIGN IS ATTACHED TO A POLE, IT SHALL BE MOUNTED USING A STANDARD CITY OF CARLSBAD MOUNTING BRACKET WITH STRAPS.
- EXISTING SIGNS REMOVED BY THE CONTRACTOR SHALL BE DELIVERED BY THE CONTRACTOR TO THE CITY OF CARLSBAD PUBLIC WORKS YARD AT 405 OAK AVENUE.
- ALL SIGNS SHOWN ON THESE PLANS SHALL BE NEW SIGNS PROVIDED AND INSTALLED BY THE CONTRACTOR EXCEPT THOSE SIGNS SPECIFICALLY SHOWN AS EXISTING TO BE RELOCATED OR TO REMAIN.
- (IF NECESSARY) WHERE R4--7 SIGN AND OM1--3 OBJECT MARKER ARE TO BE INSTALLED IN A 2--FOOT WIDE MEDIAN NOSE, THE R4--7 SIGN SHALL BE 18"x24" AND THE OM1--3 OBJECT MARKER SHALL BE 12"x12".
- ALL NEW AND RELOCATED SIGNS SHALL BE CLEARLY VISIBLE, NOT BLOCKED BY ANY OBSTRUCTION. THE CONTRACTOR IS RESPONSIBLE TO TRIM OR REMOVE ANY VEGETATION BLOCKING THE NEW AND RELOCATED SIGNS.

SPECIAL NOTES:

THE FOLLOWING NOTES ARE PROVIDED TO GIVE DIRECTIONS TO THE CONTRACTOR BY THE ENGINEER OF WORK. THE CITY ENGINEER'S SIGNATURE ON THESE PLANS DOES NOT CONSTITUTE APPROVAL OF ANY OF THESE NOTES AND THE CITY WILL NOT BE RESPONSIBLE FOR THEIR ENFORCEMENT.

- CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND THAT THE CONTRACTORS SHALL DEFEND, INDEMNIFY, AND HOLD THE OWNER, ENGINEER AND GEOLOGIST HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN THE CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROPERTY, EXCEPTING FOR LIABILITY ARISING FROM SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.
- THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING FACILITIES (ABOVEGROUND AND UNDERGROUND) WITHIN THE PROJECT SITE AND MAKE EXPLORATORY EXCAVATIONS SUFFICIENTLY AHEAD OF CONSTRUCTION TO PERMIT THE REVISION OF THE CONSTRUCTION PLANS IF IT IS FOUND THE ACTUAL LOCATIONS ARE IN CONFLICT WITH THE PROPOSED WORK.
- DURING CONSTRUCTION: THE CONTRACTOR SHALL PROPERLY GRADE ALL EXCAVATED SURFACES TO PROVIDE POSITIVE DRAINAGE AND PREVENT PONDING OF WATER. HE SHALL CONTROL SURFACE WATER TO AVOID DAMAGE TO ADJOINING PROPERTIES OR TO FINISHED WORK ON THE SITE.
- NASLAND ENGINEERING WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL PROPOSED CHANGES TO THE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY NASLAND ENGINEERING.
- WORK PERFORMED WITHOUT BENEFIT OF TESTING AND/OR INSPECTION SHALL BE SUBJECT TO REJECTION AND REMOVAL.
- THE EXISTENCE AND LOCATION OF UTILITY STRUCTURES AND FACILITIES SHOWN ON THE CONSTRUCTION PLANS WERE OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS. ATTENTION IS CALLED TO THE POSSIBLE EXISTENCE OF THE UTILITY FACILITIES OR STRUCTURES NOT KNOWN OR IN A LOCATION DIFFERENT FROM THAT SHOWN ON THE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITIES SHOWN ON THE PLANS AND ANY OTHER EXISTING FACILITIES OR STRUCTURES THAT MAY NOT BE SHOWN.

PROJECT STANDARD PLANS

SAN DIEGO STANDARD DRAWINGS (SDRSD, 2018)

G--02	CURB AND GUTTER -- COMBINED
G--07	SIDEWALK -- TYPICAL SECTIONS
G--09	SIDEWALK JOINT LOCATIONS
G--10	CONCRETE JOINT DETAILS
G--11	CONCRETE CURB, GUTTER, SIDEWALK, & PAVEMENT REMOVAL AND REPLACEMENT
G--12	CROSS GUTTER

ALL STANDARD DRAWINGS ARE SAN DIEGO REGIONAL STANDARD DRAWINGS (SDRSD) UNLESS NOTED OTHERWISE:
 * CITY OF CARLSBAD STANDARD
 ** CARLSBAD MUNICIPAL WATER DISTRICT STANDARD DRAWING
 *** SDRSD AS MODIFIED BY CITY OF CARLSBAD

CALTRANS STANDARD PLANS (2018):

AB8A	CURB RAMP DETAILS
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(TRUNCATED DOMES TO COMPLY WITH CALTRANS STANDARD PLANS)



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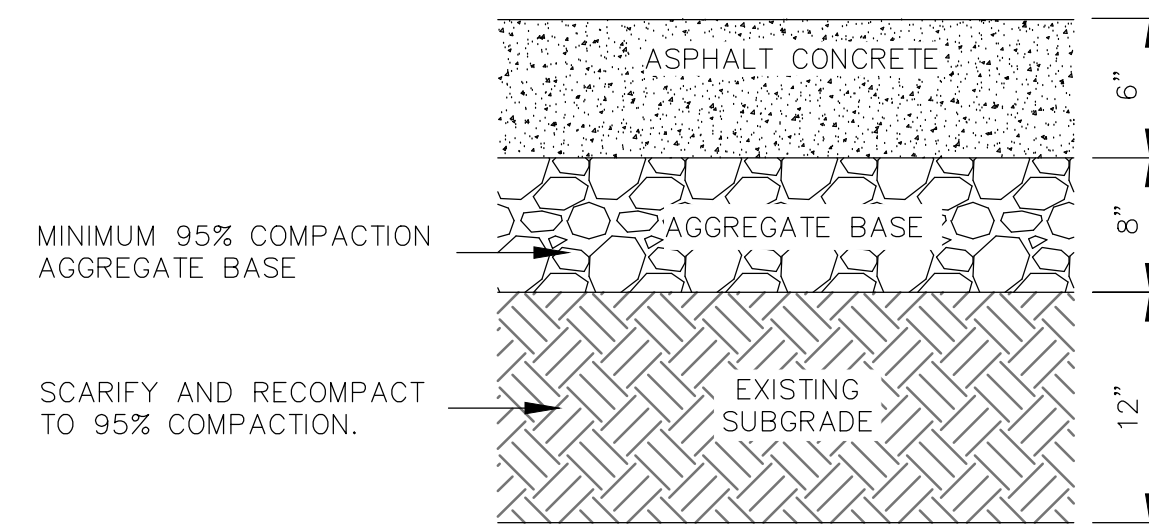
JOB NO.: 121-108.1

SHEET 2		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
NOTES FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: HOSSEIN AJUDEH					
ENGINEERING MANAGER RCE 75991 EXP.6/20/24 DATE					
DWN BY: EE/DG		CIP NO.		DRAWING NO.	
CHKD BY: DQ		6004		DWG 536--6	
REVWD BY: LT					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL		CITY APPROVAL	
REVISION DESCRIPTION					

ABBREVIATIONS

ALL ABBREVIATIONS ARE PER SECTION 1-3 OF THE GREEN BOOK WITH THE ADDITIONS INDICATED BELOW:

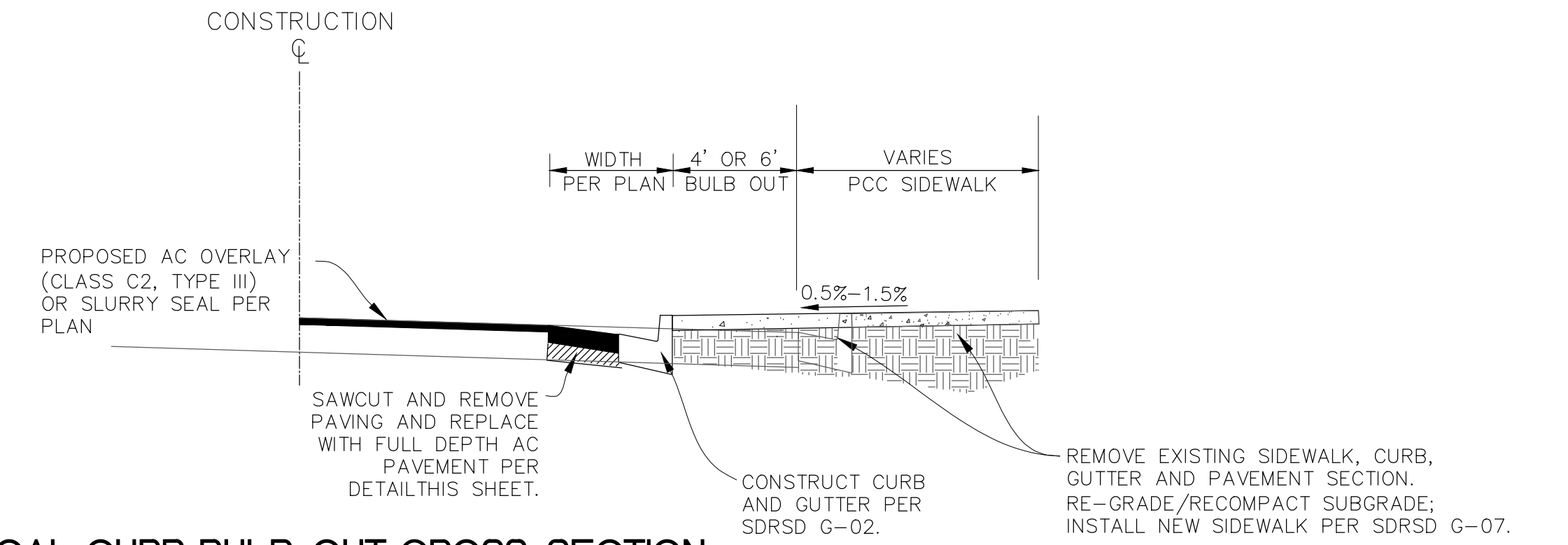
ABAND	ABANDONED	MAT'L	MATERIAL
ASS'Y	ASSEMBLY	MIN	MINIMUM
BFD	BACKFLOW DEVICE	MJ	MECHANICAL JOINT
BO	BLOW-OFF	N.I.C.	NOT A PART OF THIS CONTRACT
COMB	COMBINATION	PVC	POLYVINYL CHLORIDE
CAW	CALIFORNIA AMERICAN WATER	SDRSD	SAN DIEGO REGIONAL STANDARD DRAWINGS
DG	DECOMPOSED GRANITE	RT	RIGHT
ELEV	ELEVATION	R/W	RIGHT-OF-WAY
EXIST.	EXISTING	SWR	SEWER
EJ	EXPANSION JOINT	TB	TOP OF BERM
EUG	ELECTRICAL UNDERGROUND CONDUIT	TG	TOP OF GRATE
FLG/FLG'D	FLANGE/FLANGED	TP	TOP OF PIPE
FS	FINISHED SURFACE	WTR	WATER
GV	GATE VALVE	STA	STATION
HDPE	HIGH DENSITY POLYETHYLENE	PCC	PORTLAND CONCRETE CEMENT ASPHALT CONCRETE
HP	HIGH PRESSURE	AC	ASPHALT CONCRETE
INT	INTERSECTION	TW	TOP OF WALL
IE	INVERT ELEVATION	TC	TOP OF CURB
LT	LEFT	FL	FLOW LINE
RT	RIGHT	PCR	POINT OF CURB RETURN
HORIZ	HORIZONTAL	VERT	VERTICAL
BC	BEGINNING OF CURVE	EC	END OF CURVE
SF	SQUARE FEET	OC	ON CENTER
C&G	CURB AND GUTTER	ST	STREET
BLVD	BOULEVARD	AVE	AVENUE
ADA	AMERICANS WITH DISABILITIES ACT	MAX	MAXIMUM
ISA	INTERNATIONAL SYMBOL OF ACCESSIBILITY	MUTCD	MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
ASTM	AMERICAN SOCIETY OF TESTING & MATERIALS		
TYP	TYPICAL		



NOTES:

1. SOIL COMPACTION FOR AGGREGATE BASE TO BE INSTALLED PER ASTM D1557 MAXIMUM DRY DENSITY.
2. CONTRACTOR TO USE CLASS C3, TYPE (III) AC MIXTURE FOR SAWCUT PATCHWORK.

PROPOSED PAVEMENT SECTION
NOT TO SCALE



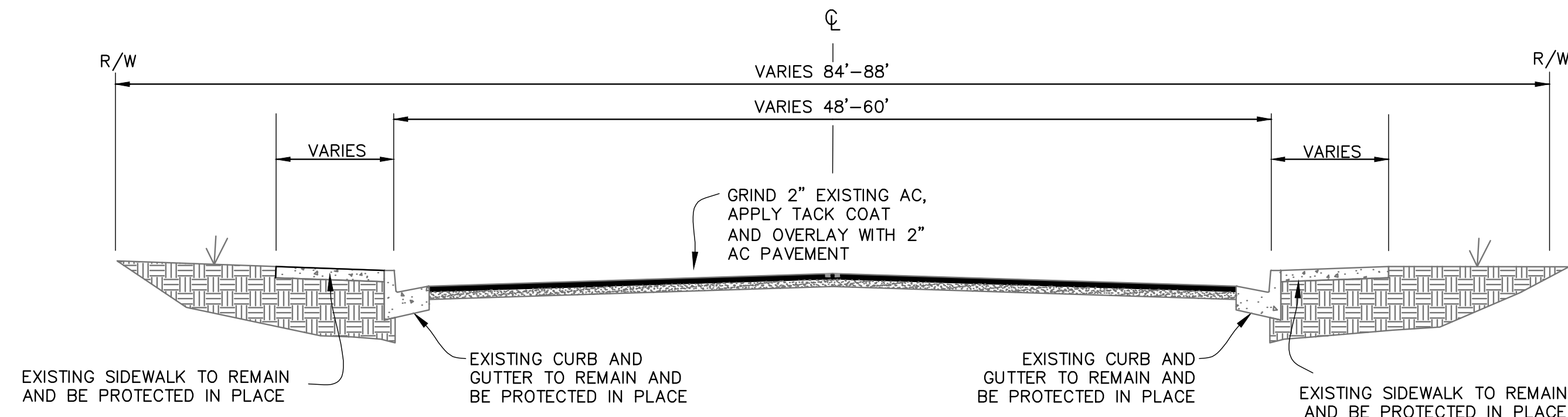
TYPICAL CURB BULB-OUT CROSS-SECTION
NO SCALE



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SHEET 3		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
DETAILS FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: _____ HOSSEIN AJIDEH					
ENGINEERING MANAGER RCE 75991 EXP.6/20/24 DATE					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					
DWN BY: EF/DG		CIP NO. 6004		DRAWING NO. DWG 536-6	
CHKD BY: DG					
RVWD BY: LT					

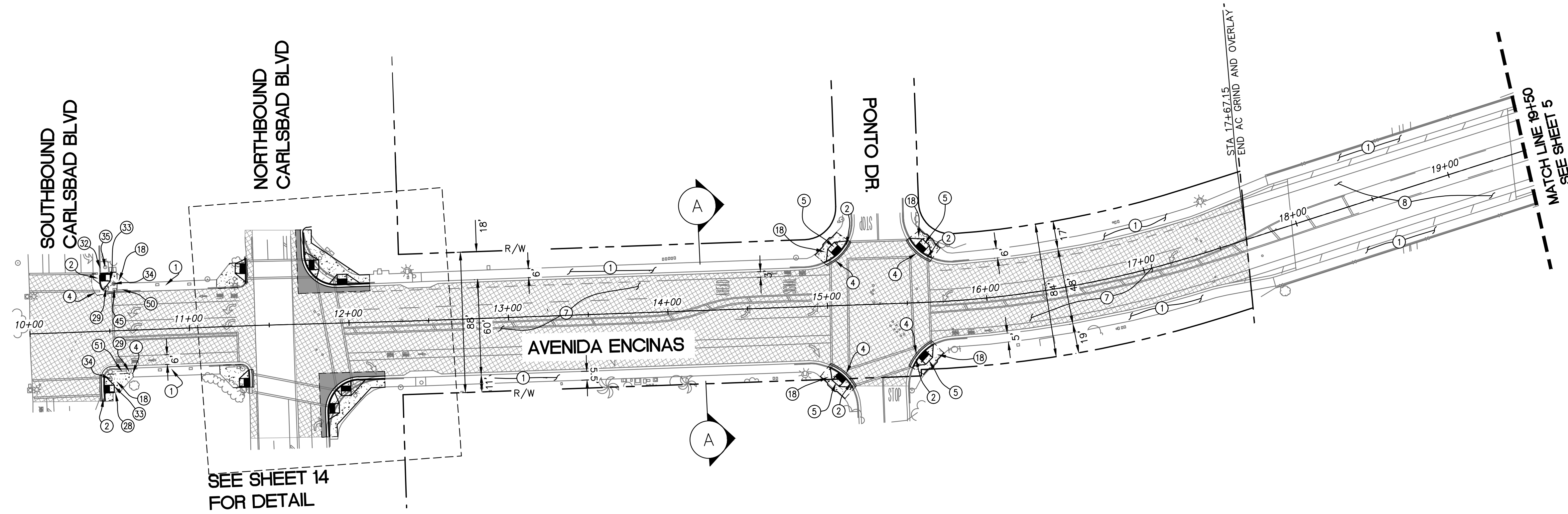


TYPICAL AVENIDA ENCINAS CROSS-SECTION (A-A)

NO SCALE
STA: 10+00 - STA: 17+67.15

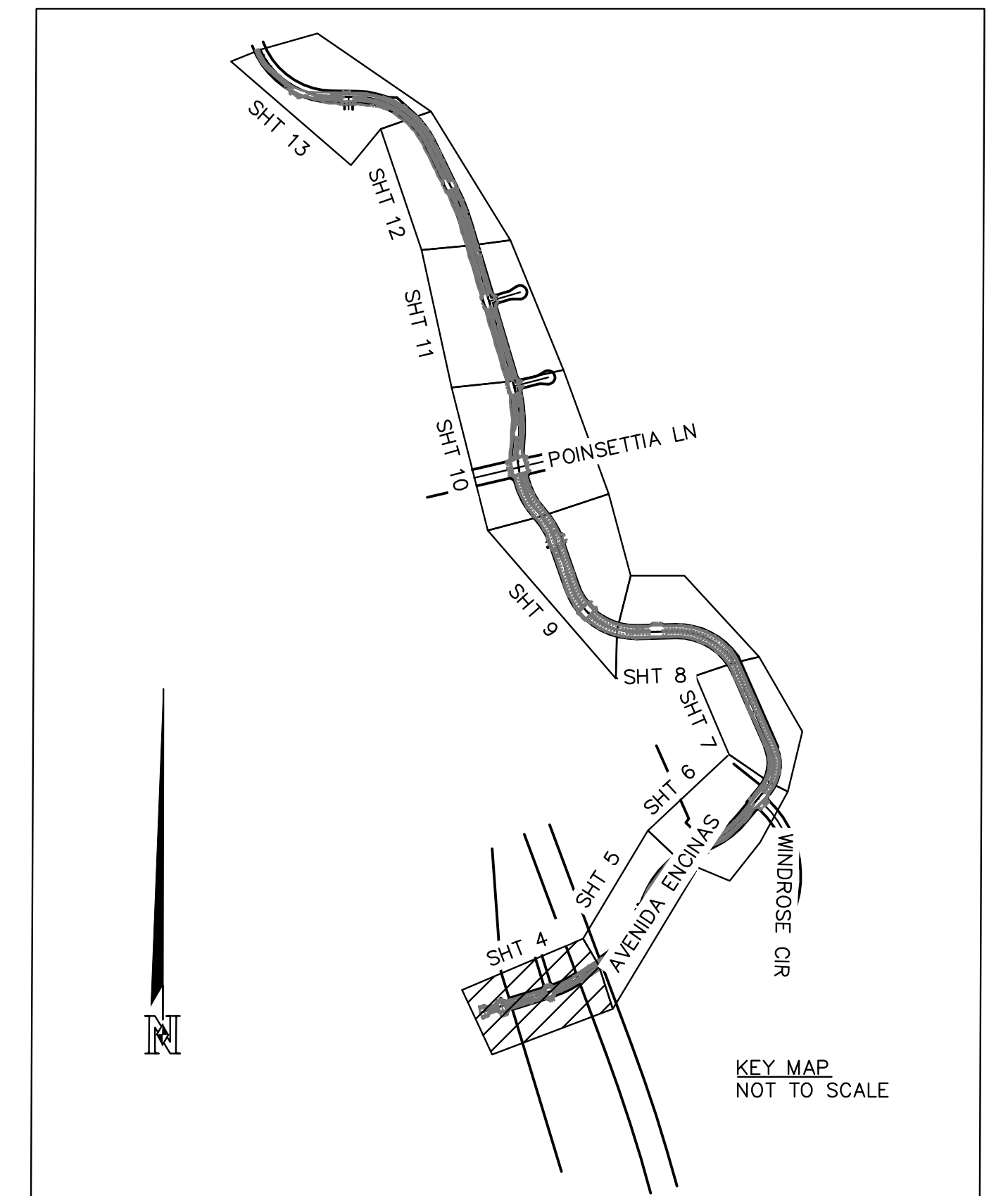
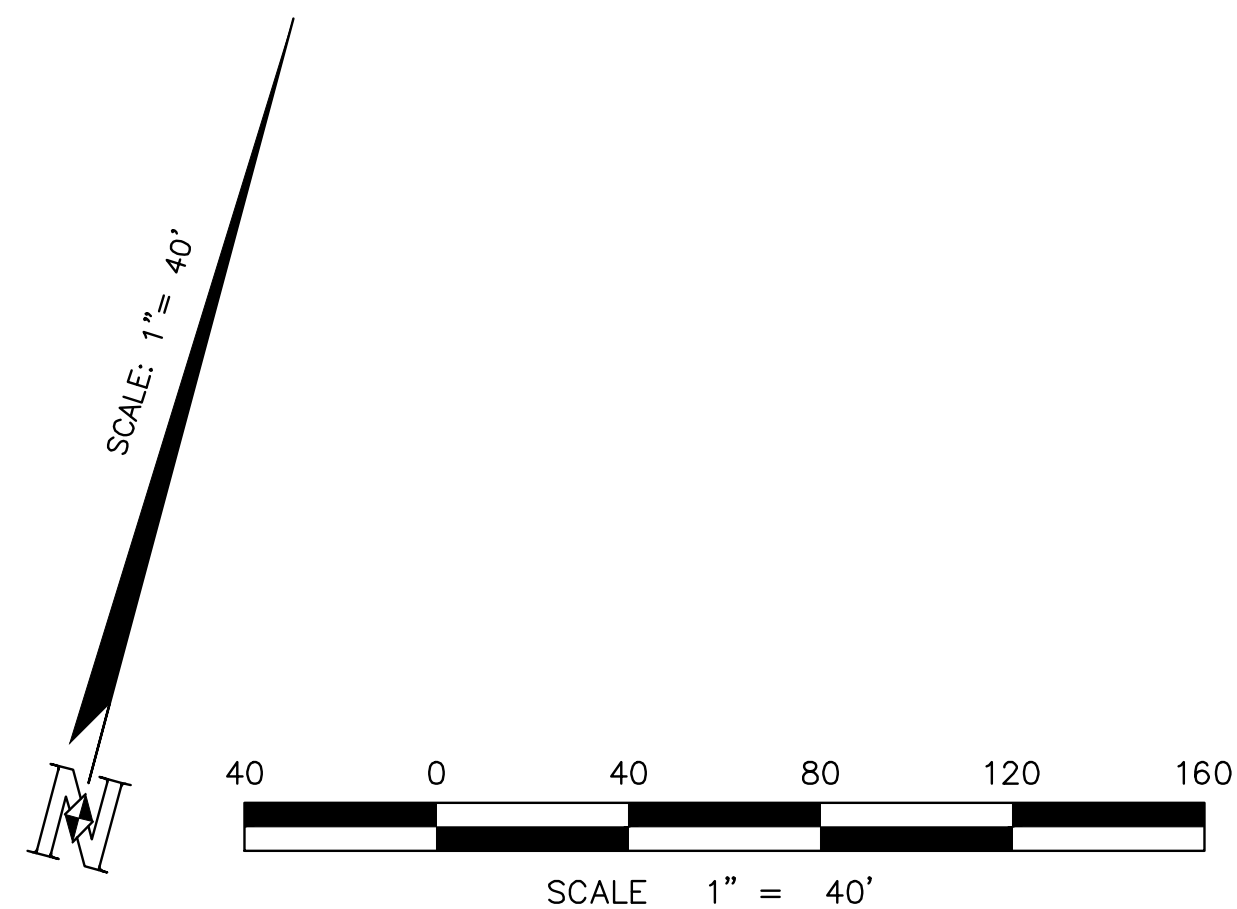
CONSTRUCTION NOTES

- 1 EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- 2 INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- 4 PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- 5 REMOVE AND REPLACE WITH CASE D CURB RAMP PER CALTRANS CRD A88A.
- 7 2" GRIND AND OVERLAY.
- 8 EXISTING PCC TO REMAIN AND BE PROTECTED IN PLACE.
- 18 INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- 28 EXISTING LANDSCAPING AND IRRIGATION IN CONFLICT TO BE REMOVED AS NEEDED.
- 29 EXISTING STREET SIGN TO BE REMOVED AND RELOCATED AS NEEDED.
- 32 EXISTING CURB INLET TO REMAIN AND BE PROTECTED IN PLACE.
- 33 REMOVE AND REPLACE WITH CASE G CURB RAMP PER CALTRANS CRD A88A.
- 34 EXISTING TRAFFIC SIGNAL TO REMAIN AND BE PROTECTED IN PLACE.
- 35 EXISTING SEWER MANHOLE TO REMAIN AND BE PROTECTED IN PLACE.
- 45 EXISTING PULL BOX TO REMAIN AND BE PROTECTED IN PLACE.
- 50 EXISTING PEDESTRIAN BARRICADE TO REMAIN AND BE PROTECTED IN PLACE.
- 51 EXISTING PEDESTRIAN BARRICADE TO BE RELOCATED AS NEEDED.



PLAN: AVENIDA ENCINA

SCALE: 1" = 40'

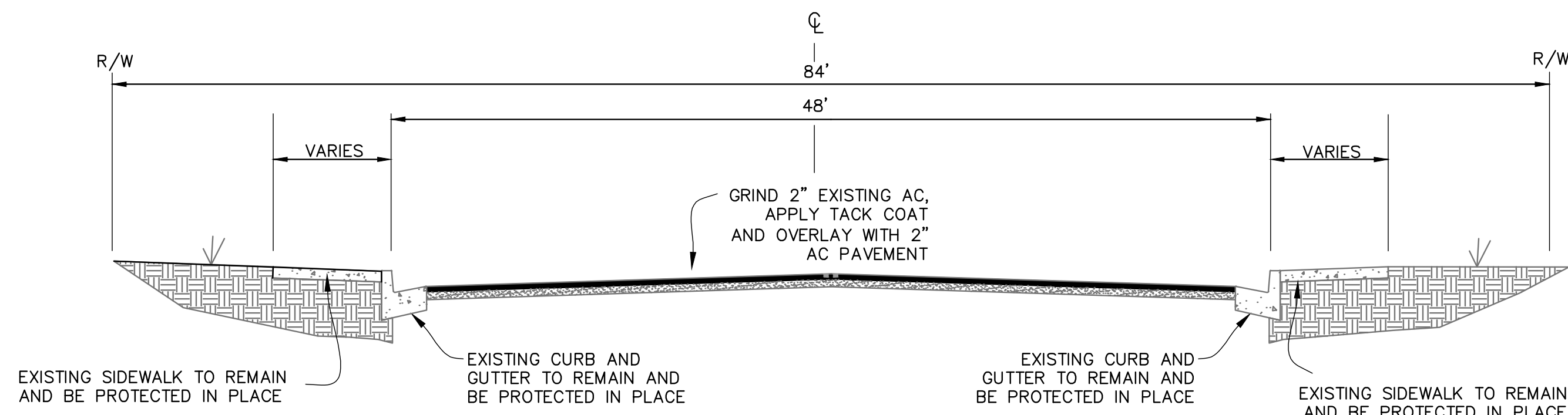


SHEET 4		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 10+00 TO 19+50					
APPROVED:		HOSSEIN AJIDEH			
ENGINEERING MANAGER		RCE 75991 EXP.6/20/24		DATE	
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DQ		6004		DWG 536-6	
RVWD BY: LT					



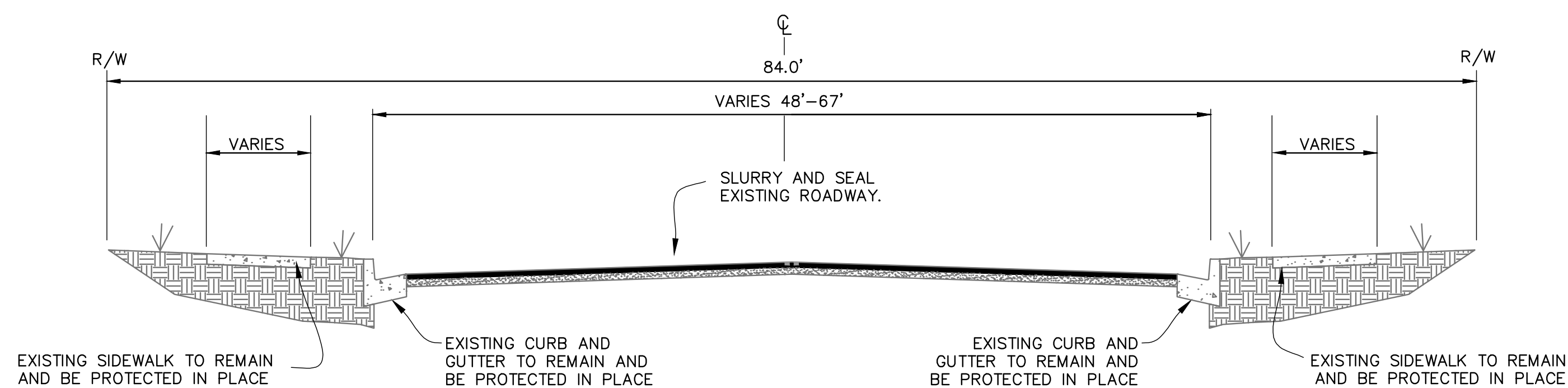
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JOB NO.: 121-108.1



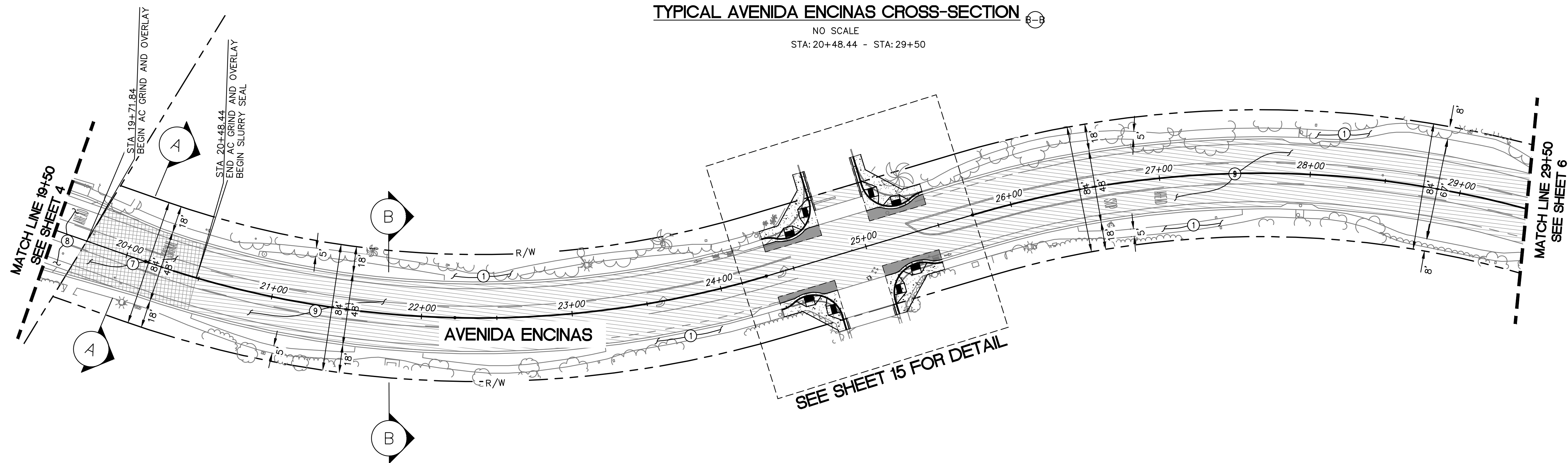
TYPICAL AVENIDA ENCINAS CROSS-SECTION A-A

NO SCALE
STA: 19+71.84 - STA: 20+48.44



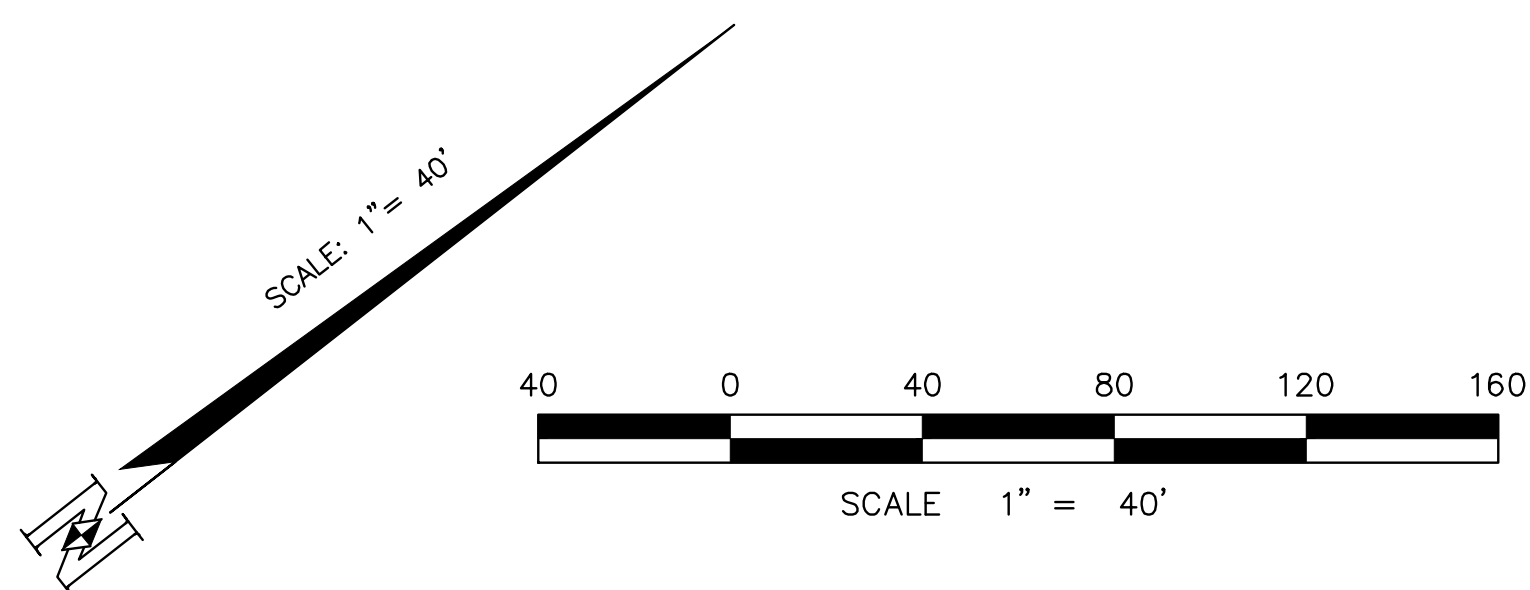
TYPICAL AVENIDA ENCINAS CROSS-SECTION B-B

NO SCALE
STA: 20+48.44 - STA: 29+50



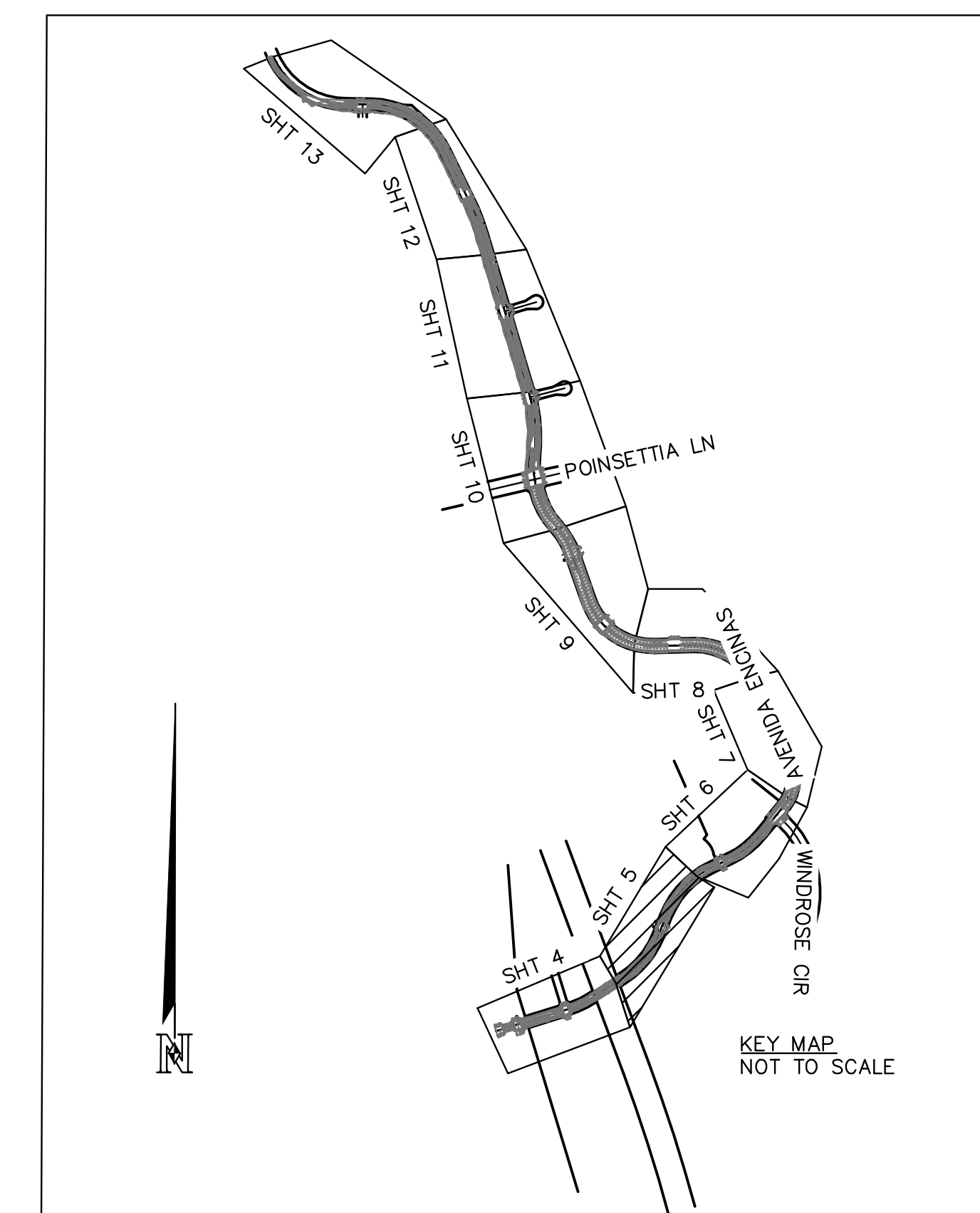
PLAN: AVENIDA ENCINA

SCALE: 1" = 40'



CONSTRUCTION NOTES

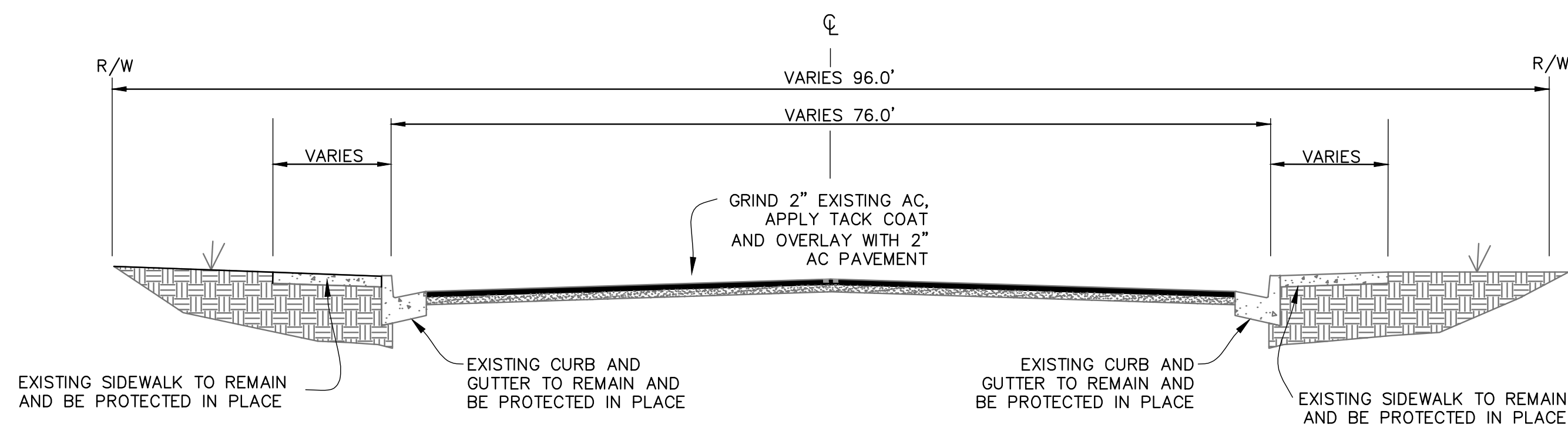
- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ⑦ 2" GRIND AND OVERLAY.
- ⑧ EXISTING PCC TO REMAIN AND BE PROTECTED IN PLACE.
- ⑨ SLURRY SEAL EXISTING ROADWAY



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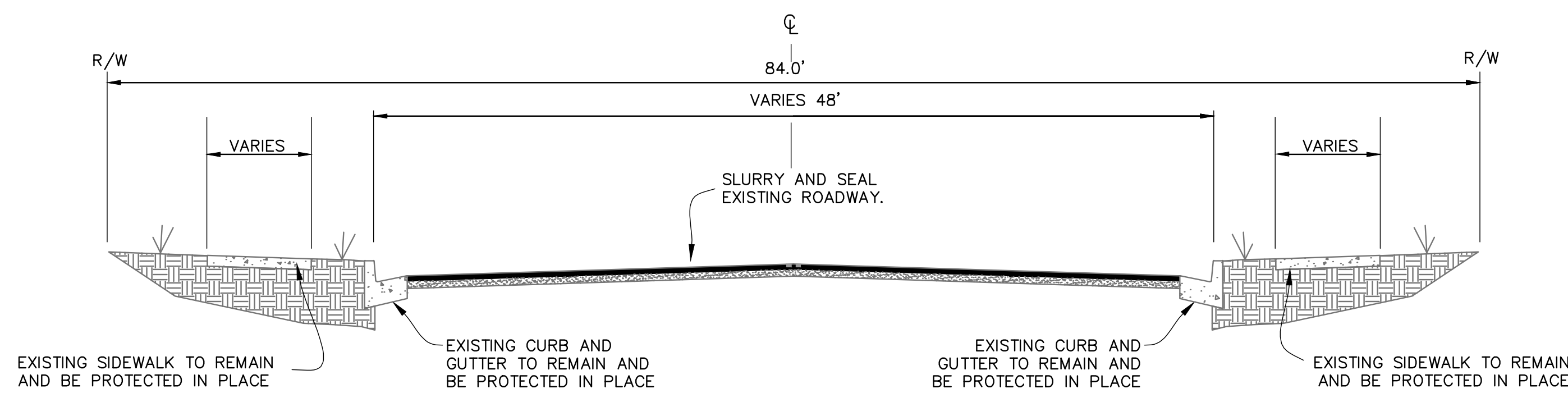
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SHEET 5		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 19+50 TO 29+50					
APPROVED:		HOSSEIN AJIDEH			
ENGINEERING MANAGER		RCE 75991 EXP. 6/20/24		DATE	
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DQ		6004		DWG 536-6	
RWVD BY: LT					



TYPICAL AVENIDA ENCINAS CROSS-SECTION (A-A)

NO SCALE
STA: 36+27.20 - STA: 38+50

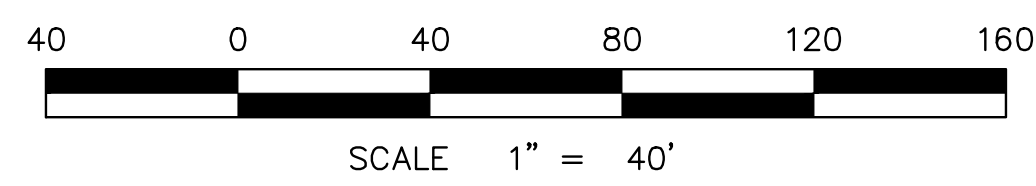
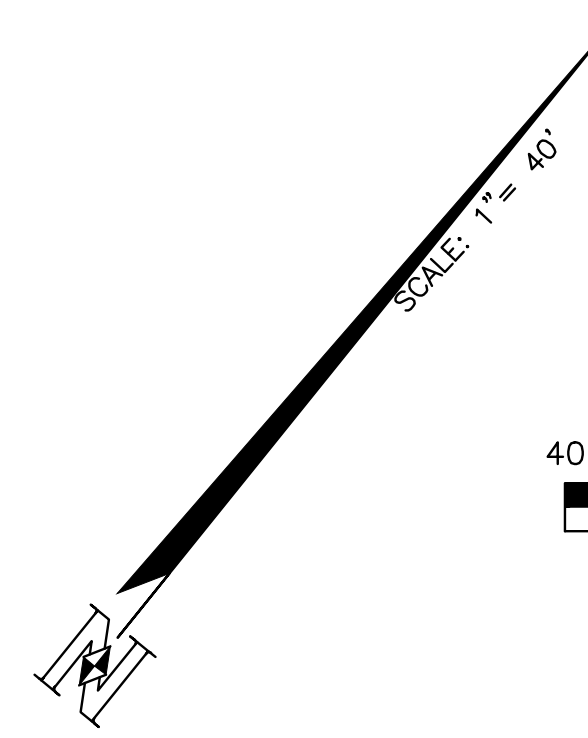
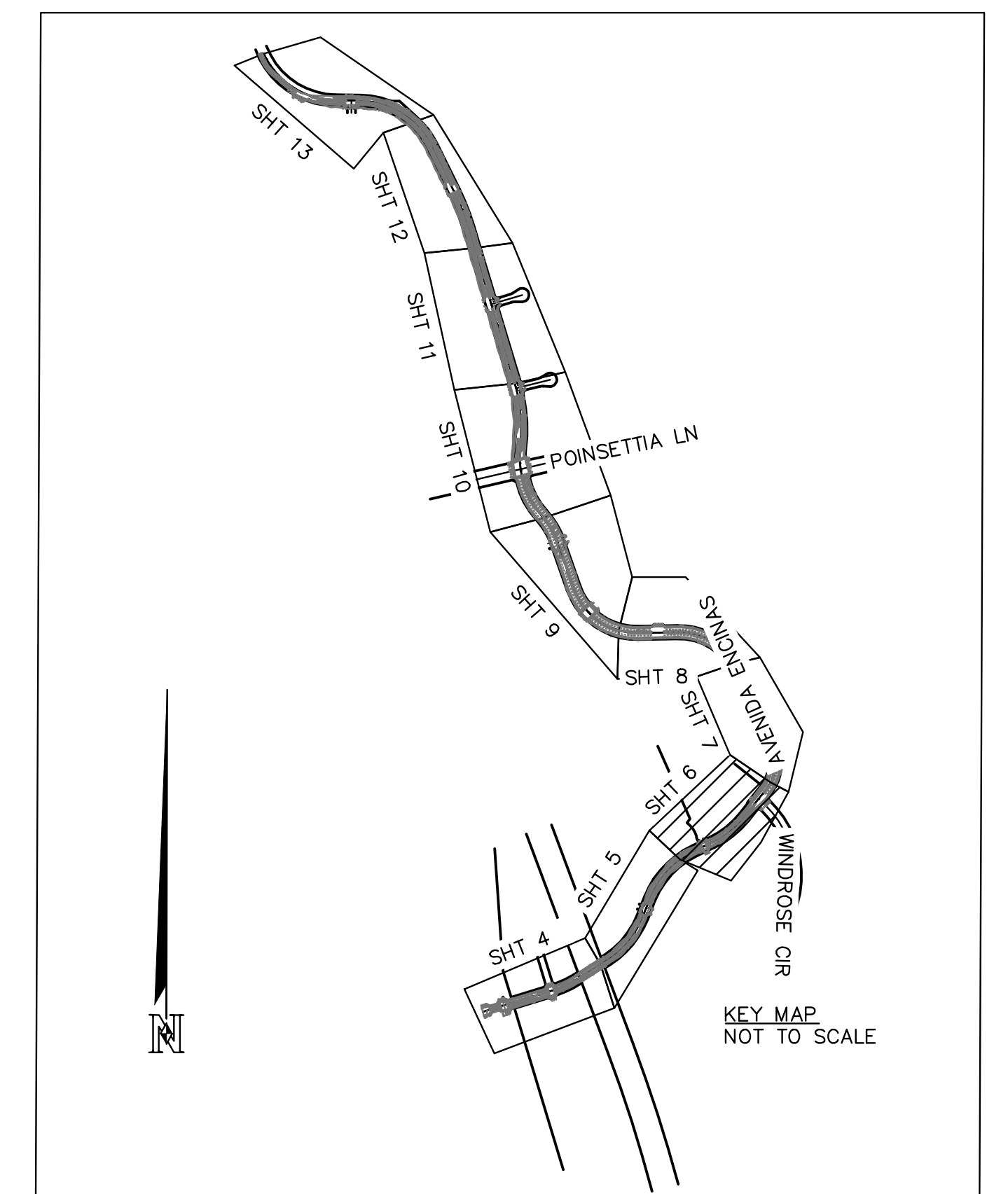
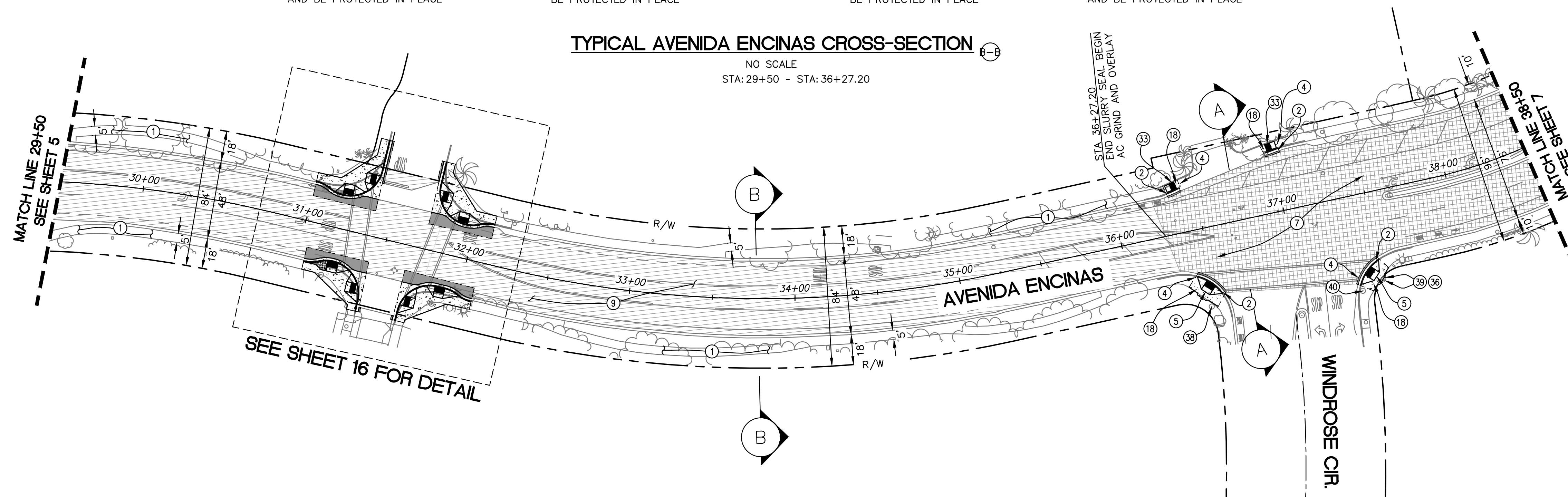


TYPICAL AVENIDA ENCINAS CROSS-SECTION (B-B)

NO SCALE
STA: 29+50 - STA: 36+27.20

CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑤ REMOVE AND REPLACE WITH CASE D CURB RAMP PER CALTRANS CRD A88A.
- ⑦ 2" GRIND AND OVERLAY.
- ⑨ SLURRY SEAL EXISTING ROADWAY
- ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑳ REMOVE AND REPLACE WITH CASE G CURB RAMP PER CALTRANS CRD A88A.
- ㉔ EXISTING PULL BOX TO BE ADJUSTED TO GRADE AS NEEDED.
- ㉖ EXISTING COMMUNICATION RISER TO REMAIN AND BE PROTECTED IN PLACE.
- ㉗ EXISTING IRRIGATION LINES TO BE ADJUSTED.
- ㉘ EXISTING STREET SIGN TO REMAIN AND BE PROTECTED IN PLACE.



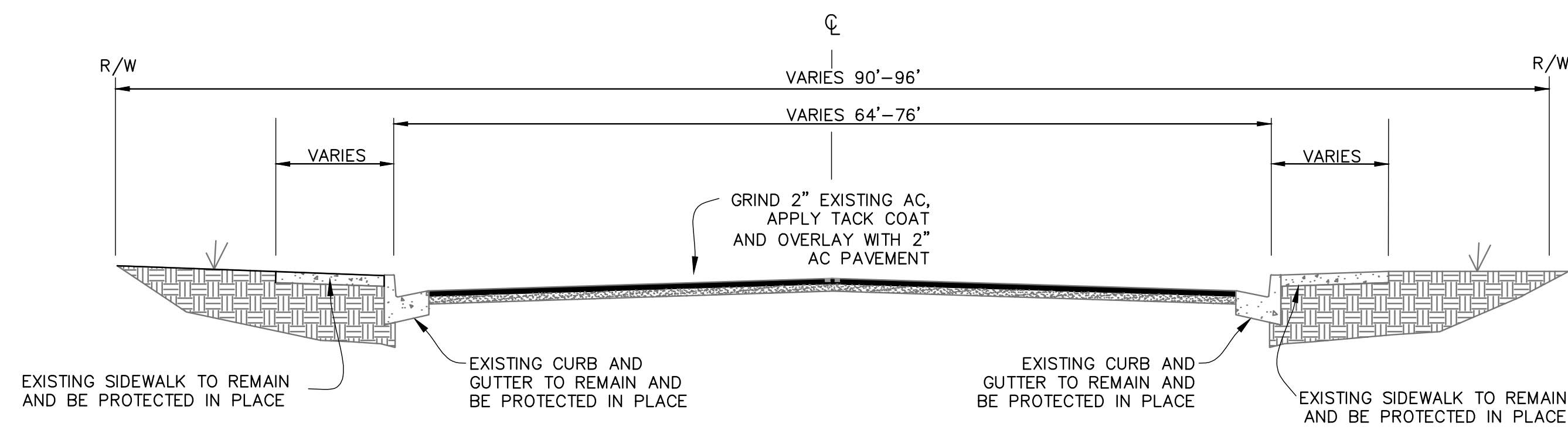
PLAN: AVENIDA ENCINA
SCALE: 1" = 40'



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SHEET 6		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 29+50 TO 38+50					
APPROVED: _____				HOSSEIN AJIDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24	
DATE _____		DATE _____		DATE _____	
INITIAL _____		INITIAL _____		INITIAL _____	
ENGINEER OF WORK		REVISION DESCRIPTION		OTHER APPROVAL	
				CITY APPROVAL	
DWN BY: EF/DG		CIP NO. 6004		DRAWING NO. DWG 536-6	
CHKD BY: DD					
RVWD BY: LT					

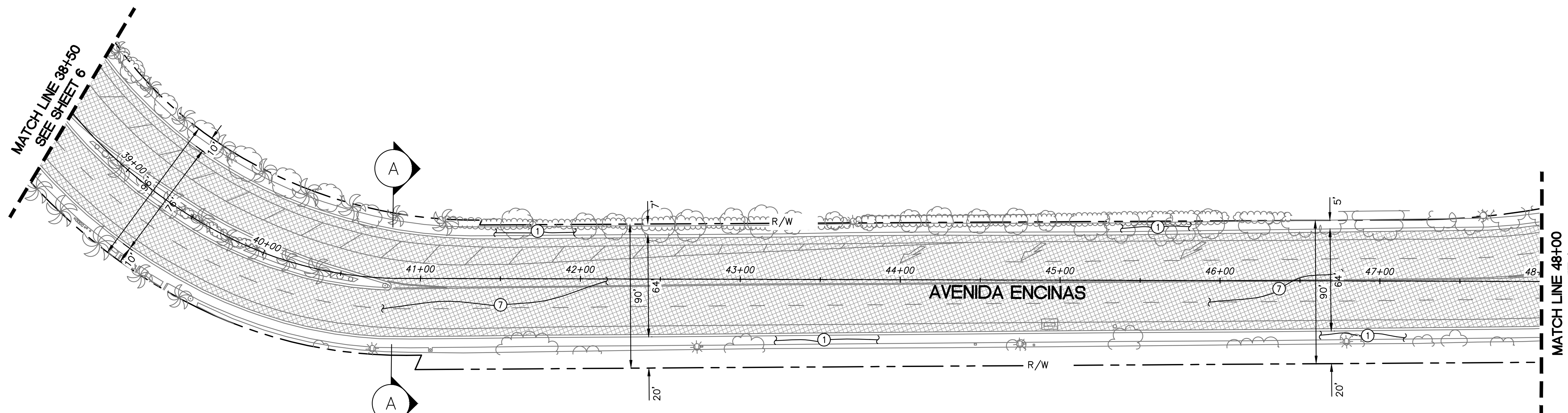


TYPICAL AVENIDA ENCINAS CROSS-SECTION (A-A)

NO SCALE
STA: 38+50 - STA: 48+00

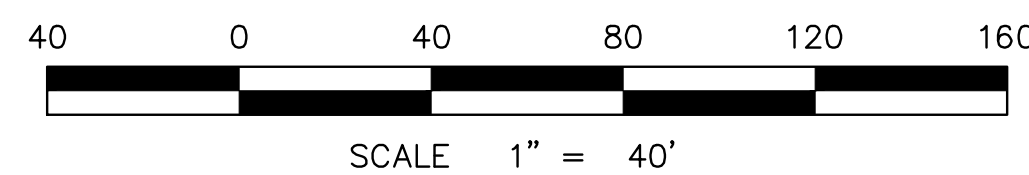
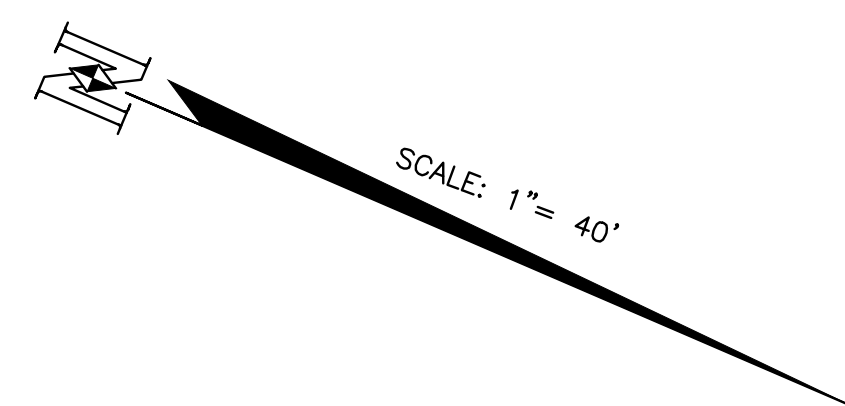
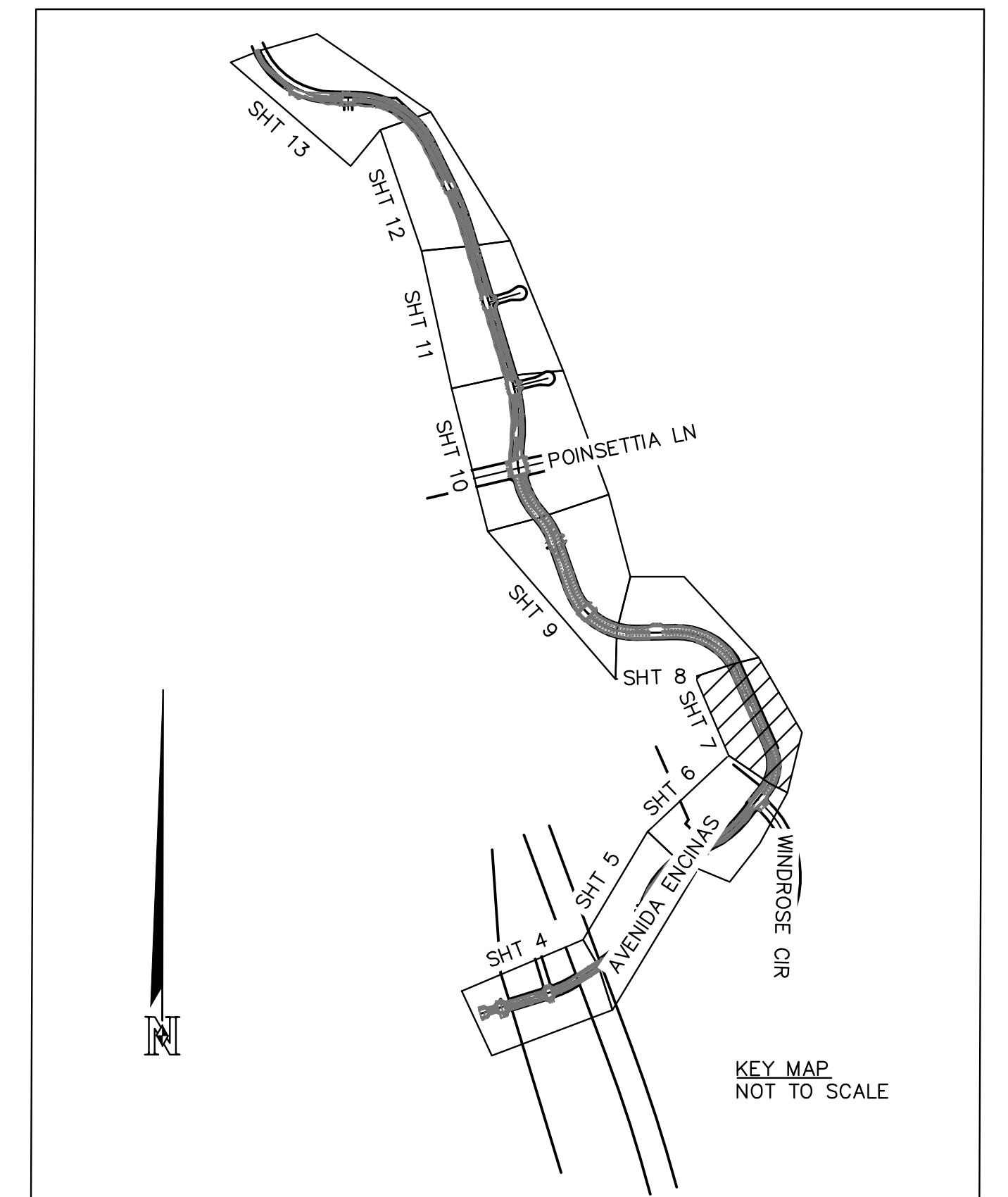
CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ⑦ 2" GRIND AND OVERLAY.



PLAN: AVENIDA ENCINA

SCALE: 1"=40'

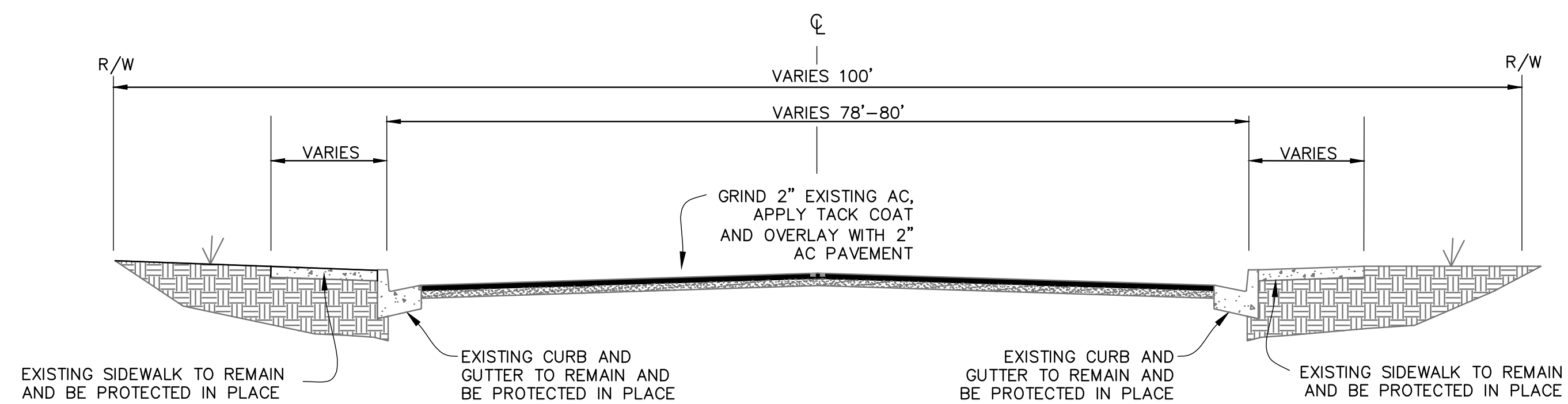


JOB NO.: 121-108.1

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SHEET 7		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 38+50 TO 48+00					
APPROVED:				HOSSEIN AJIDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE	INITIAL	DATE	INITIAL	CIP NO.	DRAWING NO.
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL	6004	DWG 536-6
REVISION DESCRIPTION					

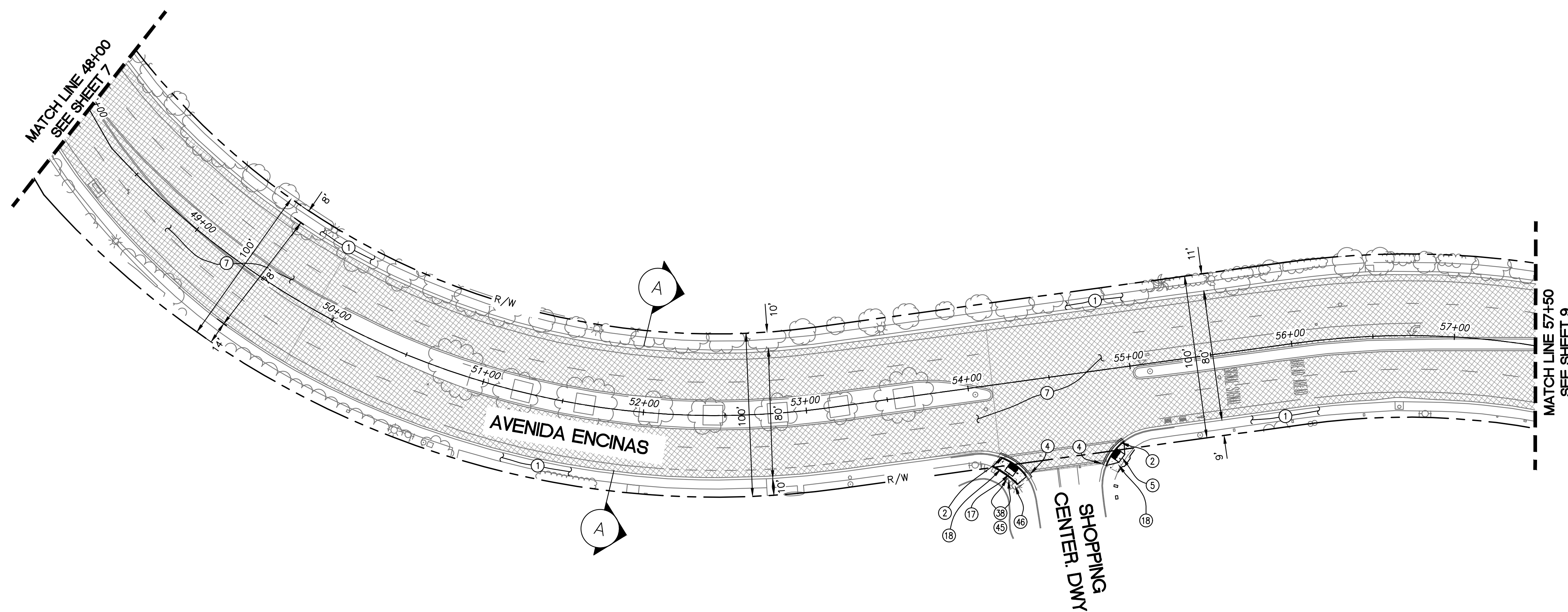


TYPICAL AVENIDA ENCINAS CROSS-SECTION (A-A)

NO SCALE
STA: 48+00 - STA: 57+50

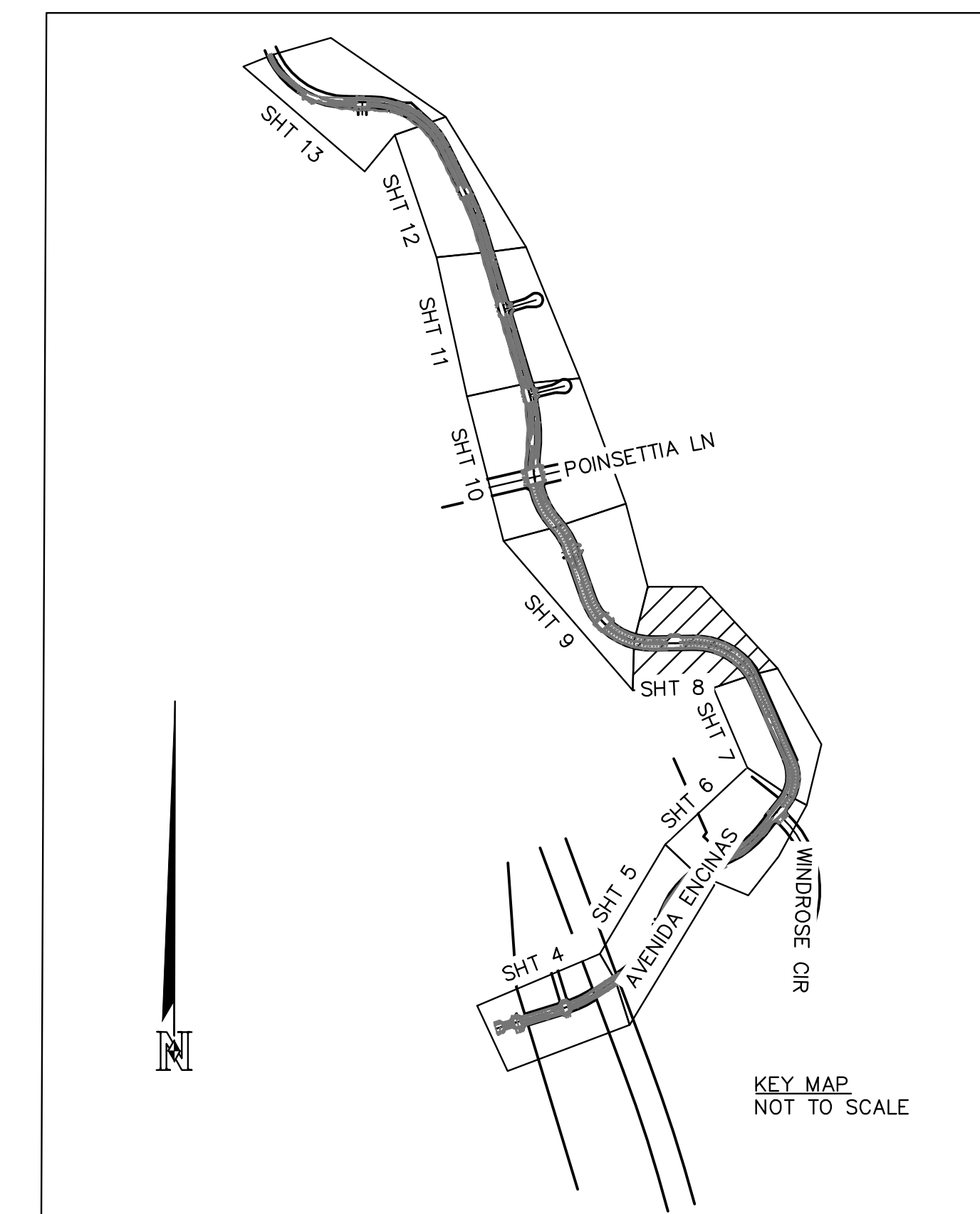
CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑤ REMOVE AND REPLACE WITH CASE D CURB RAMP PER CALTRANS CRD A88A.
- ⑦ 2" GRIND AND OVERLAY.
- ⑦ REMOVE AND REPLACE WITH CASE C CURB RAMP PER CALTRANS CRD A88A.
- ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑳ EXISTING COMMUNICATION RISER TO REMAIN AND BE PROTECTED IN PLACE.
- ④⑤ EXISTING PULL BOX TO REMAIN AND BE PROTECTED IN PLACE.
- ④⑥ EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.

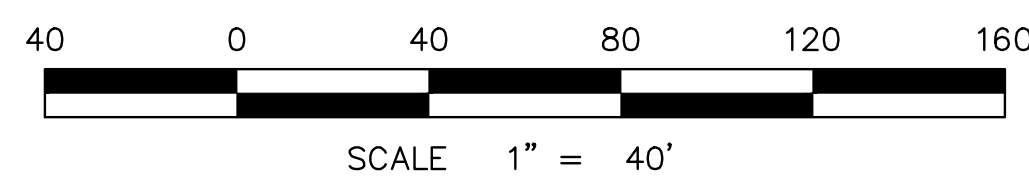
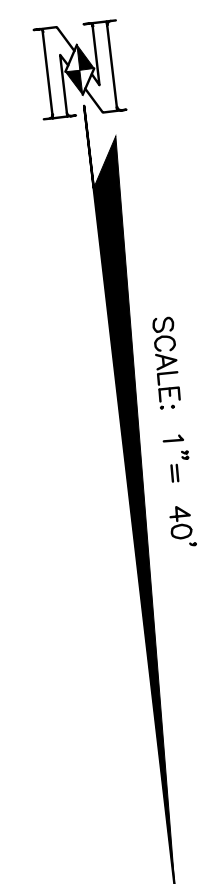


PLAN: AVENIDA ENCINA

SCALE: 1" = 40'



KEY MAP
NOT TO SCALE

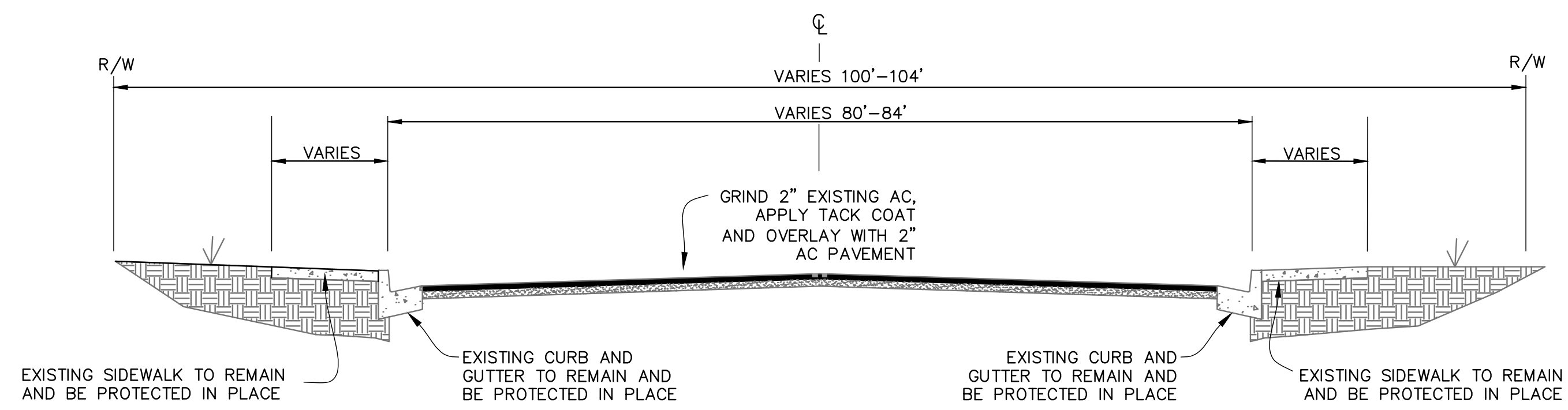


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SHEET 8		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 48+00 TO 57+50					
APPROVED:				HOSSEIN AJIDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DD		6004		DWG 536-6	
RVWD BY: LT					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL		CITY APPROVAL	
REVISION DESCRIPTION					

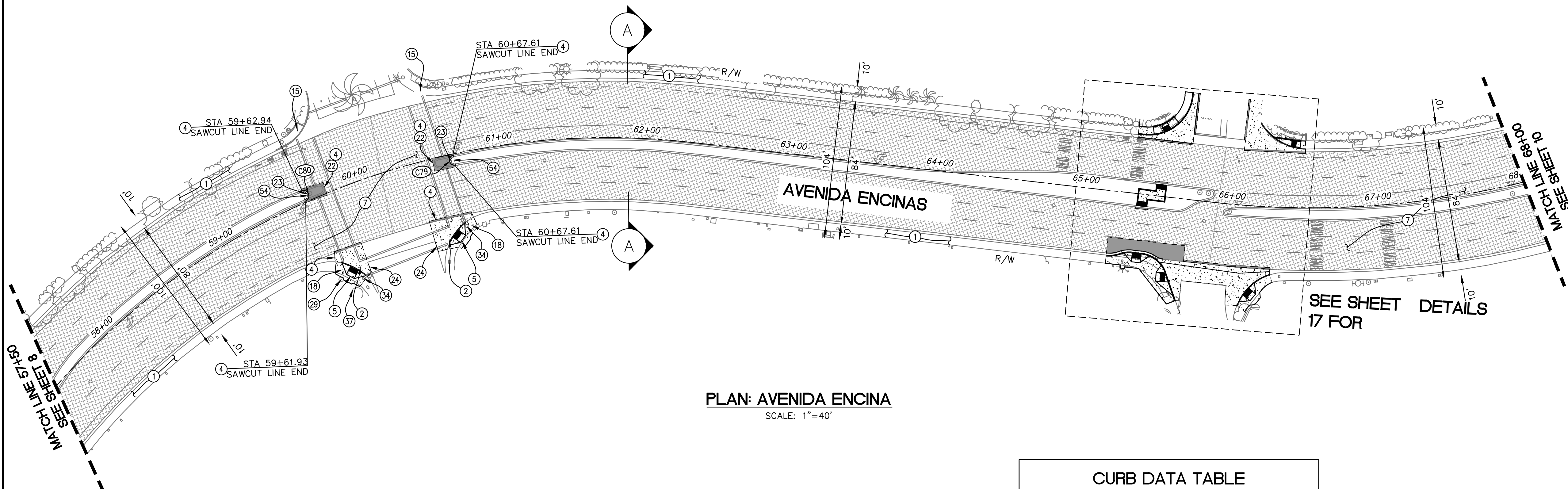


TYPICAL AVENIDA ENCINAS CROSS-SECTION (A-A)

NO SCALE
STA: 57+50 - STA: 68+00

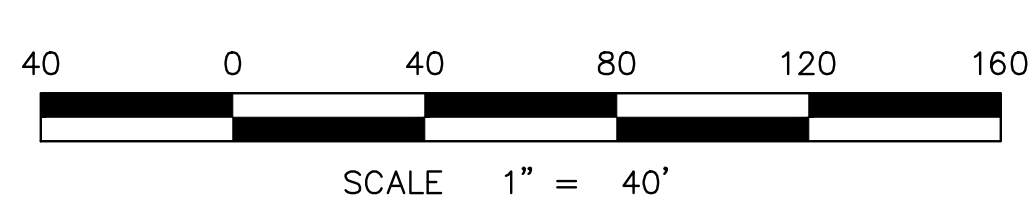
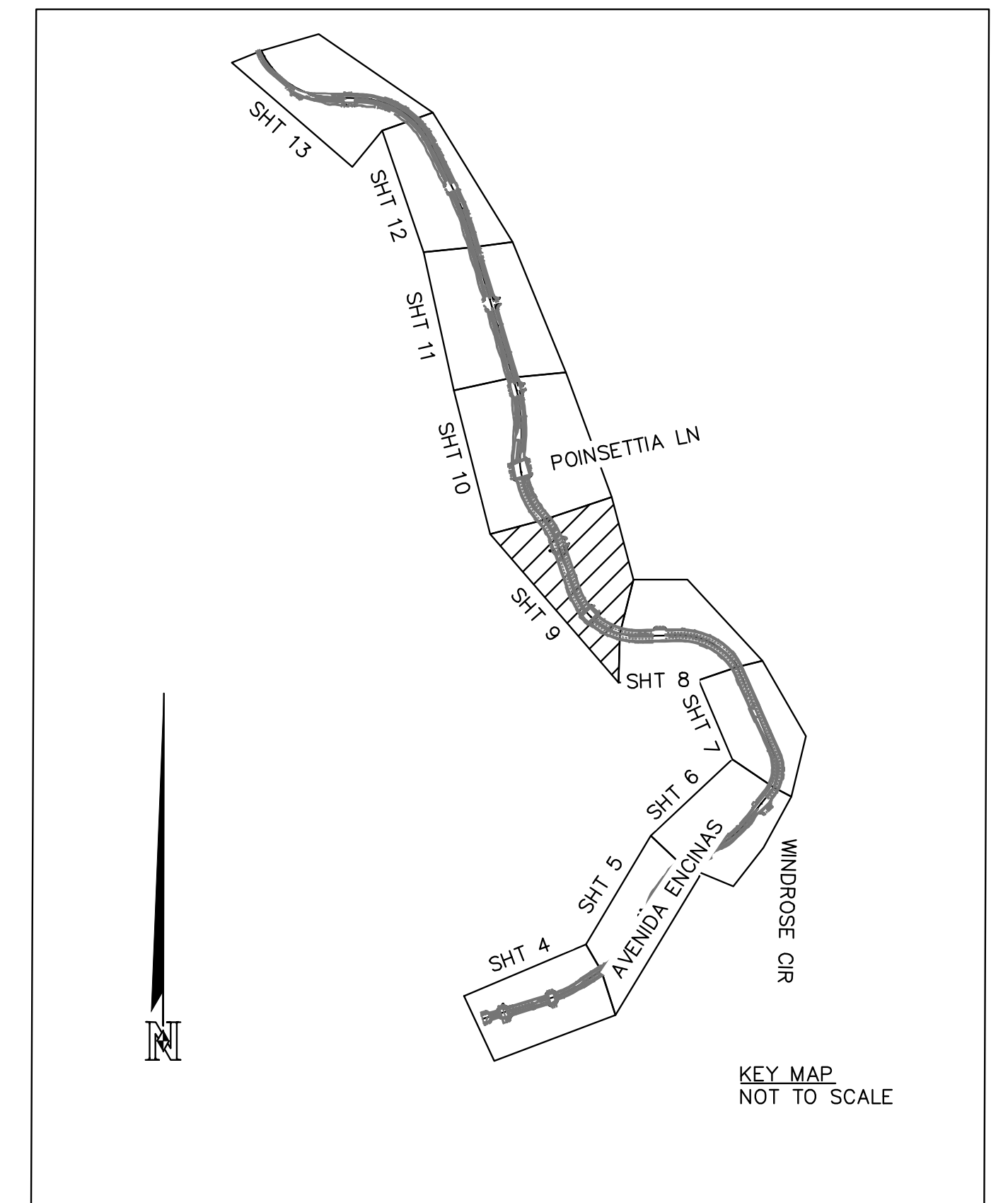
CONSTRUCTION NOTES

- 1 EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- 2 INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- 4 PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- 5 REMOVE AND REPLACE WITH CASE D CURB RAMP PER CALTRANS CRD A88A.
- 7 2" GRIND AND OVERLAY.
- 15 EXISTING CURB RAMP TO REMAIN AND BE PROTECTED IN PLACE.
- 18 INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- 22 INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
- 23 INSTALL TYPE B-2 MEDIAN CURB AND GUTTER PER SDRSD G-6.
- 24 EXISTING DECORATIVE PAVEMENT TO REMAIN AND BE PROTECTED IN PLACE.
- 29 EXISTING STREET SIGN TO BE REMOVED AND RELOCATED AS NEEDED.
- 34 EXISTING TRAFFIC SIGNAL TO REMAIN AND BE PROTECTED IN PLACE.
- 37 REMOVE CONFLICTING MOW CURB AS NEEDED.
- 54 REMOVE AND REPLACE MEDIAN DECORATIVE PAVINGS.



PLAN: AVENIDA ENCINA
SCALE: 1"=40'

#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C79	10.88'	178°07'40.1"	3.50'	
C80	11.52'	174°24'36.9"	3.78'	

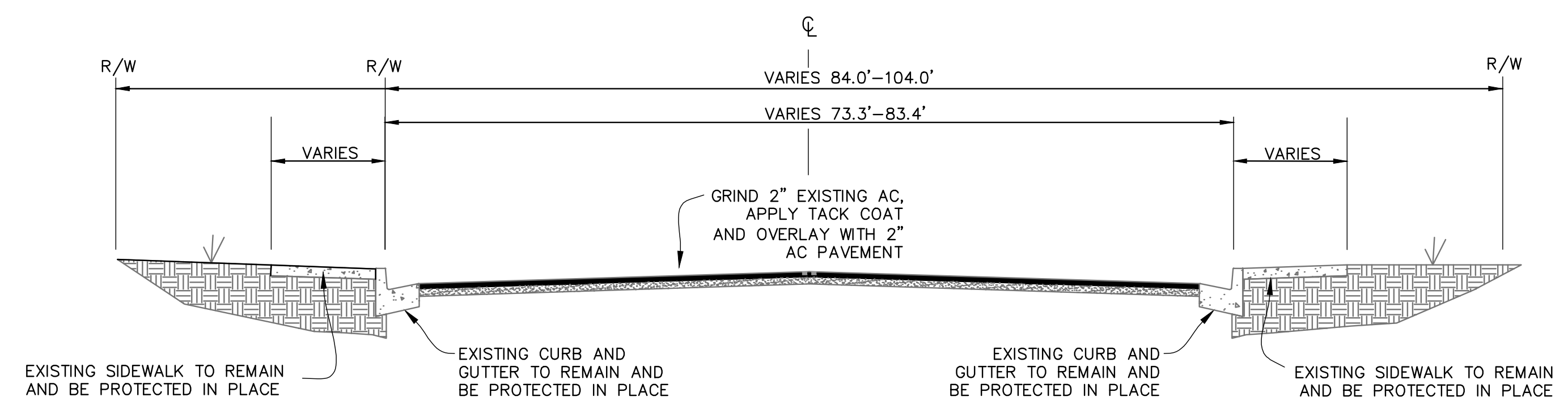


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SHEET 9		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 57+50 TO 68+00					
APPROVED: _____				HOSSEIN AJIDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL		CITY APPROVAL	
REVISION DESCRIPTION					
DWN BY: EF/DG		CIP NO. 6004		DRAWING NO. DWG 536-6	
CHKD BY: DD					
RVWD BY: LT					

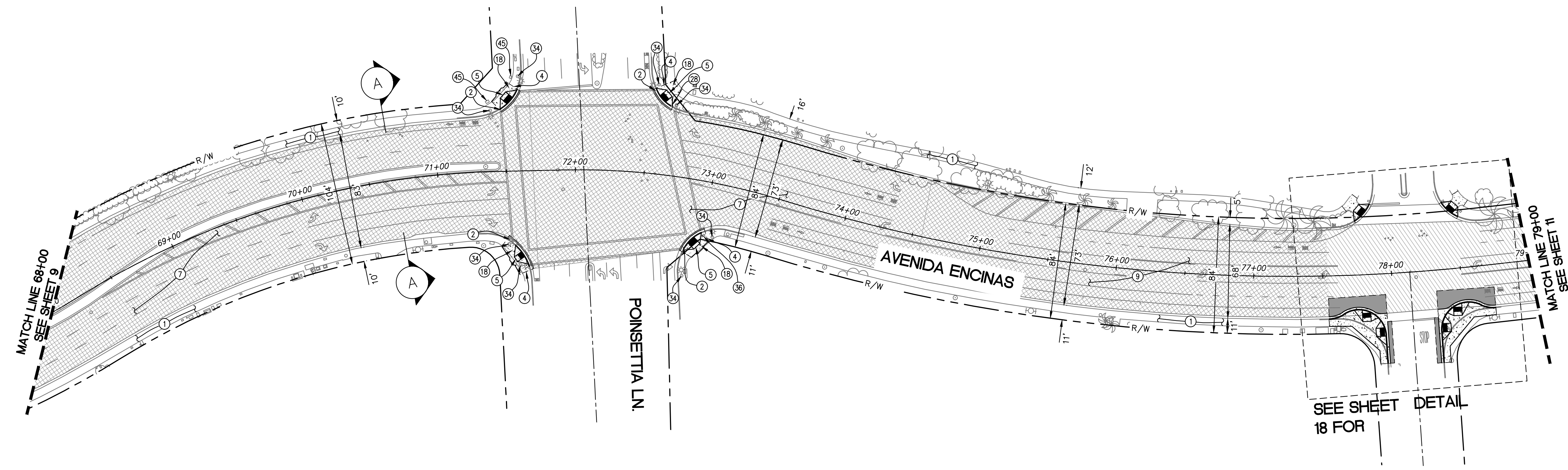


TYPICAL AVENIDA ENCINAS CROSS-SECTION A-A

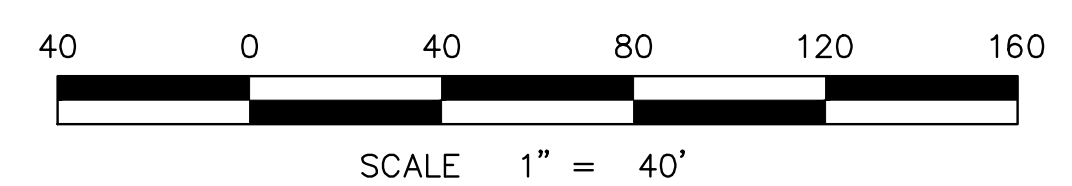
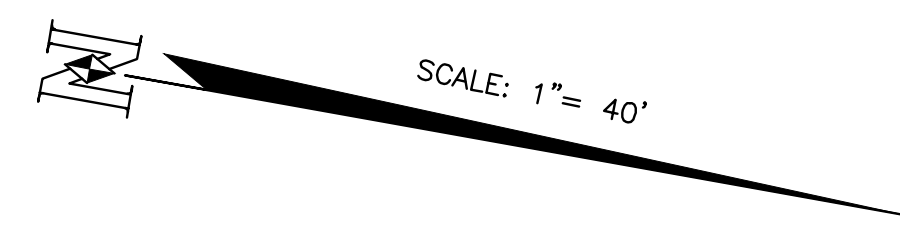
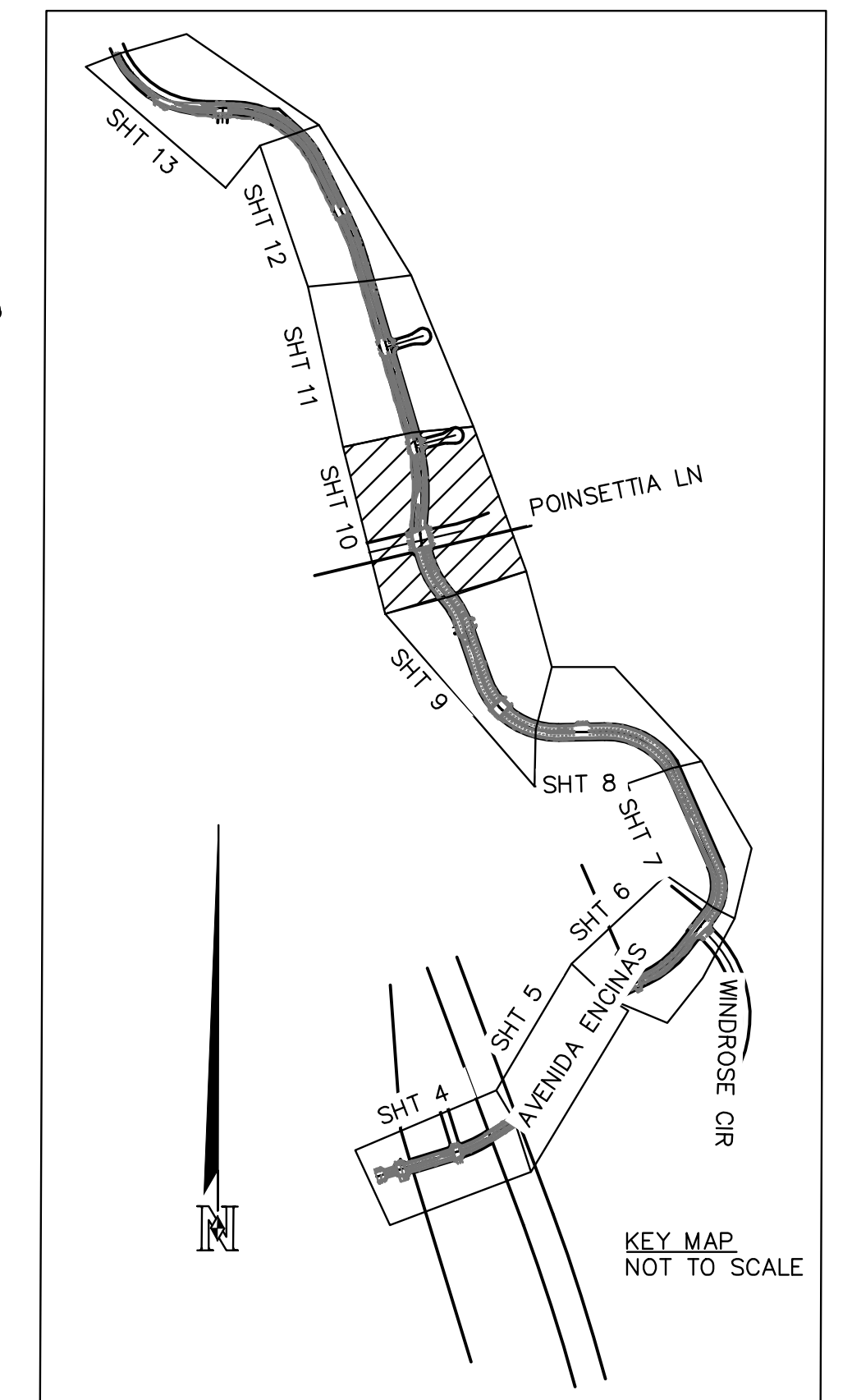
NO SCALE
STA: 68+00 - STA: 77+53.46

CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑤ REMOVE AND REPLACE WITH CASE D CURB RAMP PER CALTRANS CRD A88A.
- ⑦ 2" GRIND AND OVERLAY.
- ⑨ SLURRY SEAL EXISTING ROADWAY
- ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑳ EXISTING LANDSCAPING AND IRRIGATION IN CONFLICT TO BE REMOVED AS NEEDED.
- ⑳ EXISTING TRAFFIC SIGNAL TO REMAIN AND BE PROTECTED IN PLACE.
- ㉔ EXISTING PULL BOX TO BE ADJUSTED TO GRADE AS NEEDED.
- ㉕ EXISTING PULL BOX TO REMAIN AND BE PROTECTED IN PLACE.



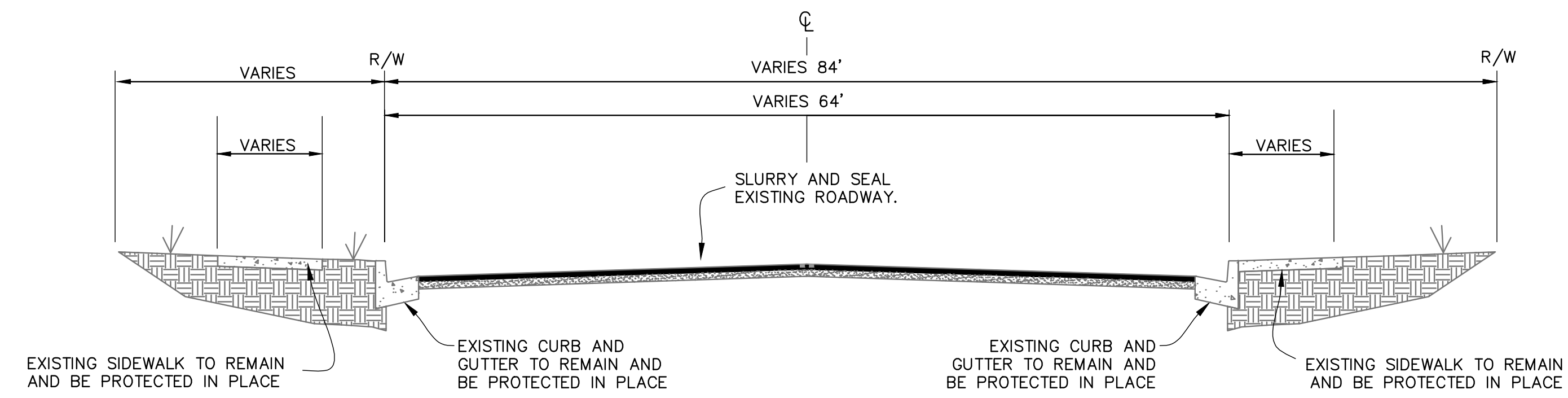
PLAN: AVENIDA ENCINA
SCALE: 1"=40'



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SHEET 10		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 68+00 TO 79+00					
APPROVED:				HOSSEIN AJIDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE	INITIAL	DATE	INITIAL	CIP NO.	DRAWING NO.
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL	6004	DWG 536-6
REVISION DESCRIPTION					

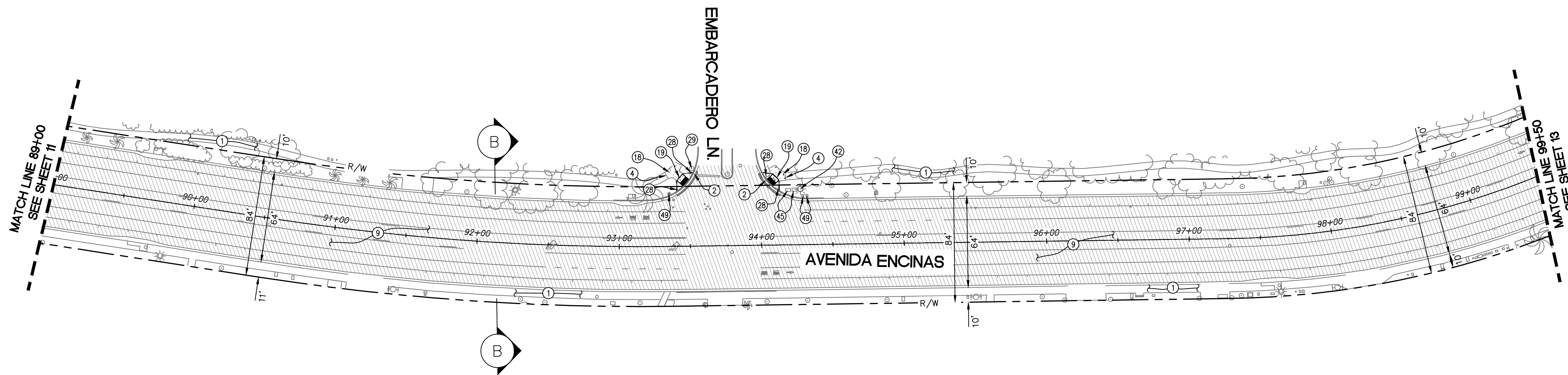


TYPICAL AVENIDA ENCINAS CROSS-SECTION

NO SCALE
STA: 79+00 - STA: 89+00

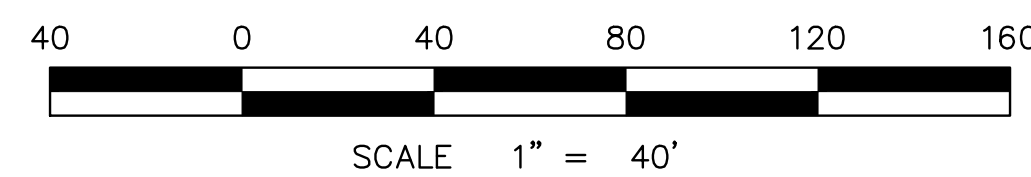
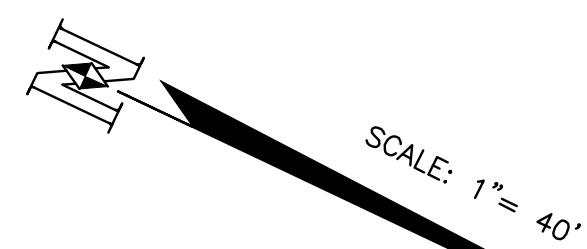
CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑨ SLURRY SEAL EXISTING ROADWAY
- ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑲ REMOVE AND REPLACE WITH CASE A CURB RAMP PER CALTRANS CRD AB8A.
- ⑳ EXISTING LANDSCAPING AND IRRIGATION IN CONFLICT TO BE REMOVED AS NEEDED.
- ㉑ EXISTING STREET SIGN TO BE REMOVED AND RELOCATED AS NEEDED.
- ㉒ EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉓ EXISTING PULL BOX TO REMAIN AND BE PROTECTED IN PLACE.
- ㉔ EXISTING UTILITY TO REMAIN AND BE PROTECTED IN PLACE.



PLAN: AVENIDA ENCINA

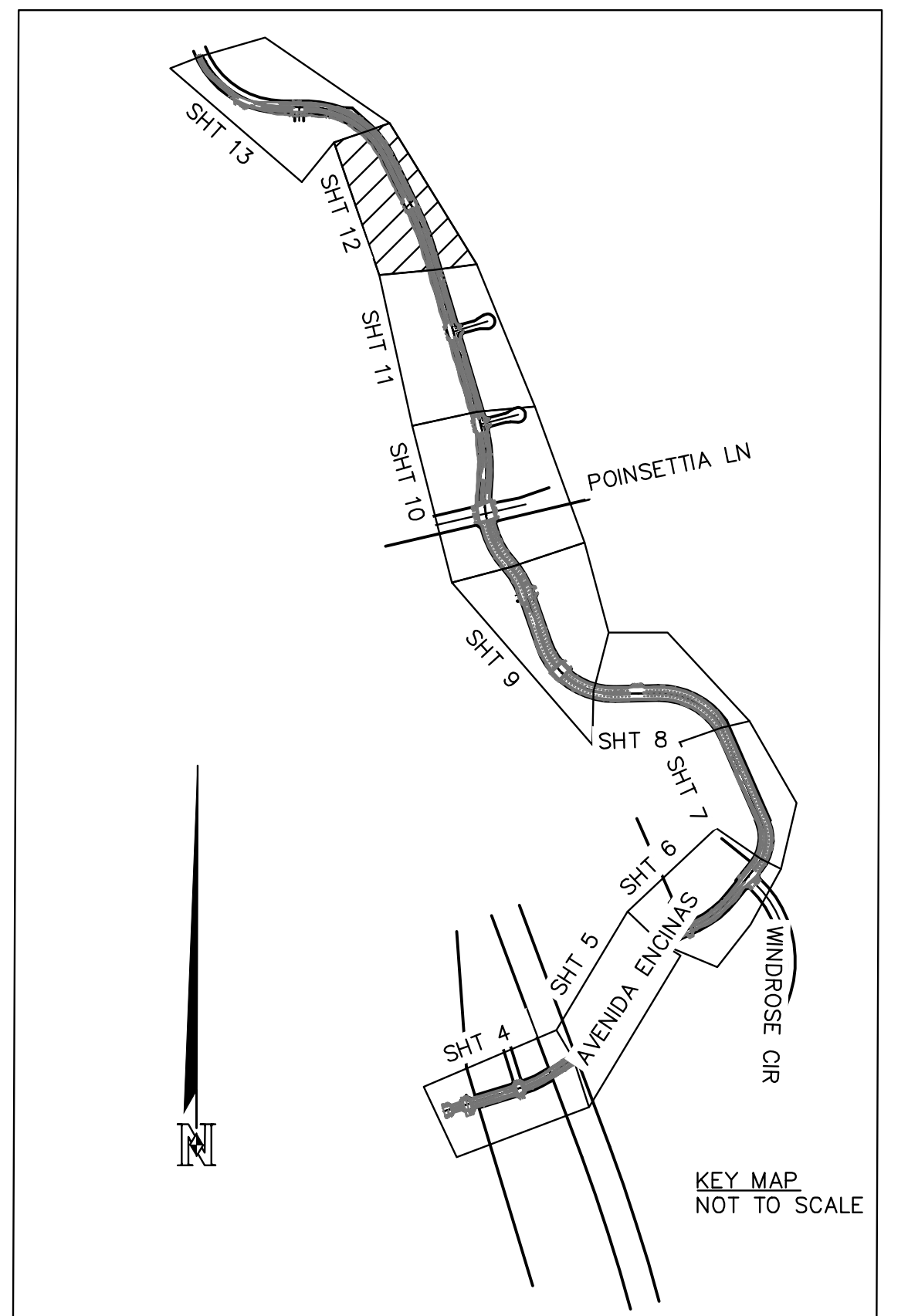
SCALE: 1"=40'



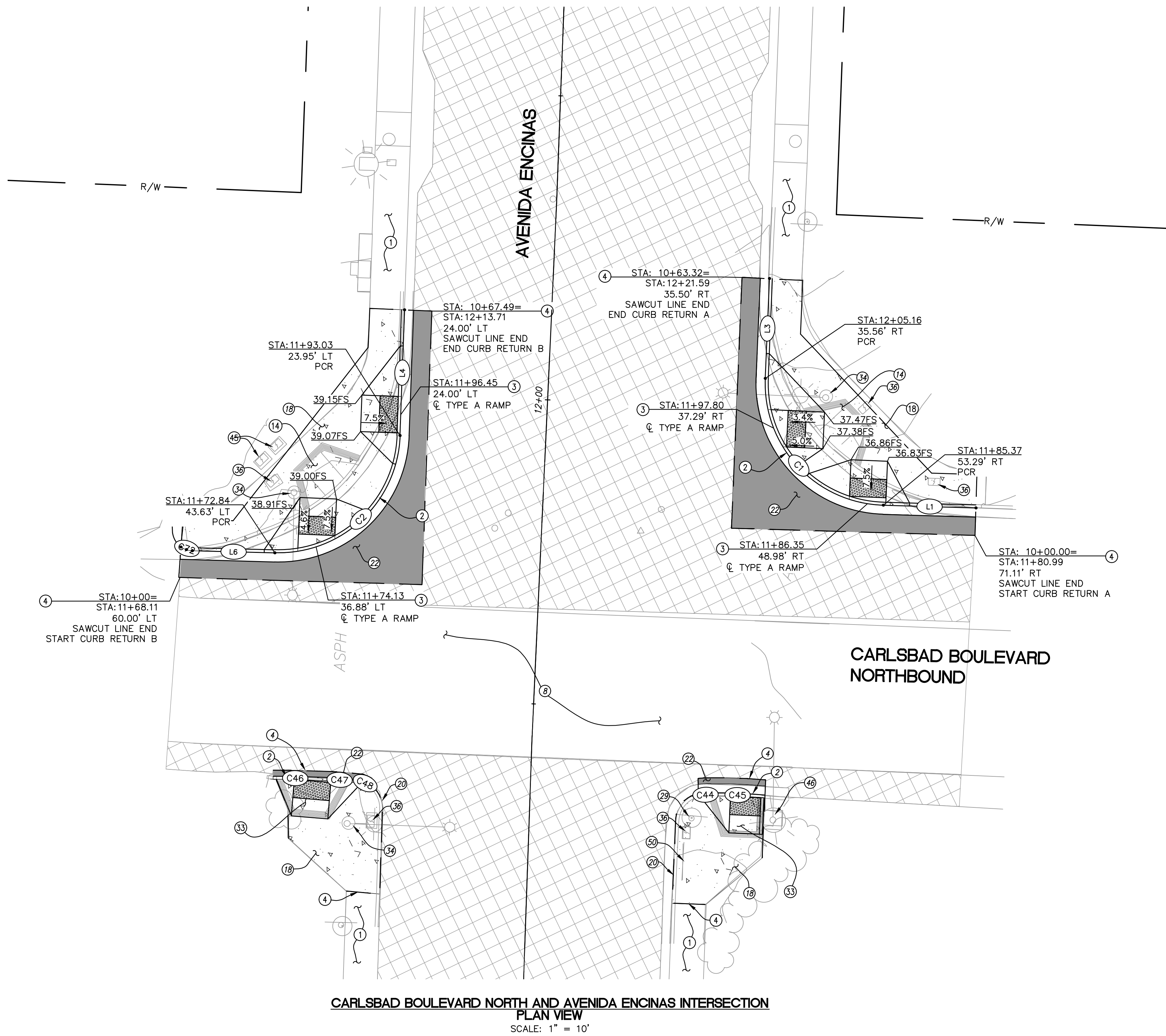
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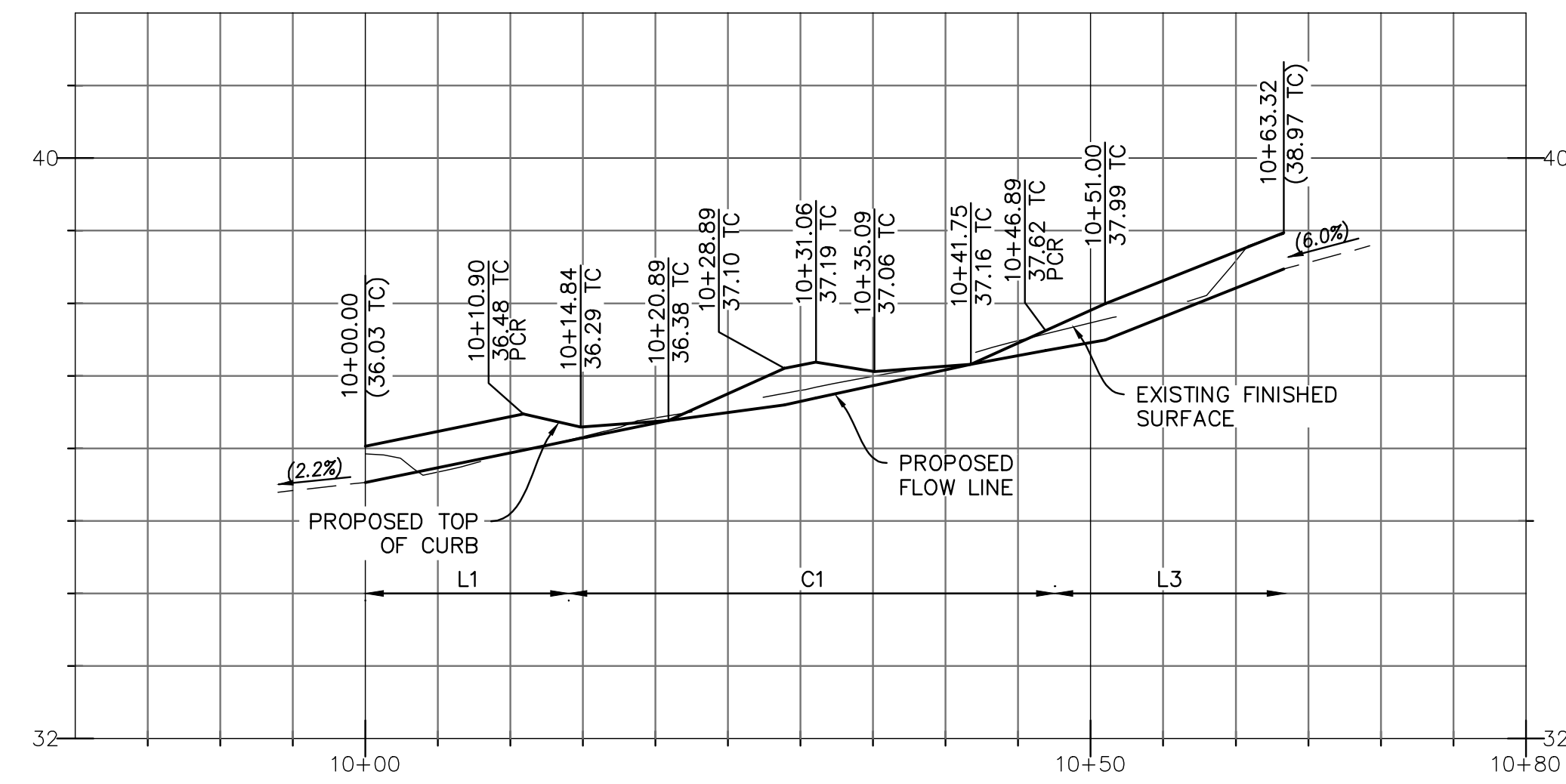
SHEET 12		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
IMPROVEMENT PLAN FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS STA. 89+00 TO 99+50					
APPROVED:				HOSSEIN AJUDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL		CITY APPROVAL	
REVISION DESCRIPTION					
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DD		6004		DWG 536-6	
RVWD BY: LT					



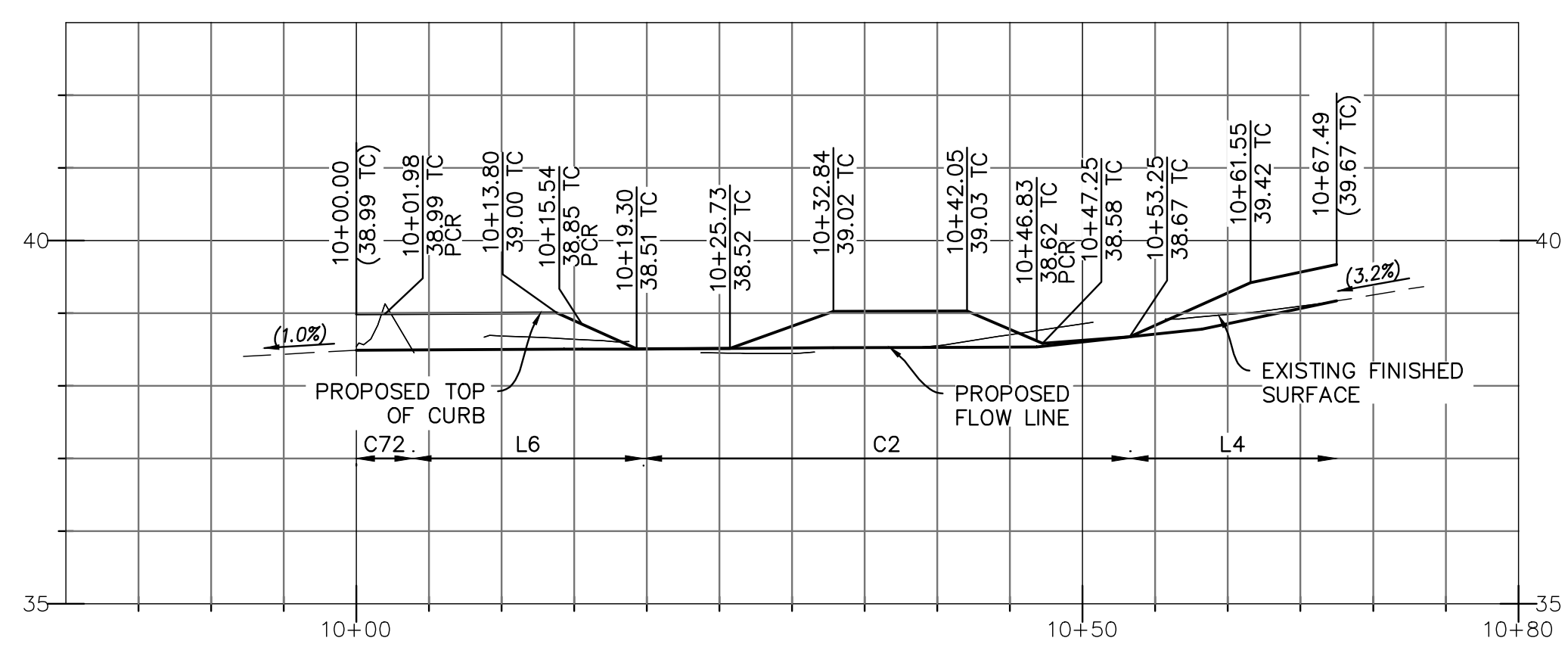
**CARLSBAD BOULEVARD NORTH AND AVENIDA ENCINAS INTERSECTION
PLAN VIEW**
SCALE: 1" = 10'

CURB DATA TABLE				
#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C1	31.66'	90°42'42.0"	20.00'	6" CURB AND GUTTER
C2	31.29'	89°37'54.6"	20.00'	6" CURB AND GUTTER
C44	1.80'	6°48'40.6"	15.13'	6" CURB AND GUTTER
C45	8.73'	6°23'33.5"	78.22'	6" CURB AND GUTTER
C46	7.22'	3°31'45.2"	117.19'	6" CURB AND GUTTER
C47	7.36'	6°49'40.9"	61.80'	6" CURB AND GUTTER
C48	0.97'	12°54'46.2"	4.30'	6" CURB AND GUTTER
C72	1.98'	28°22'11.4"	4.00'	6" CURB AND GUTTER
L1	15.23	N17°30'57"W		6" CURB AND GUTTER
L3	16.43	N73°11'45"E		6" CURB AND GUTTER
L4	20.66	S72°51'08"W		6" CURB AND GUTTER
L6	13.56	N17°46'38"W		6" CURB AND GUTTER

- CONSTRUCTION NOTES**
- EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
 - INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
 - CONSTRUCT CASE A CURB RAMP PER CALTRANS STANDARD PLAN A88A.
 - PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
 - EXISTING PCC TO REMAIN AND BE PROTECTED IN PLACE.
 - EXISTING CURB RAMP TO BE REMOVED.
 - INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
 - EXISTING CURB AND GUTTER TO REMAIN AND BE PROTECTED IN PLACE.
 - INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
 - EXISTING STREET SIGN TO BE REMOVED AND RELOCATED AS NEEDED.
 - REMOVE AND REPLACE WITH CASE G CURB RAMP PER CALTRANS CRD A88A.
 - EXISTING TRAFFIC SIGNAL TO REMAIN AND BE PROTECTED IN PLACE.
 - EXISTING PULL BOX TO BE ADJUSTED TO GRADE AS NEEDED.
 - EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.
 - EXISTING PEDESTRIAN BARRICADE TO REMAIN AND BE PROTECTED IN PLACE.



CURB RETURN A
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



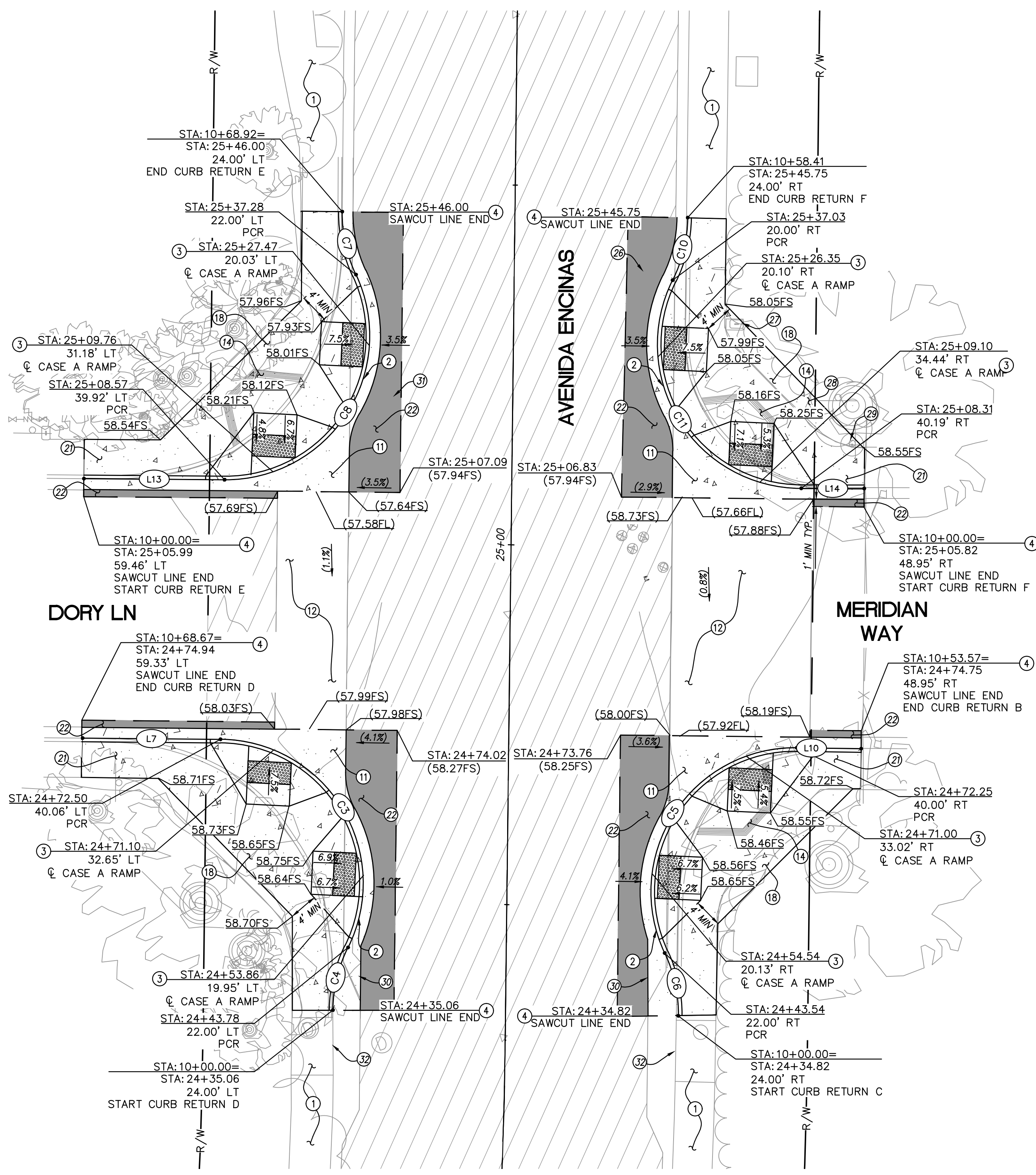
CURB RETURN B
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



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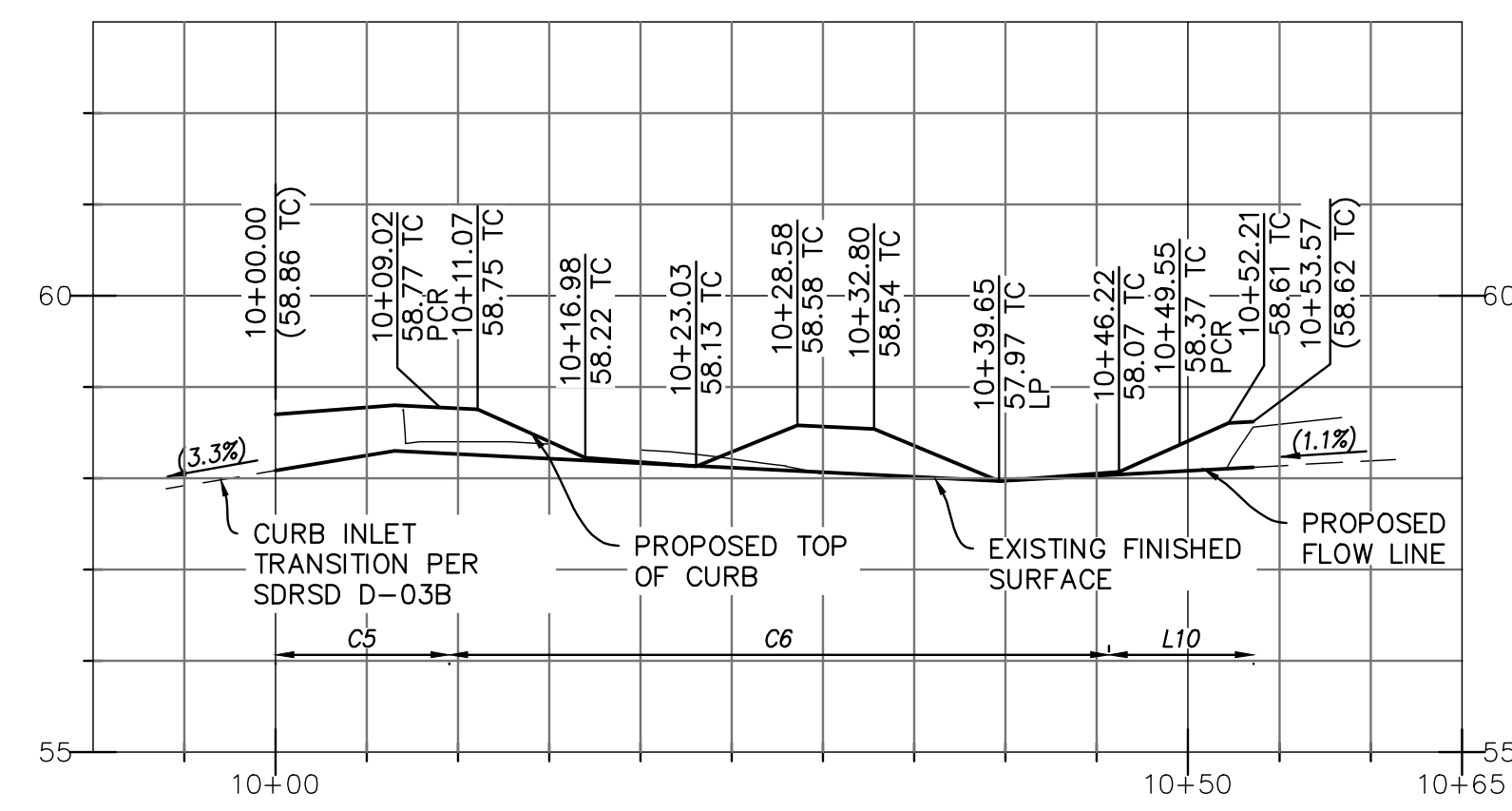
SHEET 14		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
DETAILS FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: _____				HOSSEIN AJIDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE: _____		DATE: _____		DATE: _____	
INITIAL: _____		INITIAL: _____		INITIAL: _____	
ENGINEER OF WORK		REVISION DESCRIPTION		OTHER APPROVAL	
CITY APPROVAL		CITY APPROVAL		CITY APPROVAL	
DWN BY: EF/DG		CIP NO. 6004		DRAWING NO. DWG 536-6	
CHKD BY: DD					
RVWD BY: LT					



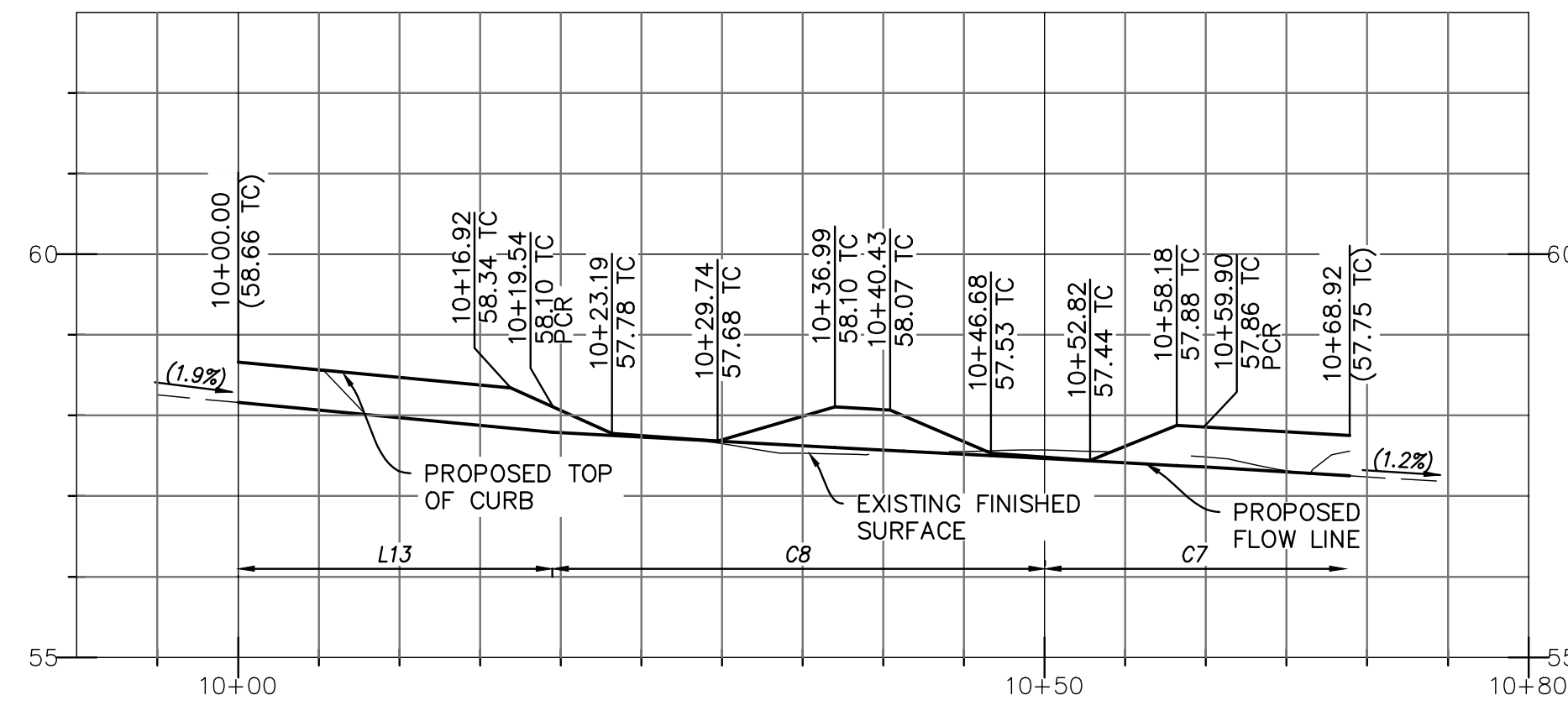
**DORIAN LN AND MERIDIAN WAY INTERSECTION
PLAN VIEW**
SCALE: 1" = 10'

CURB DATA TABLE				
#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C3	40.50'	116°00'34.7"	20.00'	6" CURB AND GUTTER
C4	9.02'	25°50'31.0"	20.00'	6" CURB AND GUTTER
C5	40.44'	115°51'19.4"	20.00'	6" CURB AND GUTTER
C6	9.02'	25°50'31.0"	20.00'	6" CURB AND GUTTER
C7	9.02'	25°50'31.0"	20.00'	6" CURB AND GUTTER
C8	40.36'	115°37'07.0"	20.00'	6" CURB AND GUTTER
C10	9.02'	25°50'31.0"	20.00'	6" CURB AND GUTTER
C11	40.63'	116°23'45.2"	20.00'	6" CURB AND GUTTER
L7	19.15	S68°42'02"E		6" CURB AND GUTTER
L10	4.10	N68°31'10"W		6" CURB AND GUTTER
L13	19.54	N68°45'22"W		6" CURB AND GUTTER
L14	8.75	S69°06'30"E		6" CURB AND GUTTER

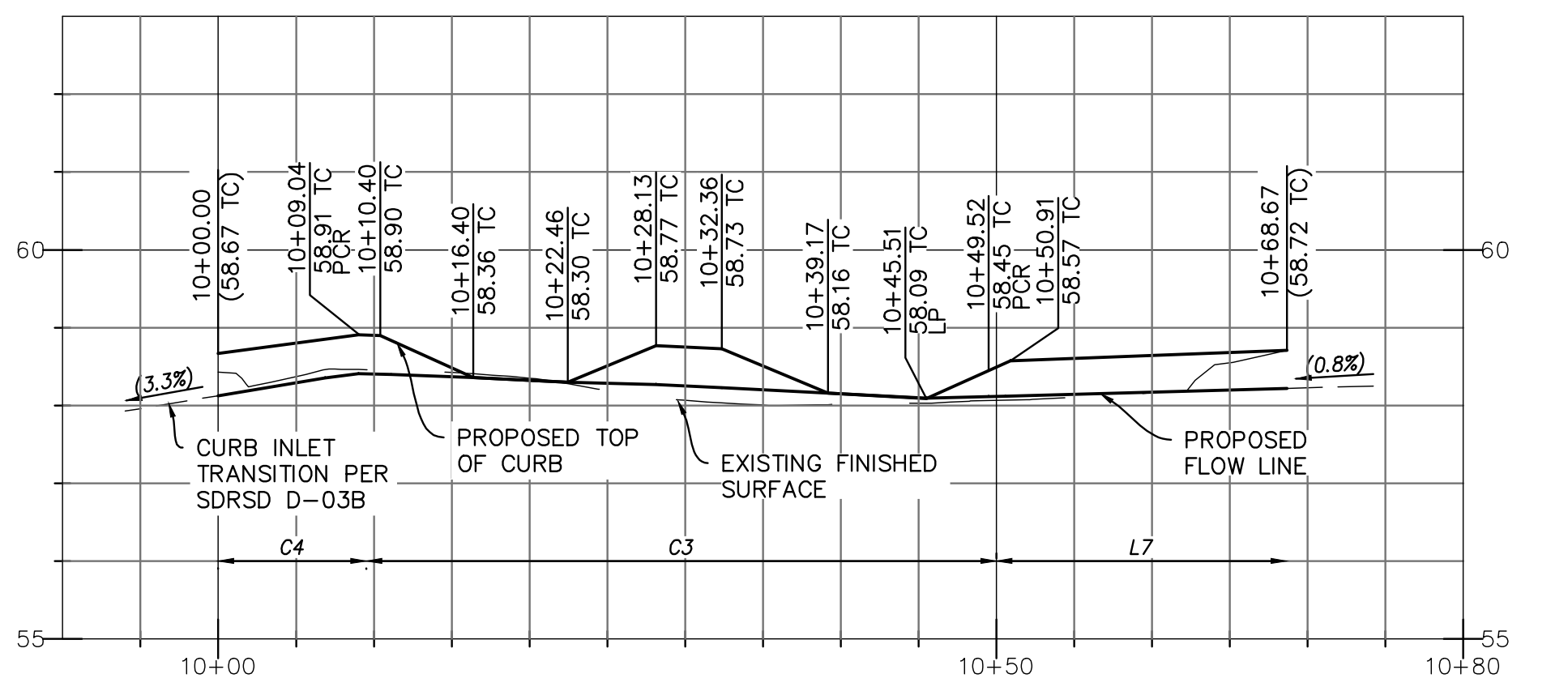
- CONSTRUCTION NOTES**
- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
 - ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
 - ③ CONSTRUCT CASE A CURB RAMP PER CALTRANS STANDARD PLAN A88A.
 - ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
 - ⑪ REMOVE AND REPLACE CROSS GUTTER PER SDRSD G-12.
 - ⑫ EXISTING CROSS GUTTER TO REMAIN AND BE PROTECTED IN PLACE.
 - ⑭ EXISTING CURB RAMP TO BE REMOVED.
 - ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. SAWCUT, REMOVE AND REPLACE PAVEMENT SECTIONS.
 - ⑳ SAWCUT AND REMOVE EXISTING CURB, GUTTER, SIDEWALK, AND PAVEMENT SECTION CONFLICTING WITH PROPOSED IMPROVEMENTS.
 - ㉑ INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
 - ㉒ EXISTING WATER VALVE TO BE ADJUSTED TO GRADE AS NEEDED.
 - ㉓ EXISTING WATER METER TO BE ADJUSTED TO GRADE AS NEEDED.
 - ㉔ EXISTING LANDSCAPING AND IRRIGATION IN CONFLICT TO BE REMOVED AS NEEDED.
 - ㉕ EXISTING STREET SIGN TO BE REMOVED AND RELOCATED AS NEEDED.
 - ㉖ PROPOSED INLET TRANSITION PER SDRSD D-03B.
 - ㉗ EXISTING SEWER MANHOLE TO BE ADJUSTED TO GRADE AS NEEDED.
 - ㉘ EXISTING CURB INLET TO REMAIN AND BE PROTECTED IN PLACE.



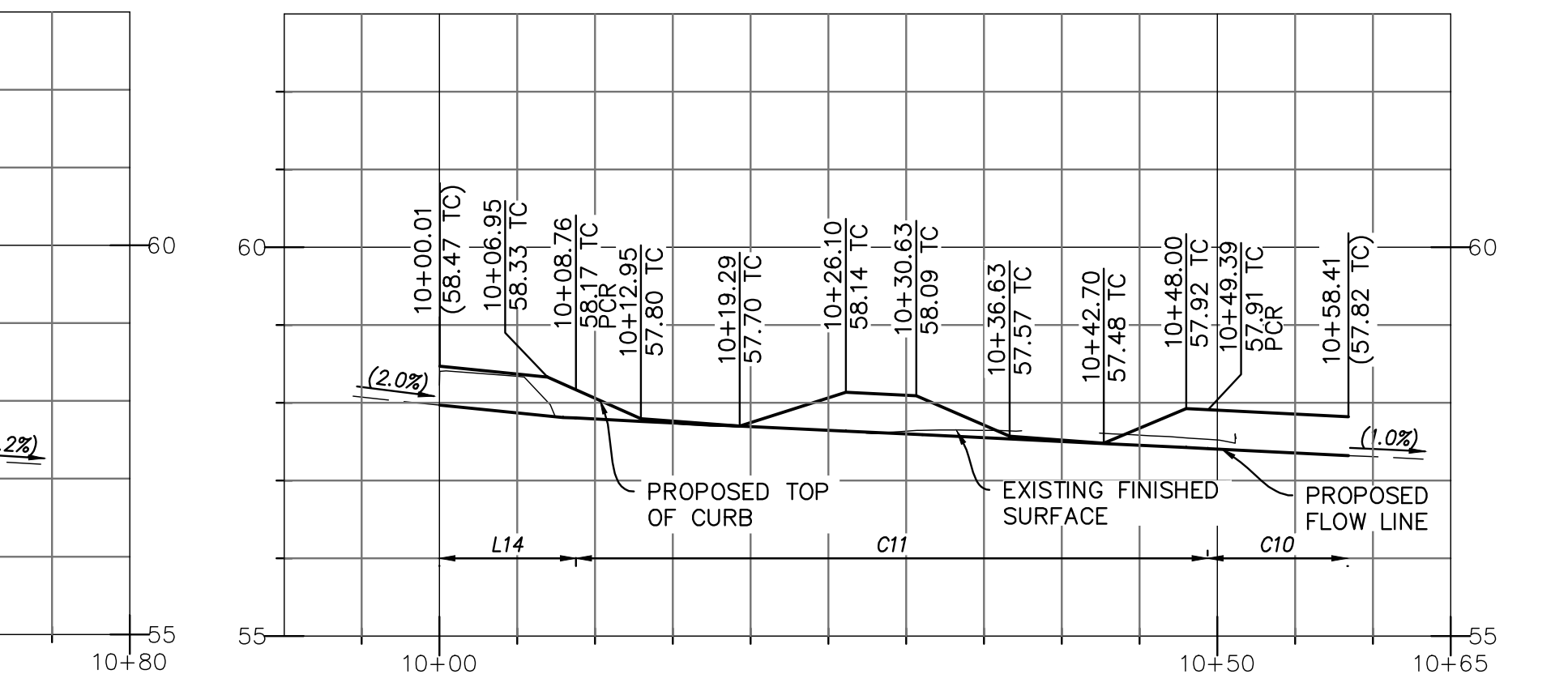
CURB RETURN C
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



CURB RETURN E
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



CURB RETURN D
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



CURB RETURN F
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'

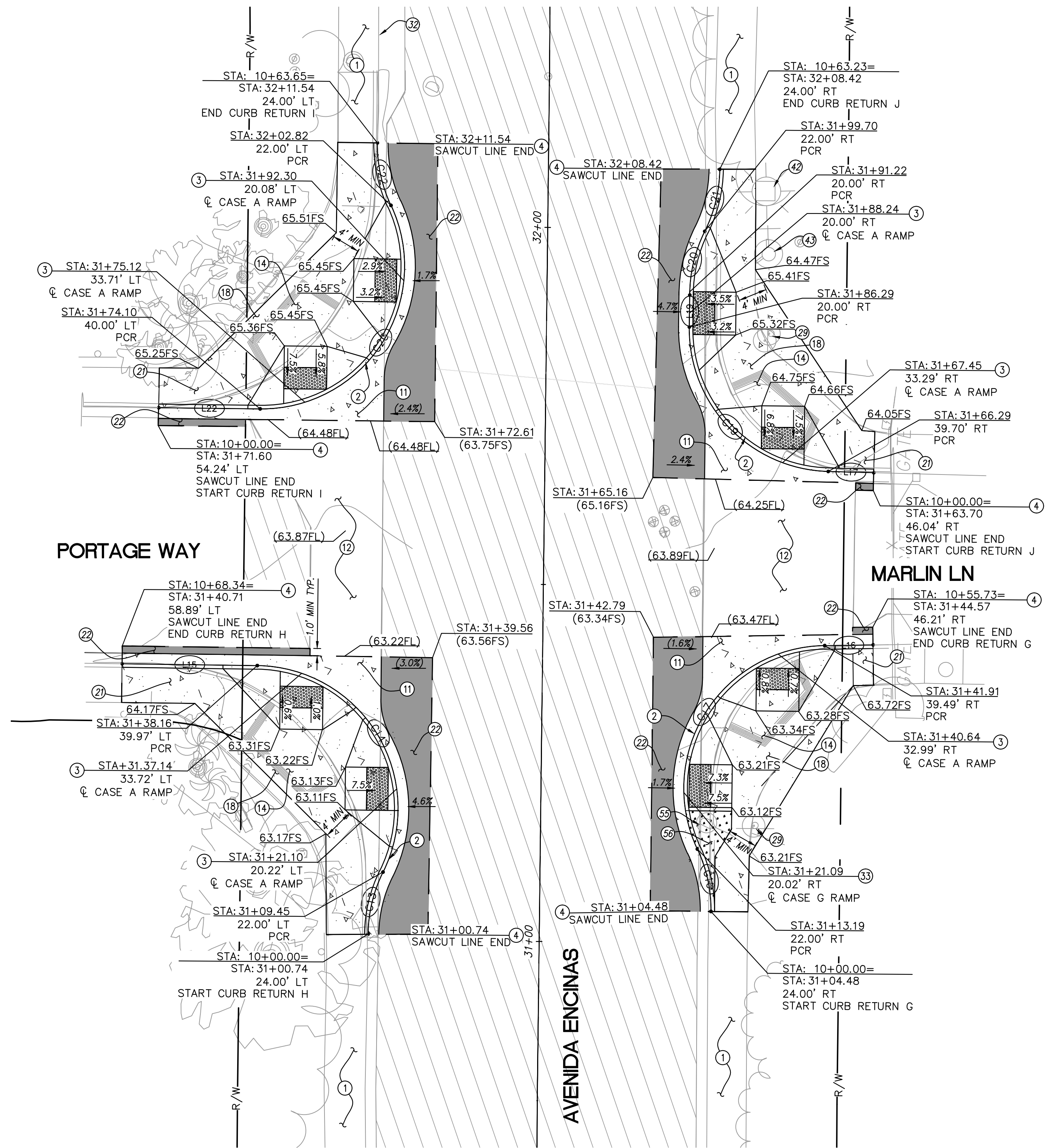


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SHEET 15		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
DETAILS FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: _____			HOSSEIN AJUDEH		
ENGINEERING MANAGER			RCE 75991 EXP.6/20/24 DATE		
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DD		6004		DWG 536-6	
RVWD BY: LT					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		REVISION DESCRIPTION		OTHER APPROVAL	
				CITY APPROVAL	

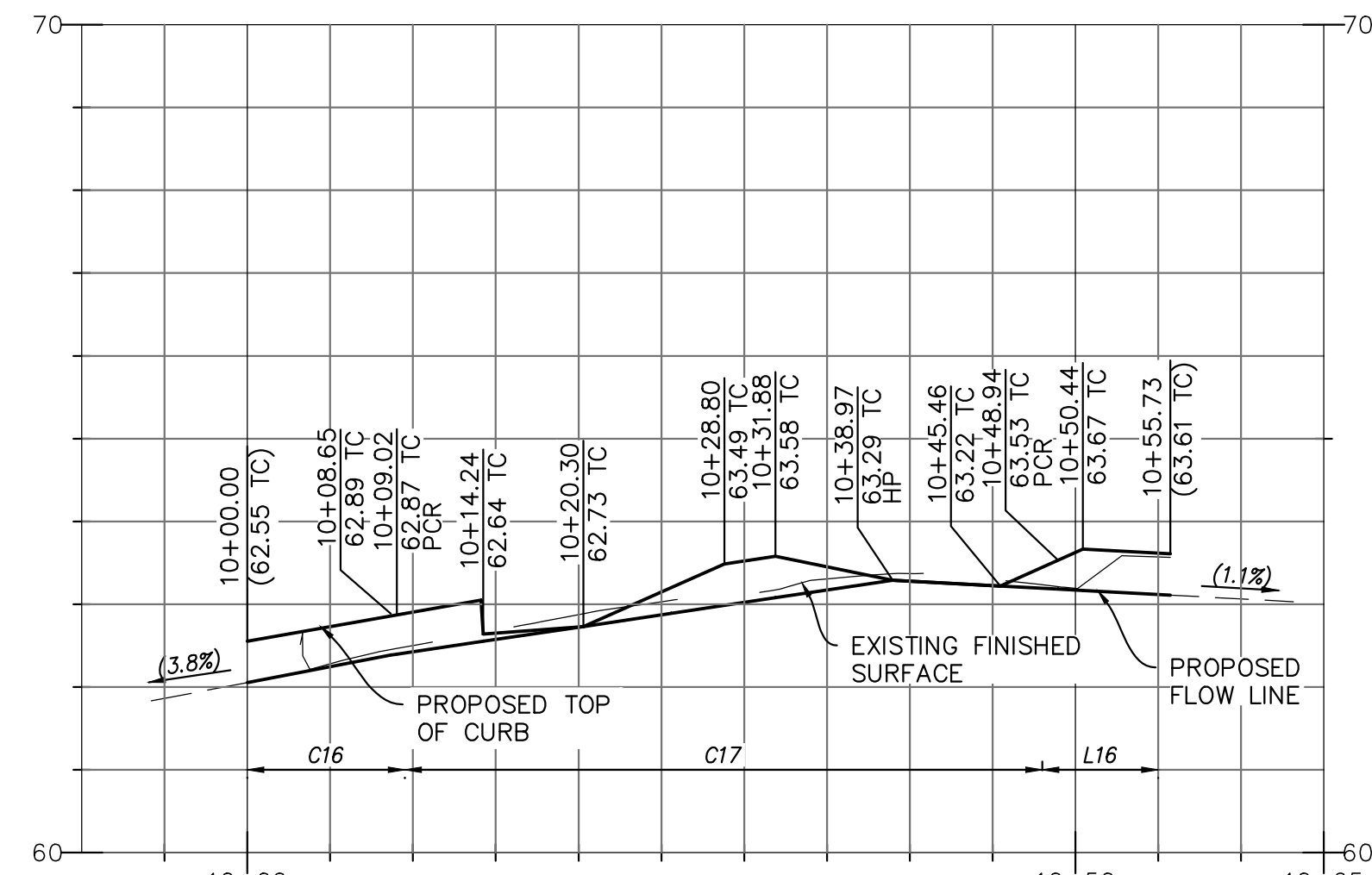


PORTAGE WAY AND MARLIN LANE INTERSECTION
PLAN VIEW
SCALE: 1" = 10'

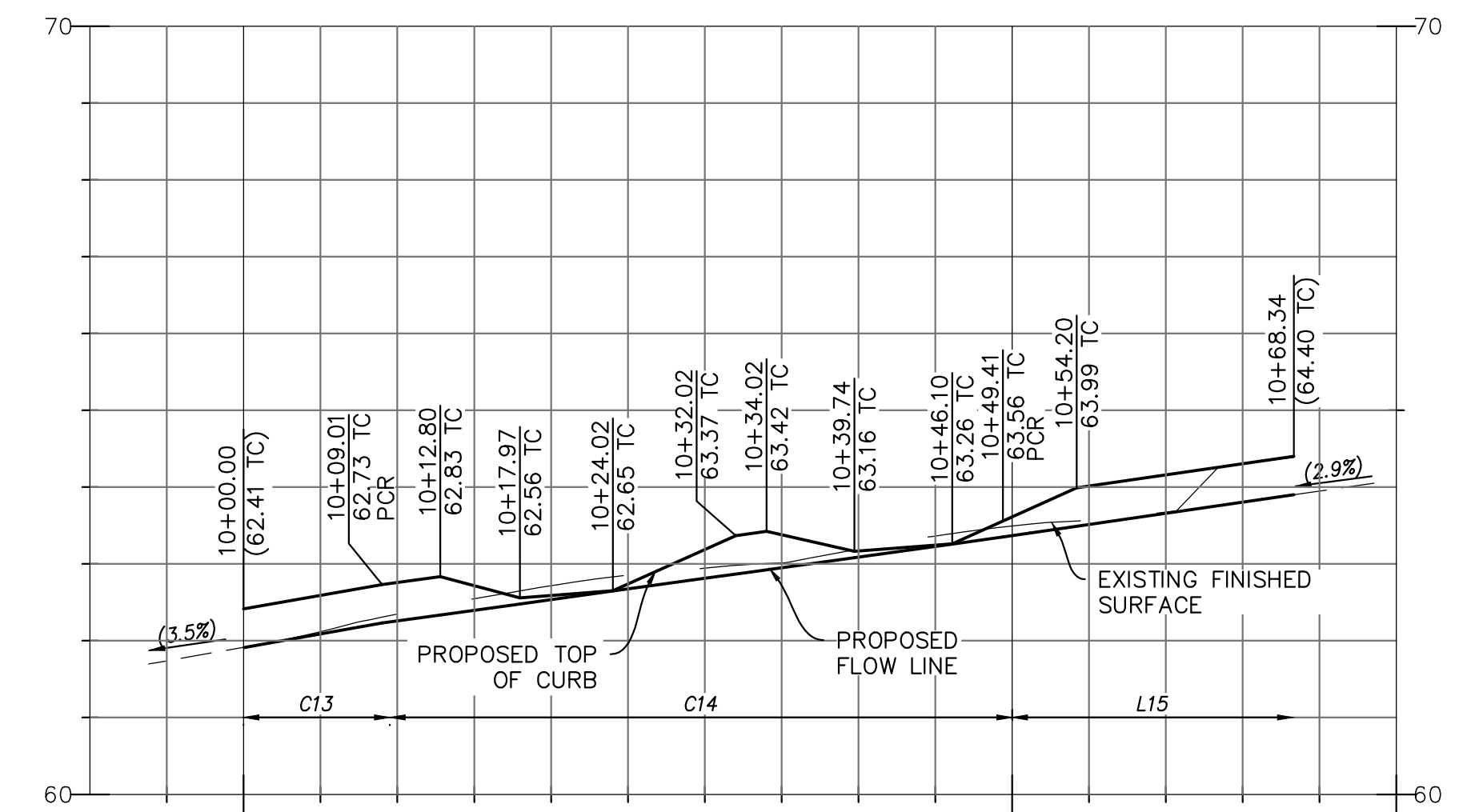
#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C13	9.01'	25°49'24.2"	20.00'	6" CURB AND GUTTER
C14	40.39'	115°43'06.4"	20.00'	6" CURB AND GUTTER
C16	9.02'	25°50'31.0"	20.00'	6" CURB AND GUTTER
C17	39.92'	114°22'24.0"	20.00'	6" CURB AND GUTTER
C19	31.12'	89°08'31.9"	20.00'	6" CURB AND GUTTER
C20	9.26'	26°31'16.1"	20.00'	6" CURB AND GUTTER
C21	9.02'	25°50'47.4"	20.00'	6" CURB AND GUTTER
C22	9.03'	25°51'17.1"	20.00'	6" CURB AND GUTTER
C23	40.43'	115°49'55.6"	20.00'	6" CURB AND GUTTER
L15	18.93'	N25°19'13"W		6" CURB AND GUTTER
L16	6.78'	S26°52'40"E		6" CURB AND GUTTER
L17	6.38'	N24°33'05"W		6" CURB AND GUTTER
L19	4.46'	N64°35'27"E		6" CURB AND GUTTER
L22	14.20'	N25°25'08"W		6" CURB AND GUTTER

CONSTRUCTION NOTES

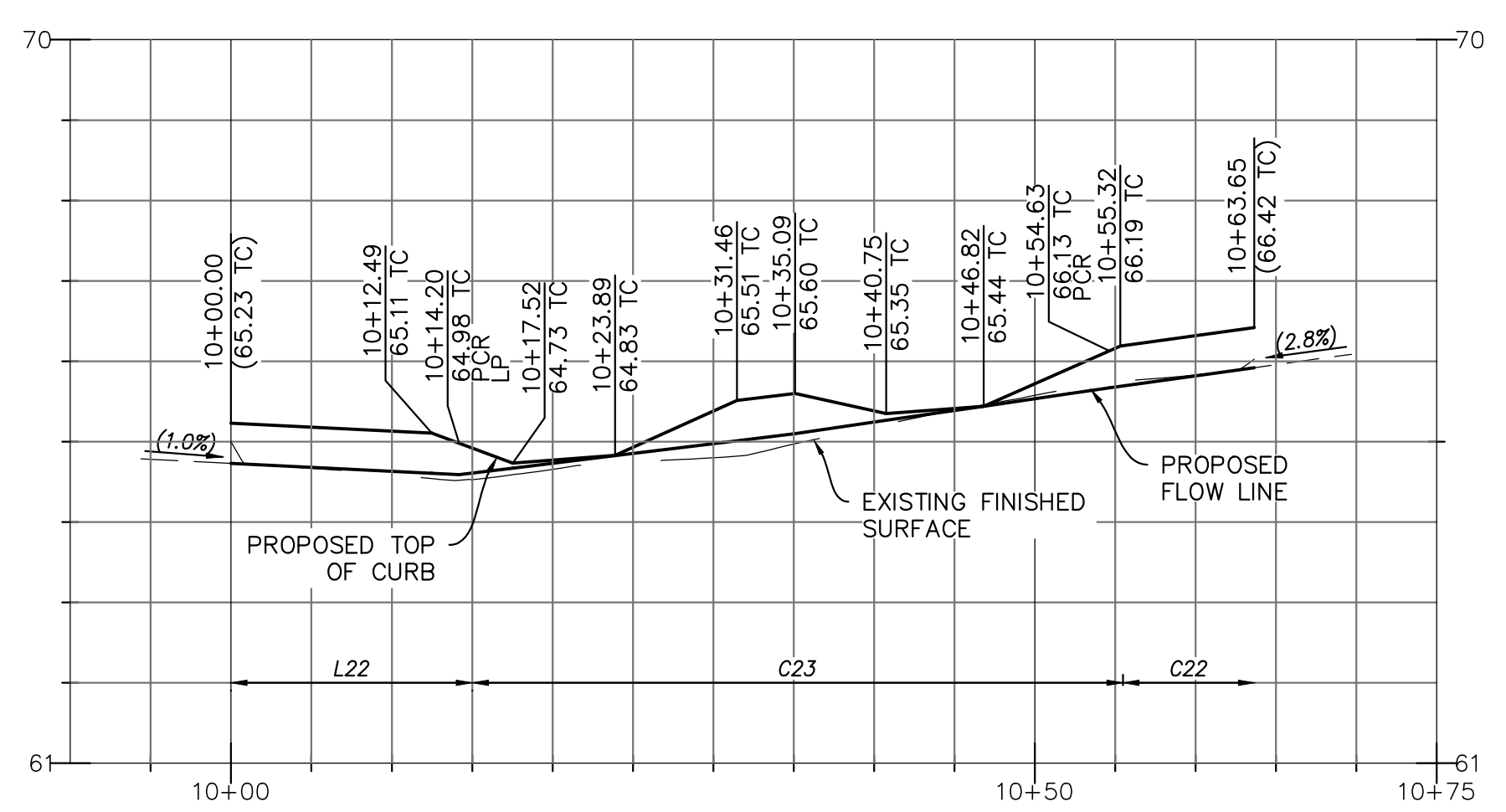
- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ③ CONSTRUCT CASE A CURB RAMP PER CALTRANS STANDARD PLAN A88A.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑪ REMOVE AND REPLACE CROSS GUTTER PER SDRSD G-12.
- ⑫ EXISTING CROSS GUTTER TO REMAIN AND BE PROTECTED IN PLACE.
- ⑬ EXISTING CURB RAMP TO BE REMOVED.
- ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑳ INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
- ㉑ EXISTING STREET SIGN TO BE REMOVED AND RELOCATED AS NEEDED.
- ㉒ EXISTING CURB INLET TO REMAIN AND BE PROTECTED IN PLACE.
- ㉓ REMOVE AND REPLACE WITH CASE G CURB RAMP PER CALTRANS CRD A88A.
- ㉔ EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉕ EXISTING HYDRANT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉖ EXISTING STORM DRAIN CLEANOUT RIM TO BE ADJUSTED TO GRADE.
- ㉗ PROPOSED COBBLESTONE AREA.



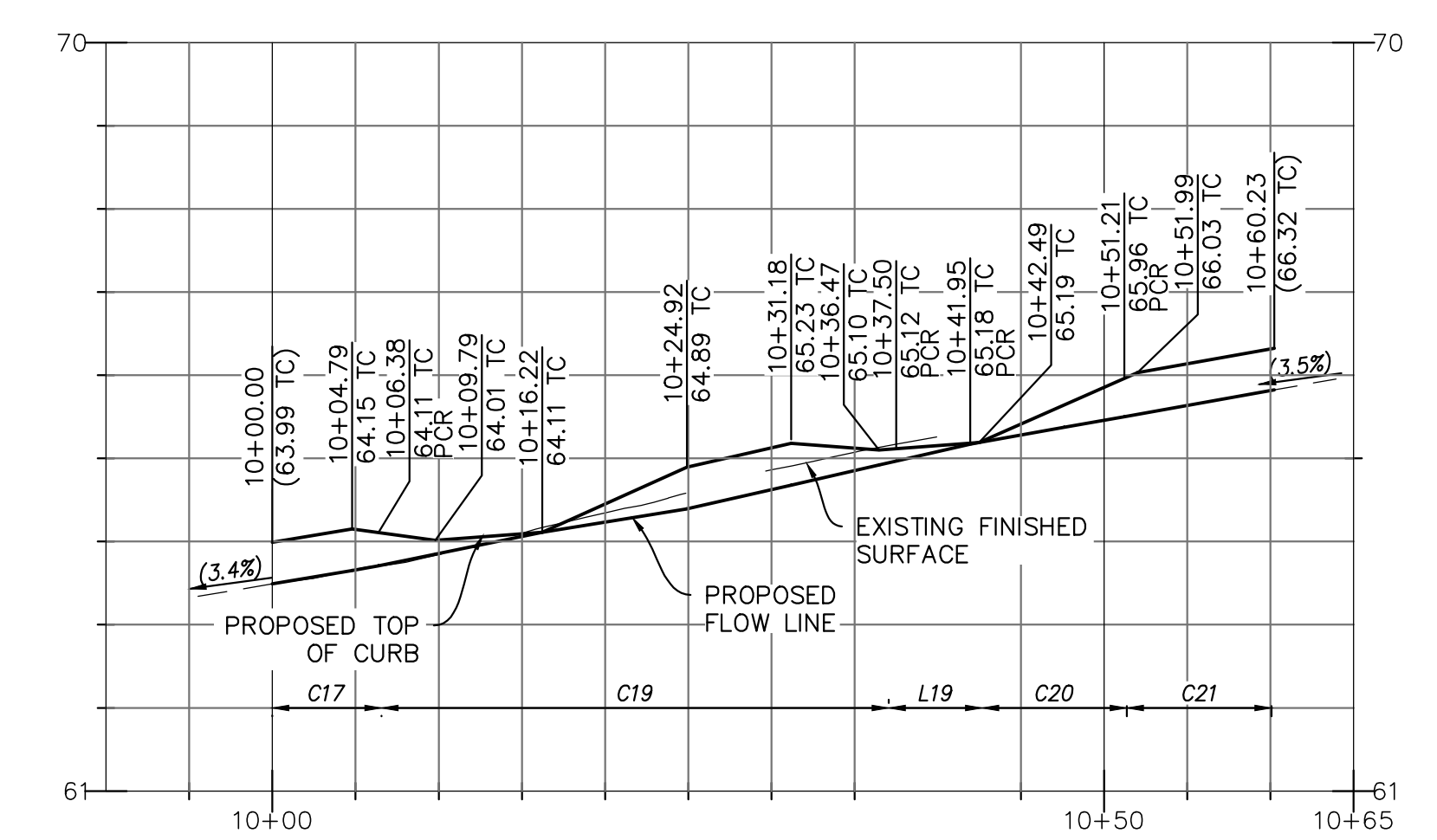
CURB RETURN G
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



CURB RETURN H
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



CURB RETURN I
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



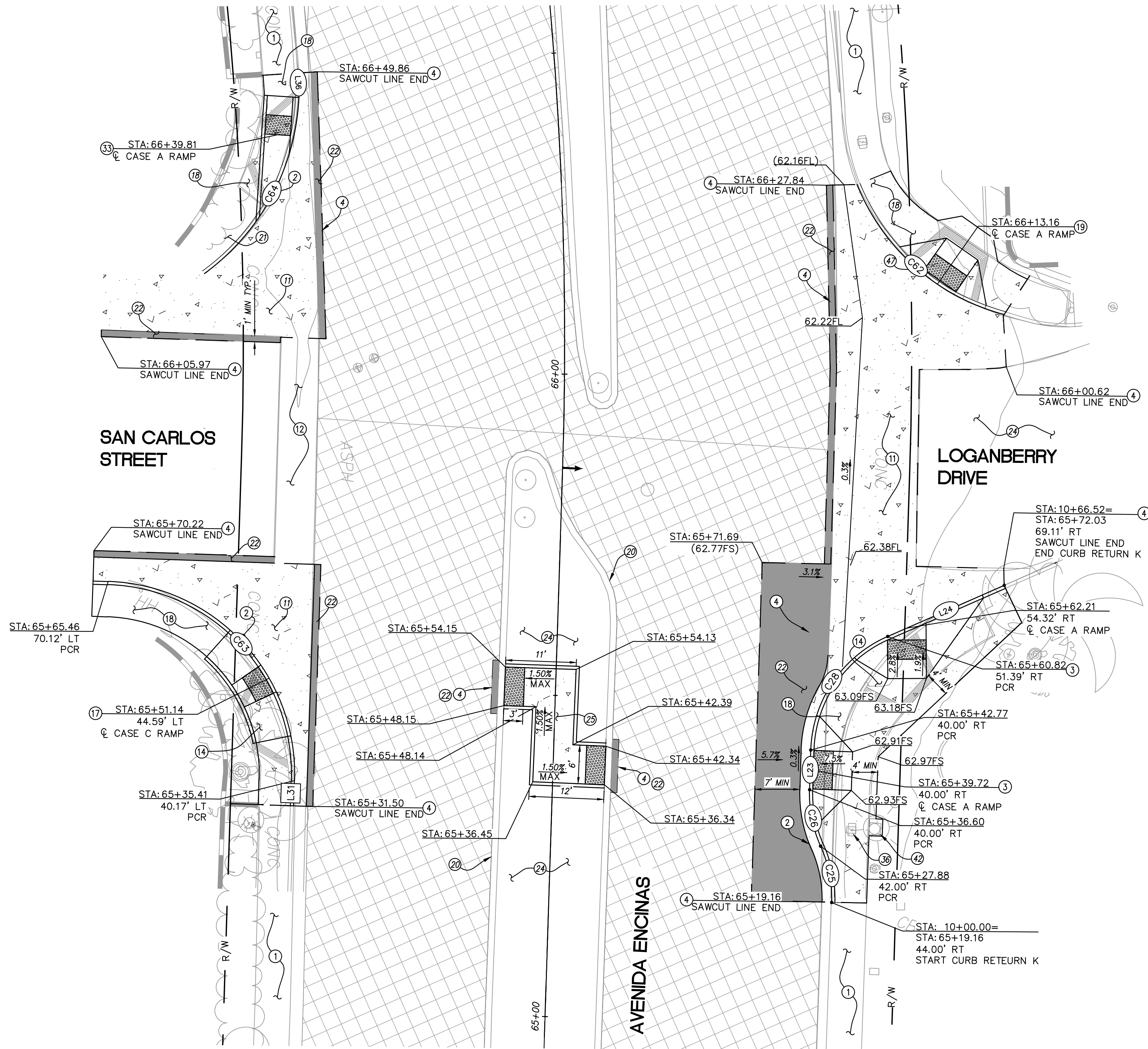
CURB RETURN J
SCALE HORIZ: 1"=10'
SCALE VERT: 1"=2'



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JOB NO.: 121-108.1

SHEET 16		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
DETAILS FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: _____ HOSSEIN AJUDEH					
ENGINEERING MANAGER RCE 75991 EXP.6/20/24 DATE _____					
DWN BY: EE/DG		CIP NO. 6004		DRAWING NO. DWG 536-6	
CHKD BY: DD					
RWVD BY: LT					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		REVISION DESCRIPTION		OTHER APPROVAL	
				CITY APPROVAL	



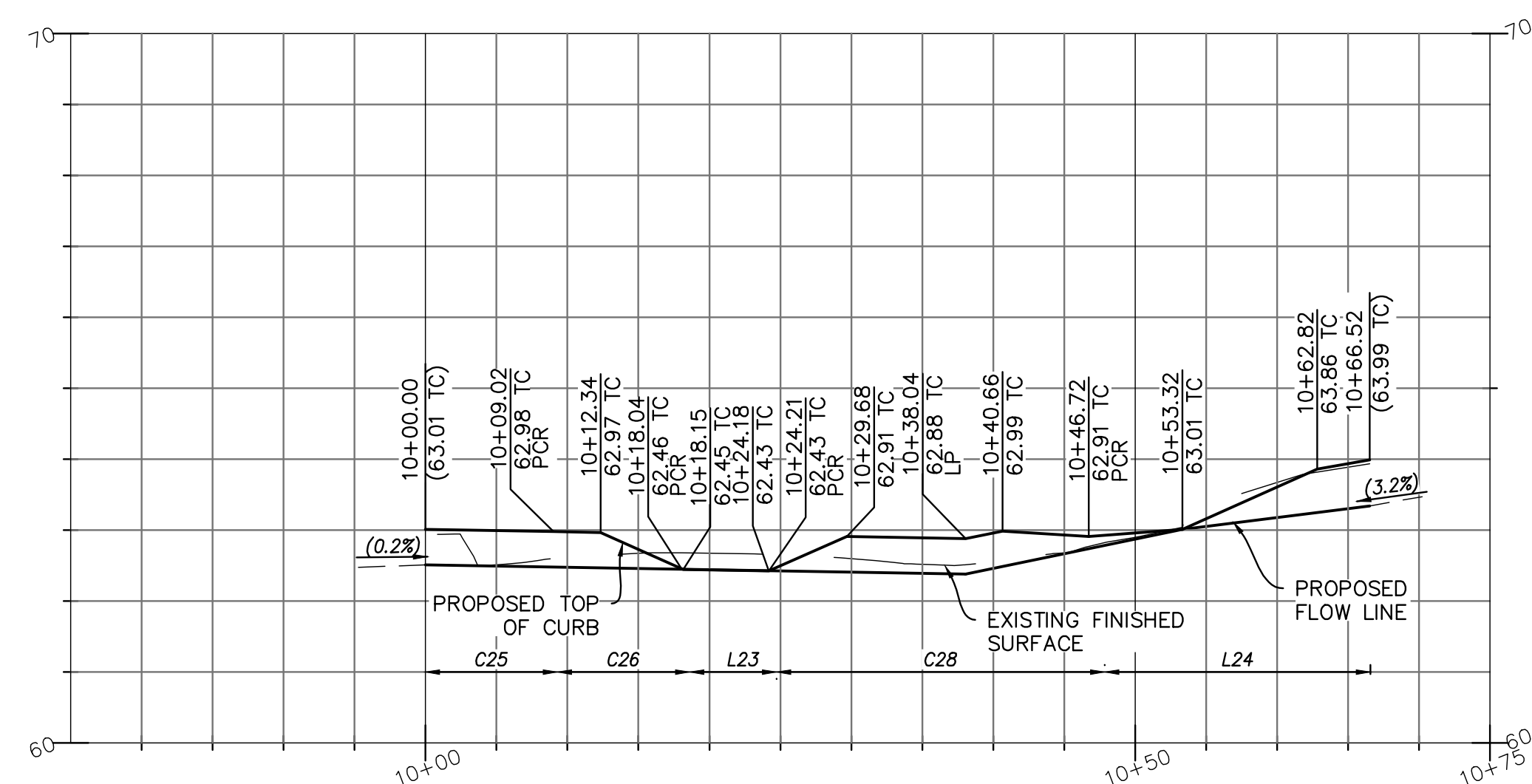
SAN CARLOS STREET AND LOGANBERRY DR INTERSECTION
PLAN VIEW
 SCALE: 1" = 10'

CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ③ CONSTRUCT CASE A CURB RAMP PER CALTRANS STANDARD PLAN A88A.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑪ REMOVE AND REPLACE CROSS GUTTER PER SDRSD G-12.
- ⑫ EXISTING CROSS GUTTER TO REMAIN AND BE PROTECTED IN PLACE.
- ⑭ EXISTING CURB RAMP TO BE REMOVED.
- ⑰ REMOVE AND REPLACE WITH CASE C CURB RAMP PER CALTRANS CRD A88A.
- ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑲ REMOVE AND REPLACE WITH CASE A CURB RAMP PER CALTRANS CRD A88A.
- ⑳ EXISTING CURB AND GUTTER TO REMAIN AND BE PROTECTED IN PLACE.
- ㉑ INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
- ㉒ EXISTING DECORATIVE PAVEMENT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉓ INSTALL TYPE B ISLAND PASSAGEWAY PER CALTRANS STANDARD PLAN A88A. SAWCUT AND REMOVE CONFLICTING MEDIAN PAVING.
- ㉔ REMOVE AND REPLACE WITH CASE G CURB RAMP PER CALTRANS CRD A88A.
- ㉕ EXISTING PULL BOX TO BE ADJUSTED TO GRADE AS NEEDED.
- ㉖ EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉗ PROPOSED MONOLITHIC CURB PER SDRSD G-01.

CURB DATA TABLE

#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C25	9.02'	25°50'31.0"	20.00'	6" CURB AND GUTTER
C26	9.02'	25°50'29.7"	20.00'	6" CURB AND GUTTER
C28	22.51'	64°29'55.4"	20.00'	6" CURB AND GUTTER
C62	31.59'	53°14'03.7"	34.00'	6" CURB
C63	46.34'	90°00'00.0"	29.50'	6" CURB AND GUTTER
C64	32.95'	52°26'20.8"	36.00'	6" CURB AND GUTTER
L23	6.17	N19°15'00"W		6" CURB AND GUTTER
L24	19.80	N45°14'55"E		6" CURB
L31	3.92	N19°09'22"W		6" CURB AND GUTTER
L36	3.04	N24°53'08"W		6" CURB AND GUTTER



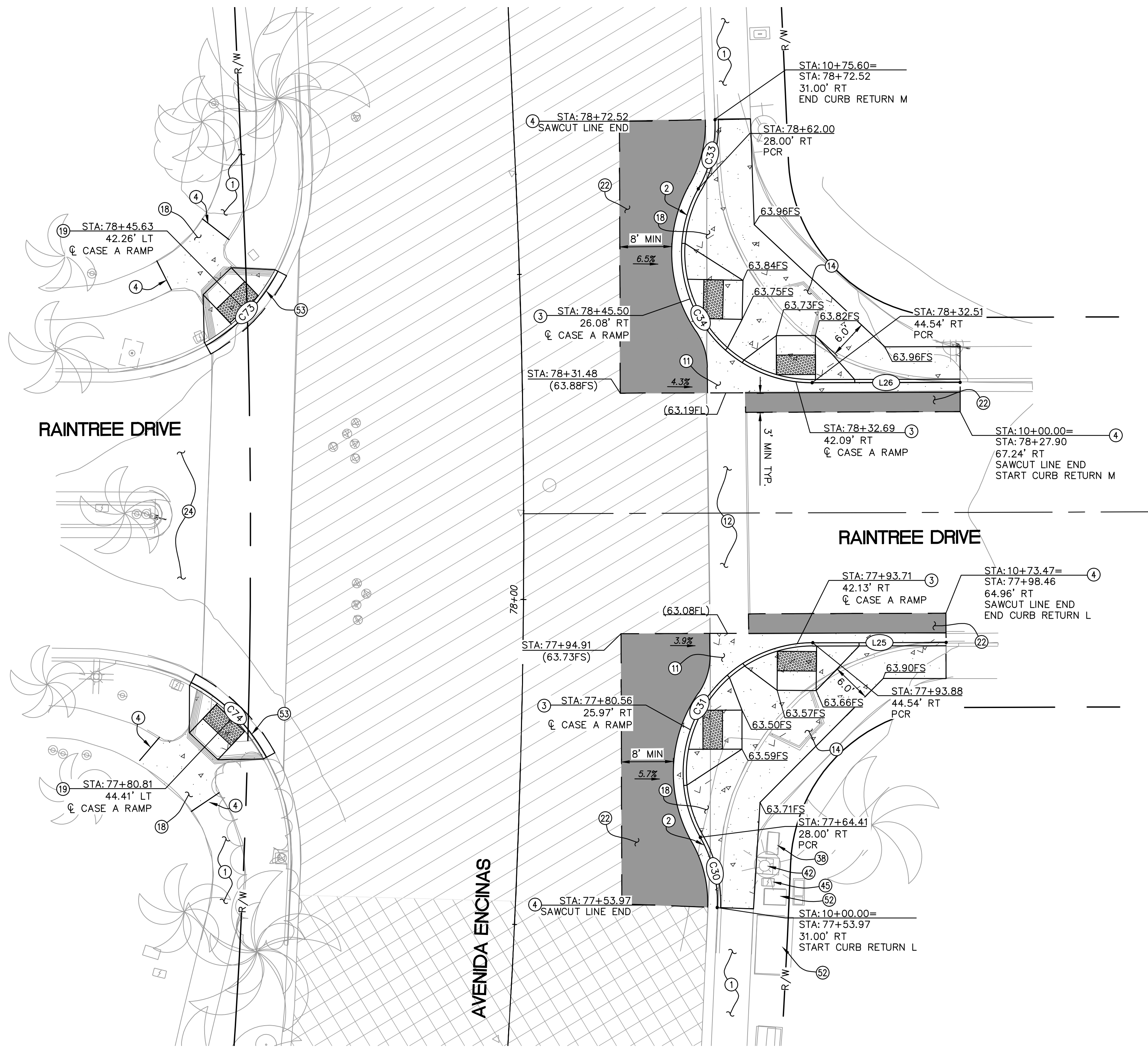
CURB RETURN K
 SCALE HORIZ: 1"=10'
 SCALE VERT: 1"=2'



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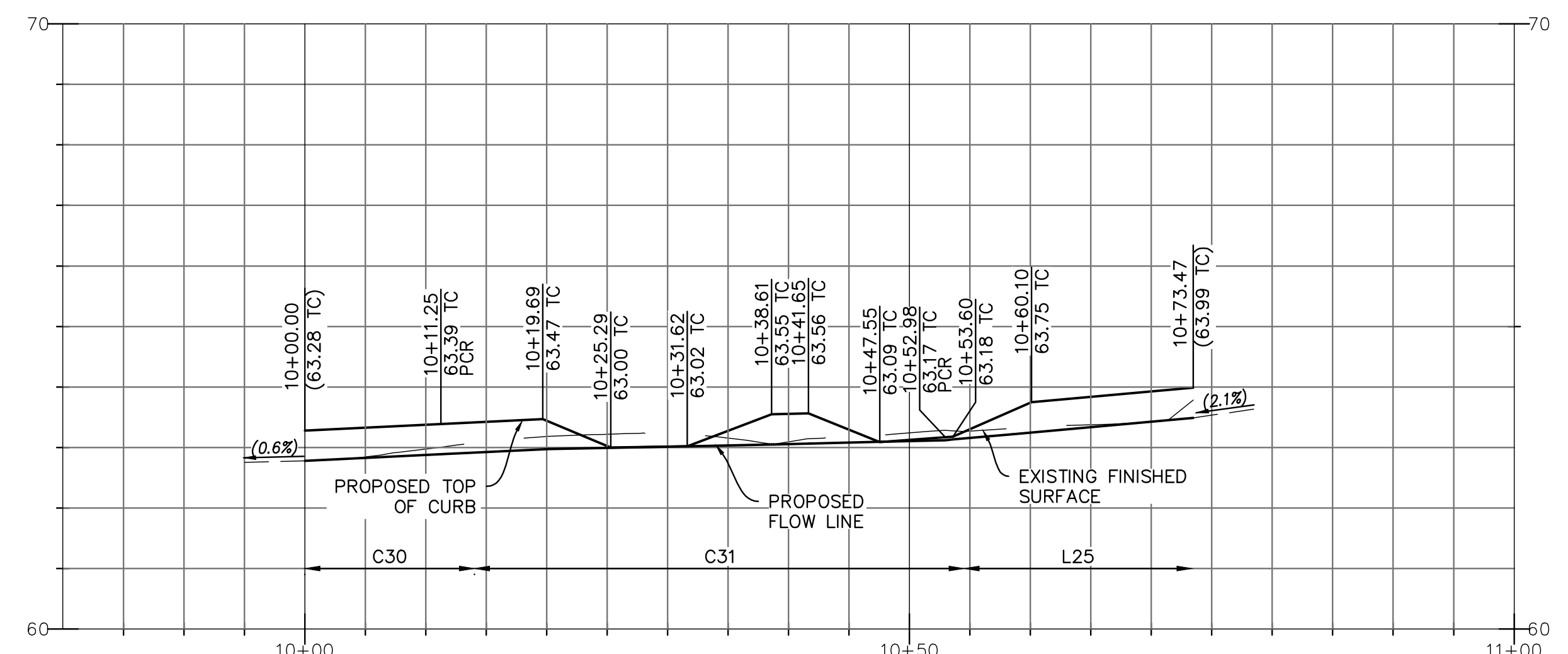
SHEET 17		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
DETAILS FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: _____				HOSSEIN AJUDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL		CITY APPROVAL	
REVISION DESCRIPTION					
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DD		6004		DWG 536-6	
RWMD BY: LT					



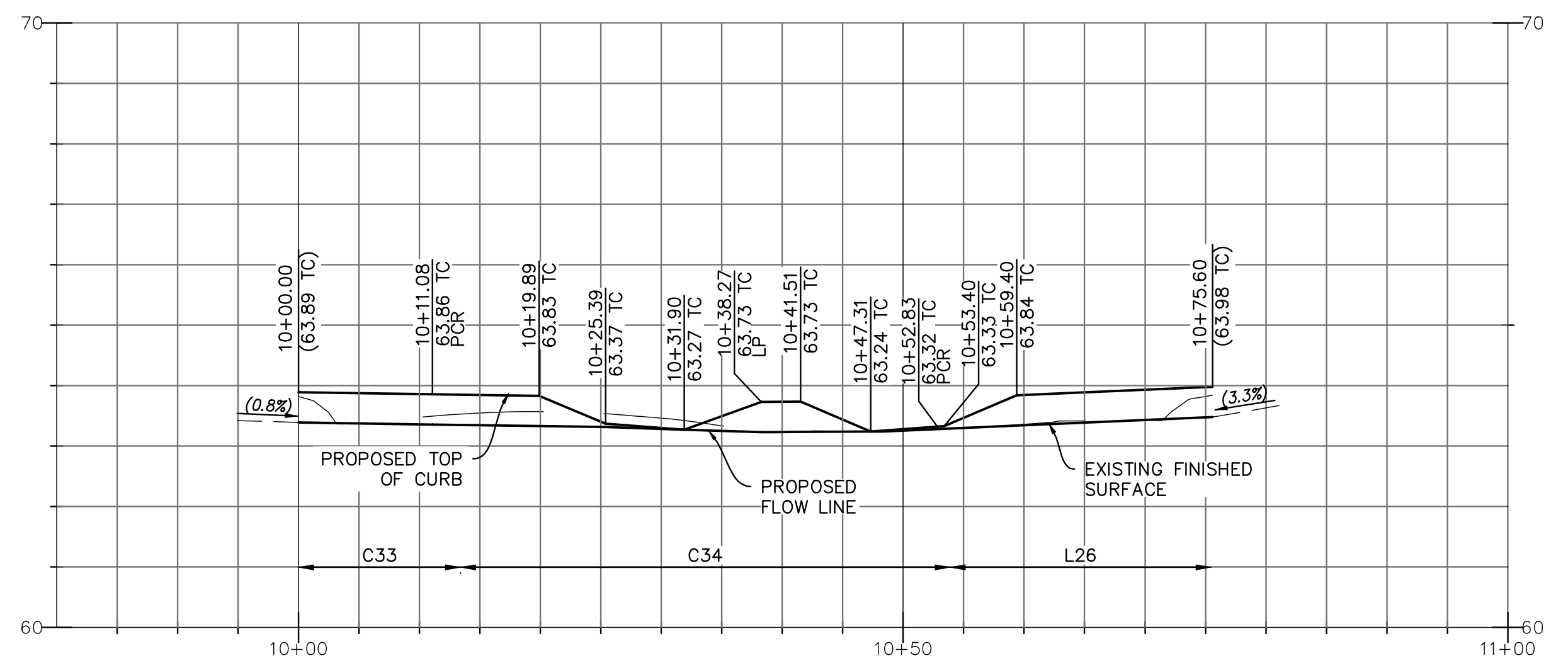
AVENIDA ENCINAS AND RAIN TREE DR INTERSECTION
PLAN VIEW
 SCALE: 1" = 10'

CURB DATA TABLE				
#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C30	11.25'	32°14'01.2"	20.00'	6" CURB AND GUTTER
C31	41.73'	119°32'57.3"	20.00'	6" CURB AND GUTTER
C33	11.08'	31°43'55.2"	20.00'	6" CURB AND GUTTER
C34	41.75'	119°36'09.5"	20.00'	6" CURB AND GUTTER
C73	16.09'	32°55'05.4"	28.00'	6" CURB
C74	16.55'	33°52'05.5"	28.00'	6" CURB
L25	20.48	N75°49'45"E		6" CURB AND GUTTER
L26	22.77	N75°49'45"E		6" CURB AND GUTTER

- CONSTRUCTION NOTES**
- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
 - ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
 - ③ CONSTRUCT CASE A CURB RAMP PER CALTRANS STANDARD PLAN A88A.
 - ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
 - ⑪ REMOVE AND REPLACE CROSS GUTTER PER SDRSD G-12.
 - ⑫ EXISTING CROSS GUTTER TO REMAIN AND BE PROTECTED IN PLACE.
 - ⑭ EXISTING CURB RAMP TO BE REMOVED.
 - ⑱ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
 - ⑲ REMOVE AND REPLACE WITH CASE A CURB RAMP PER CALTRANS CRD A88A.
 - ⑳ INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
 - ㉔ EXISTING DECORATIVE PAVEMENT TO REMAIN AND BE PROTECTED IN PLACE.
 - ㉘ EXISTING COMMUNICATION RISER TO REMAIN AND BE PROTECTED IN PLACE.
 - ㉚ EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.
 - ㉜ EXISTING PULL BOX TO REMAIN AND BE PROTECTED IN PLACE.
 - ㉞ EXISTING VAULT TO REMAIN AND BE PROTECTED IN PLACE.
 - ㉟ REMOVE AND REPLACE CURB AND GUTTER PER SDRSD G-02.



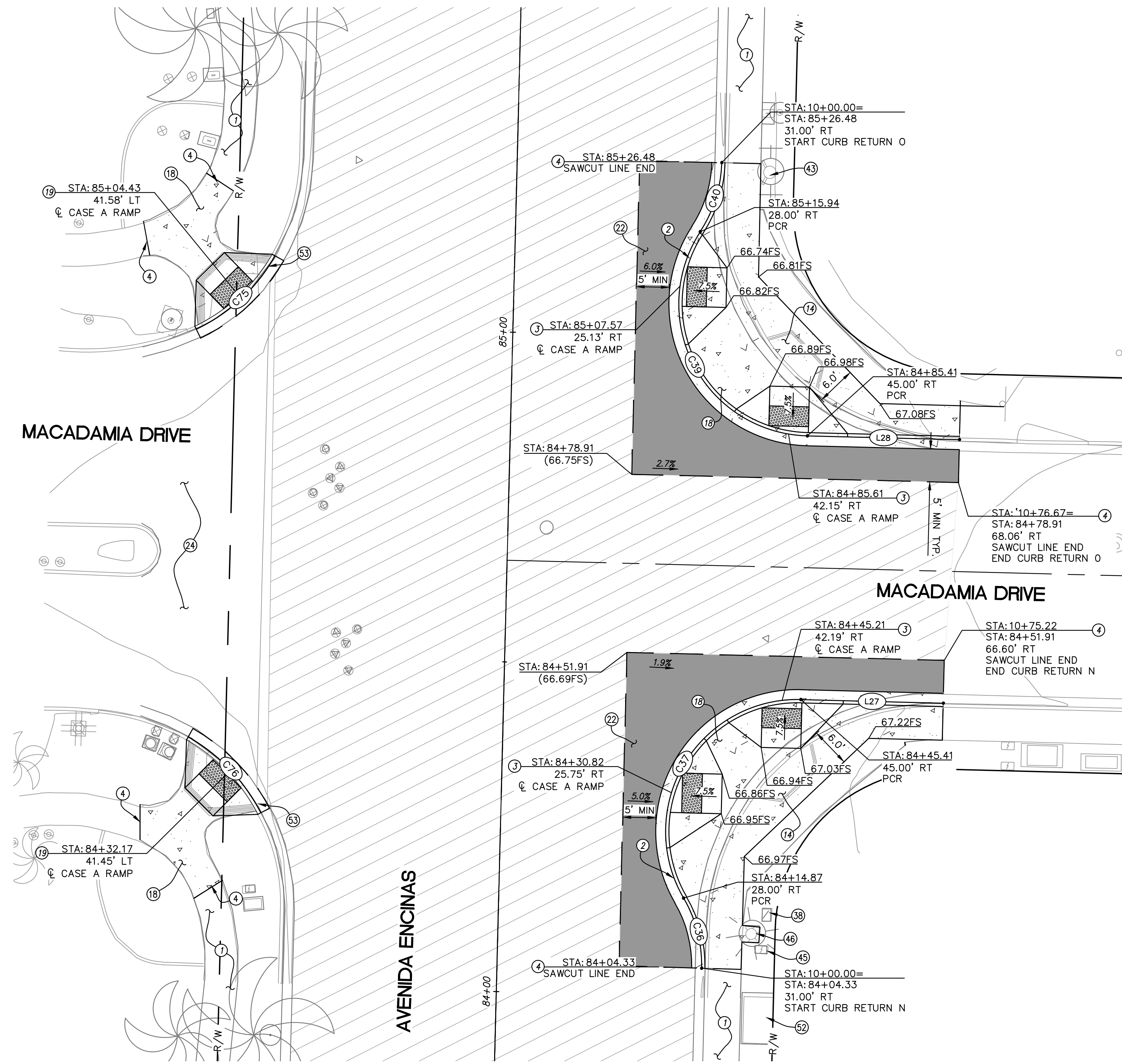
CURB RETURN L
 SCALE HORIZ: 1"=10'
 SCALE VERT: 1"=2'



CURB RETURN M
 SCALE HORIZ: 1"=10'
 SCALE VERT: 1"=2'



SHEET 18		CITY OF CARLSBAD TRANSPORTATION DEPARTMENT		SHEETS 29	
DETAILS FOR: AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED: _____				HOSSEIN AJUDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24	
DATE				DATE	
DWN BY: EF/DG		CIP NO.		DRAWING NO.	
CHKD BY: DD		6004		DWG 536-6	
RVWD BY: LT					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					

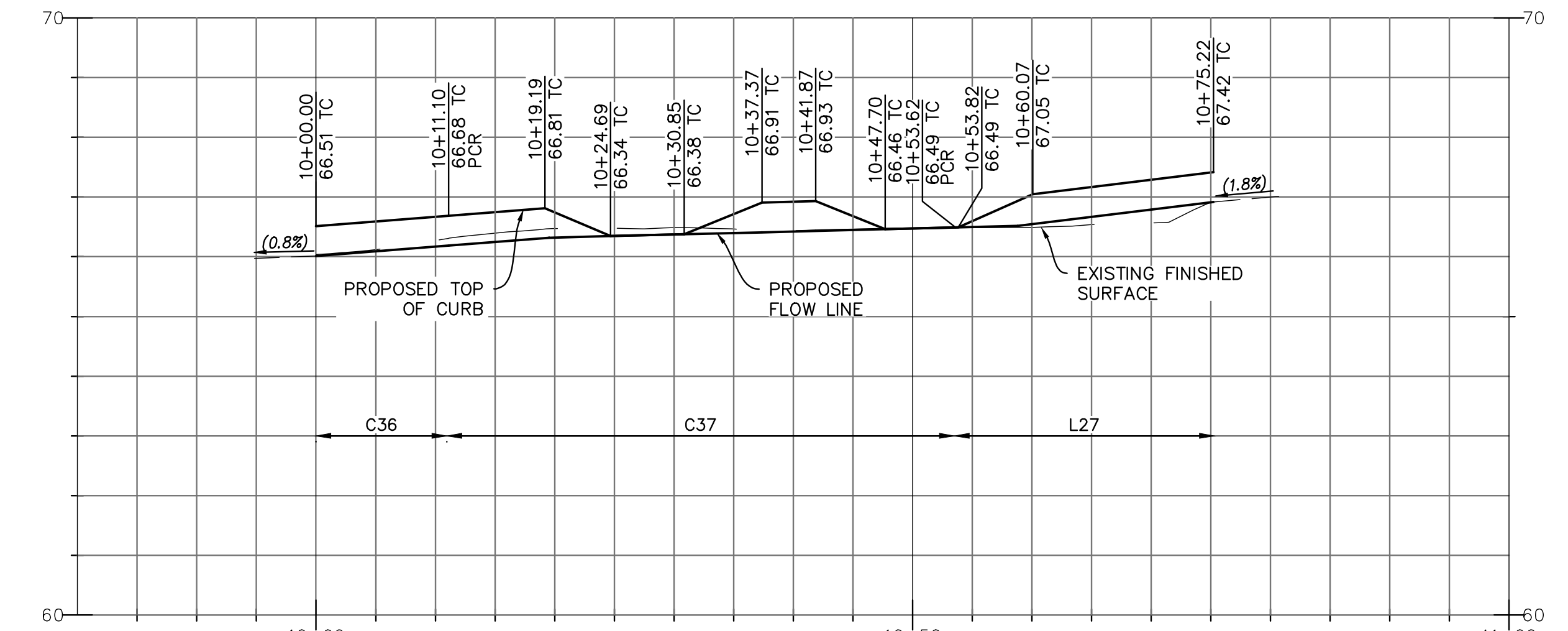


AVENIDA ENCINAS AND MACADAMIA DR INTERSECTION
PLAN VIEW
 SCALE: 1" = 10'

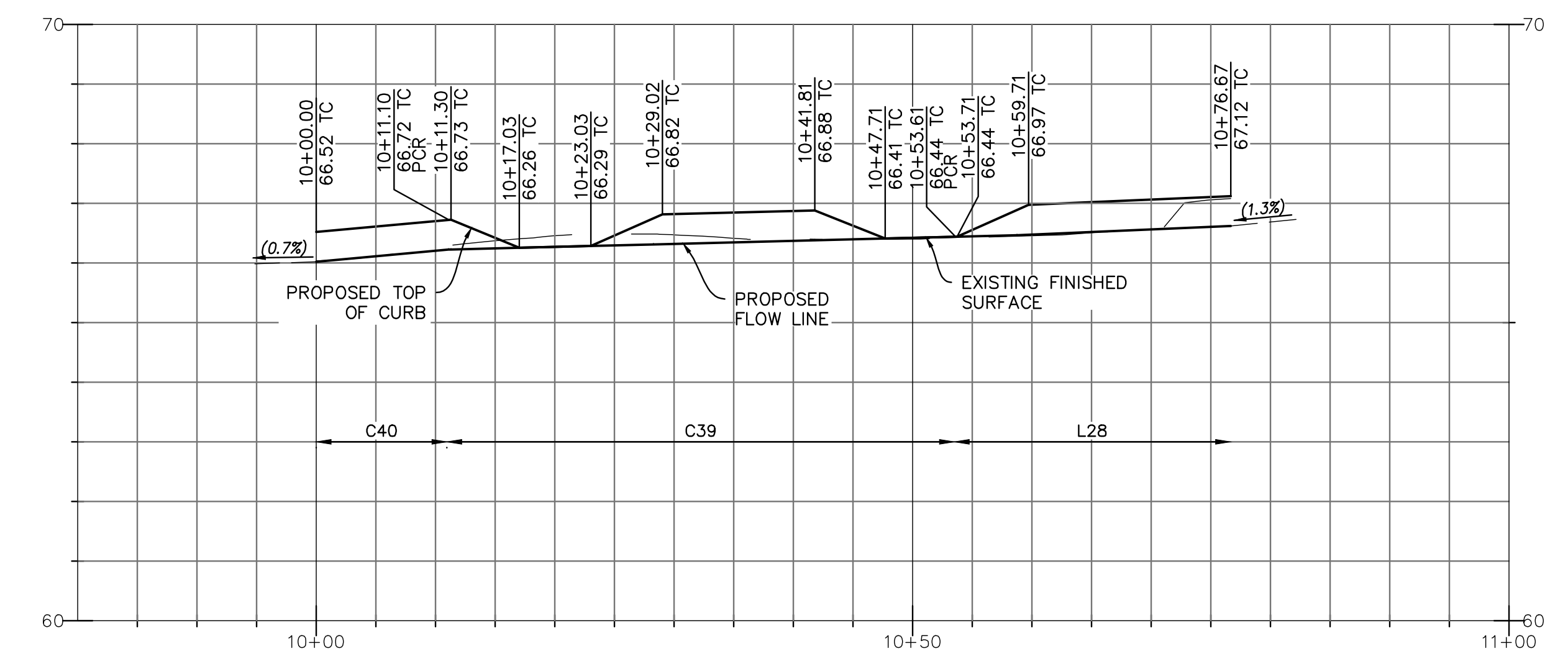
CURB DATA TABLE				
#	LENGTH	Δ/BEARING	RADIUS	REMARKS
C36	11.10'	31°48'47.0"	20.00'	6" CURB AND GUTTER
C37	42.51'	121°47'18.0"	20.00'	6" CURB AND GUTTER
C39	42.51'	121°47'18.0"	20.00'	6" CURB AND GUTTER
C40	11.10'	31°47'27.2"	20.00'	6" CURB AND GUTTER
C75	16.57'	35°10'13.3"	27.00'	6" CURB
C76	16.13'	33°00'12.0"	28.00'	6" CURB
L27	21.60'	N73°26'09"E		6" CURB AND GUTTER
L28	23.06'	S73°26'09"W		6" CURB AND GUTTER

CONSTRUCTION NOTES

- ① EXISTING SIDEWALK TO REMAIN AND BE PROTECTED IN PLACE.
- ② INSTALL TYPE A CURB & GUTTER PER SDRSD G-02.
- ③ CONSTRUCT CASE A CURB RAMP PER CALTRANS STANDARD PLAN A88A.
- ④ PROPOSED SAWCUT LINE. MATCH EXISTING ELEVATIONS.
- ⑬ EXISTING CURB RAMP TO BE REMOVED.
- ⑭ INSTALL TYPE A SIDEWALK PER SDRSD G-07. REMOVE EXISTING CONCRETE, CURB & GUTTER, AND PAVEMENT AS SHOWN.
- ⑰ REMOVE AND REPLACE WITH CASE A CURB RAMP PER CALTRANS CRD A88A.
- ⑱ INSTALL NEW PAVEMENT SECTION, 4" AC PAVEMENT WITH 8" AGGREGATE BASE.
- ⑳ EXISTING DECORATIVE PAVEMENT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉑ EXISTING COMMUNICATION RISER TO REMAIN AND BE PROTECTED IN PLACE.
- ㉒ EXISTING HYDRANT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉓ EXISTING PULL BOX TO REMAIN AND BE PROTECTED IN PLACE.
- ㉔ EXISTING STREET LIGHT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉕ EXISTING VAULT TO REMAIN AND BE PROTECTED IN PLACE.
- ㉖ REMOVE AND REPLACE CURB AND GUTTER PER SDRSD G-02.



CURB RETURN N
 SCALE HORIZ: 1" = 10'
 SCALE VERT: 1" = 2'



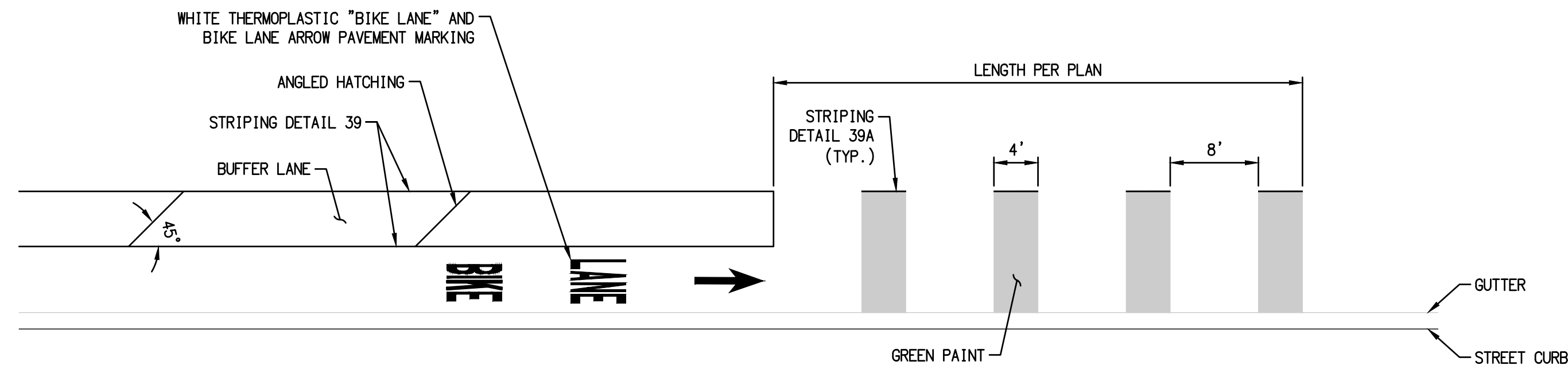
CURB RETURN O
 SCALE HORIZ: 1" = 10'
 SCALE VERT: 1" = 2'



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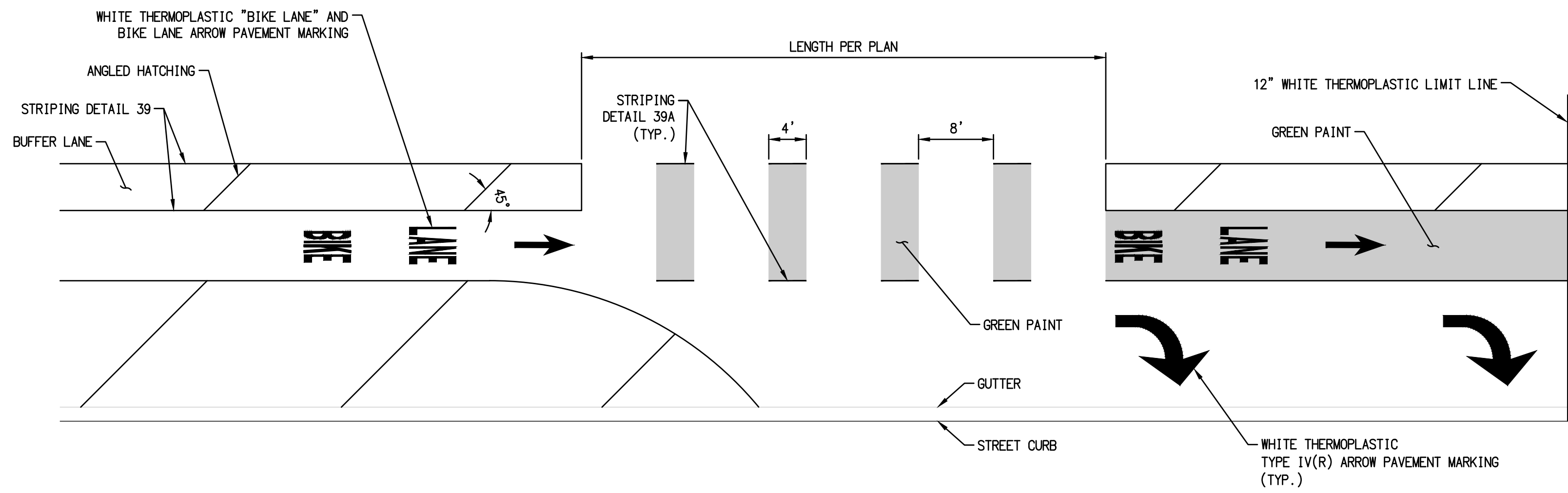
JOB NO.: 121-108.1

SHEET 19		CITY OF CARLSBAD		SHEETS 29	
		TRANSPORTATION DEPARTMENT			
DETAILS FOR:					
AVENIDA ENCINAS COASTAL RAIL TRAIL AND PEDESTRIAN IMPROVEMENTS					
APPROVED:				HOSSEIN AJUDEH	
ENGINEERING MANAGER				RCE 75991 EXP.6/20/24 DATE	
DATE		INITIAL		DATE	
ENGINEER OF WORK		REVISION DESCRIPTION		OTHER APPROVAL	
				CITY APPROVAL	
				CIP NO. 6004	
				DRAWING NO. DWG 536-6	



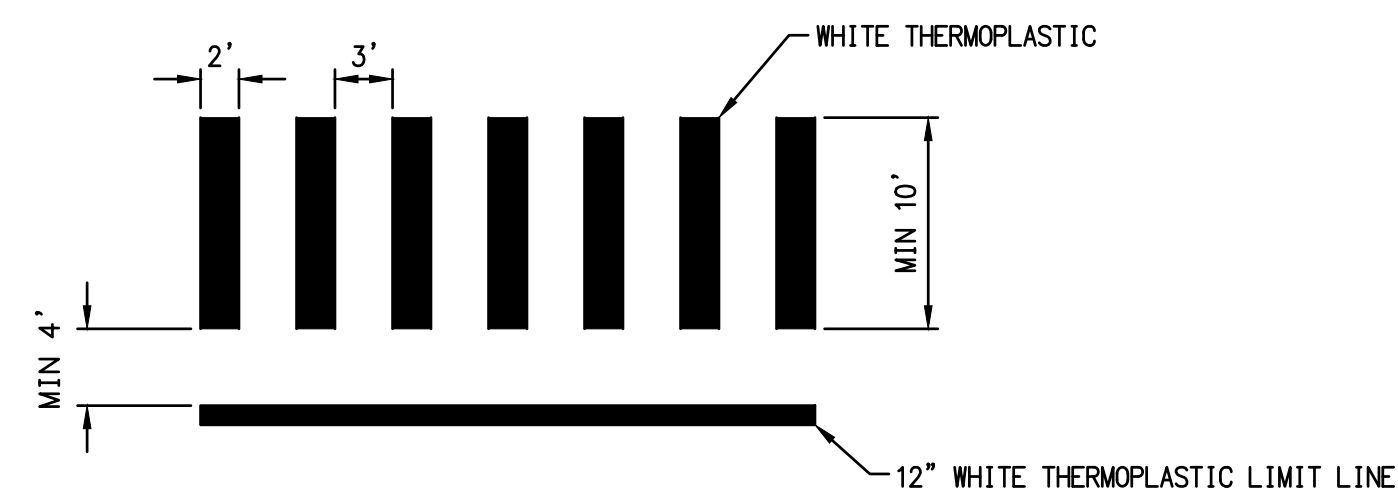
DETAIL "A"
CONFLICT ZONE STRIPING WITHOUT DEDICATED RIGHT-TURN LANE
NOT TO SCALE

- NOTES:
1. GREEN PAINT SHALL BE ENNIS-FLINT 985206 GREEN STANDARD FAST DRY WATERBORNE 1952F 1/2 TRAFFIC PAINT, PRODUCT CODE 183, PRODUCT COLOR GREEN (34108), WITH REFLECTIVE GLASS BEADS.
 2. ANGLE HATCHING FOR BUFFERED BIKE LANES SHALL BE 12" WIDE, AT A 45° ANGLE, AND SPACED AT 40 FEET ON CENTER. ANGLE HATCHING WITHIN RED CURB AREAS SHALL BE SPACED AT 20 FEET ON CENTER.
 3. LANE AND BUFFER WIDTHS VARY PER PLAN.



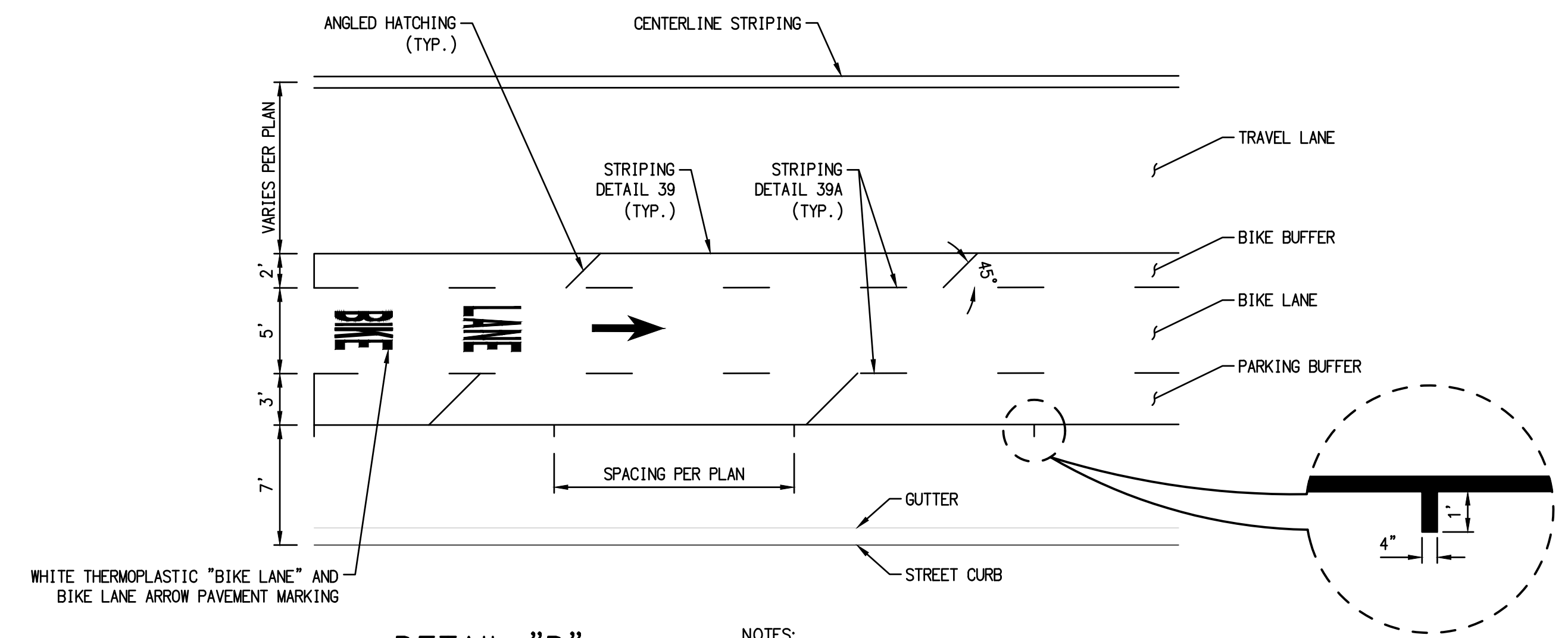
DETAIL "B"
CONFLICT ZONE STRIPING WITH DEDICATED RIGHT-TURN LANE
NOT TO SCALE

- NOTES:
1. GREEN PAINT SHALL BE ENNIS-FLINT 985206 GREEN STANDARD FAST DRY WATERBORNE 1952F 1/2 TRAFFIC PAINT, PRODUCT CODE 183, PRODUCT COLOR GREEN (34108), WITH REFLECTIVE GLASS BEADS.
 2. ANGLE HATCHING FOR BUFFERED BIKE LANES SHALL BE 12" WIDE, AT A 45° ANGLE, AND SPACED AT 40 FEET ON CENTER. ANGLE HATCHING WITHIN RED CURB AREAS SHALL BE SPACED AT 20 FEET ON CENTER.
 3. LANE AND BUFFER WIDTHS VARY PER PLAN.



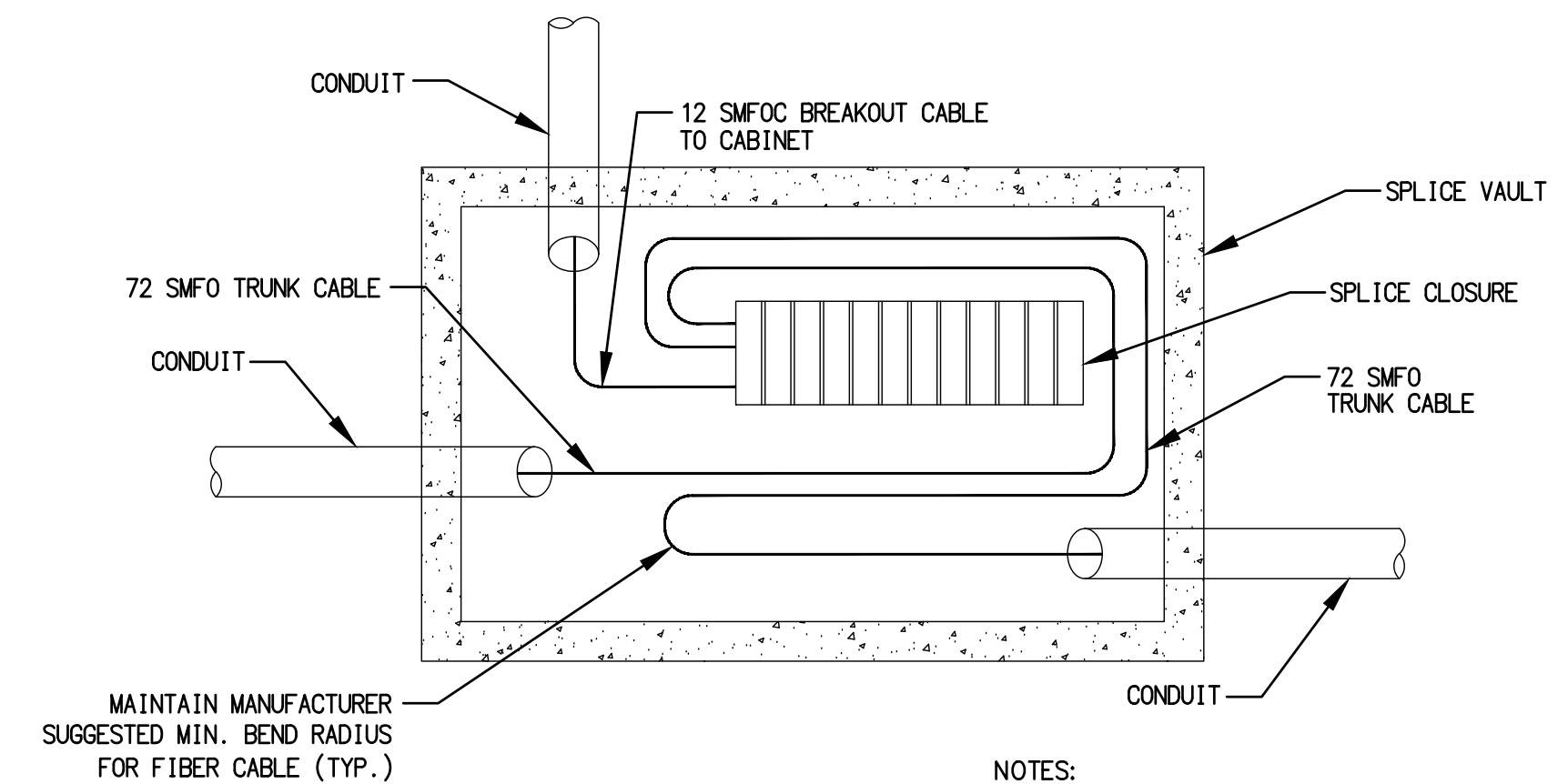
DETAIL "C"
CONTINENTAL CROSSWALK MARKINGS
NOT TO SCALE

- NOTES:
1. CONTINENTAL CROSSWALK MARKINGS SHALL BE ALIGNED PARALLEL TO THE DIRECTION OF VEHICULAR TRAVEL.
 2. MARKED CROSSWALKS SHOULD BE A MINIMUM OF 10 FEET IN WIDTH, PLACEMENT OF CONTINENTAL CROSSWALKS SHALL COMPLY WITH ACCESSIBILITY REGULATIONS PER THE MOST RECENT VERSION OF AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS.
 3. THE CROSSWALK BETWEEN A DUAL RAMP CORNER AND A SINGLE RAMP CORNER SHALL BE AT LEAST 10 FEET WIDE AND SATISFY THE MINIMUM OF 2 FEET BEYOND THE FLARE REQUIREMENT FOR THE SINGLE RAMP.
 4. CONTINENTAL CROSSWALK BARS SHALL BE UNIFORM WITHIN THE SAME CROSSING. NO PARTIAL BARS SHALL BE INSTALLED.
 5. A CROSSWALK BAR SHALL BE CENTERED IN THE CENTER OF THE CROSSING.
 6. CROSSWALK MARKINGS SHALL BE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA-MUTCD) RETROREFLECTIVITY COMPLIANT AND SKID RESISTANT.



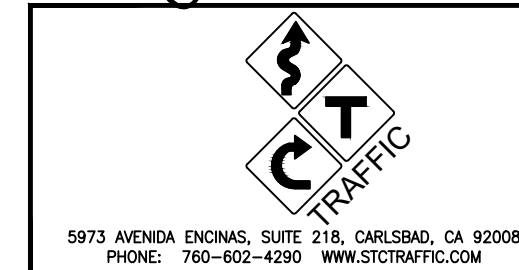
DETAIL "D"
PARKING STALL MARKINGS
NOT TO SCALE

- NOTES:
1. ANGLE HATCHING FOR BUFFERED BIKE LANES SHALL BE 12" WIDE, AT A 45° ANGLE, AND SPACED AT 40 FEET ON CENTER. ANGLE HATCHING WITHIN RED CURB AREAS SHALL BE SPACED AT 10 FEET ON CENTER.
 2. LANE AND BUFFER WIDTHS VARY PER PLAN.



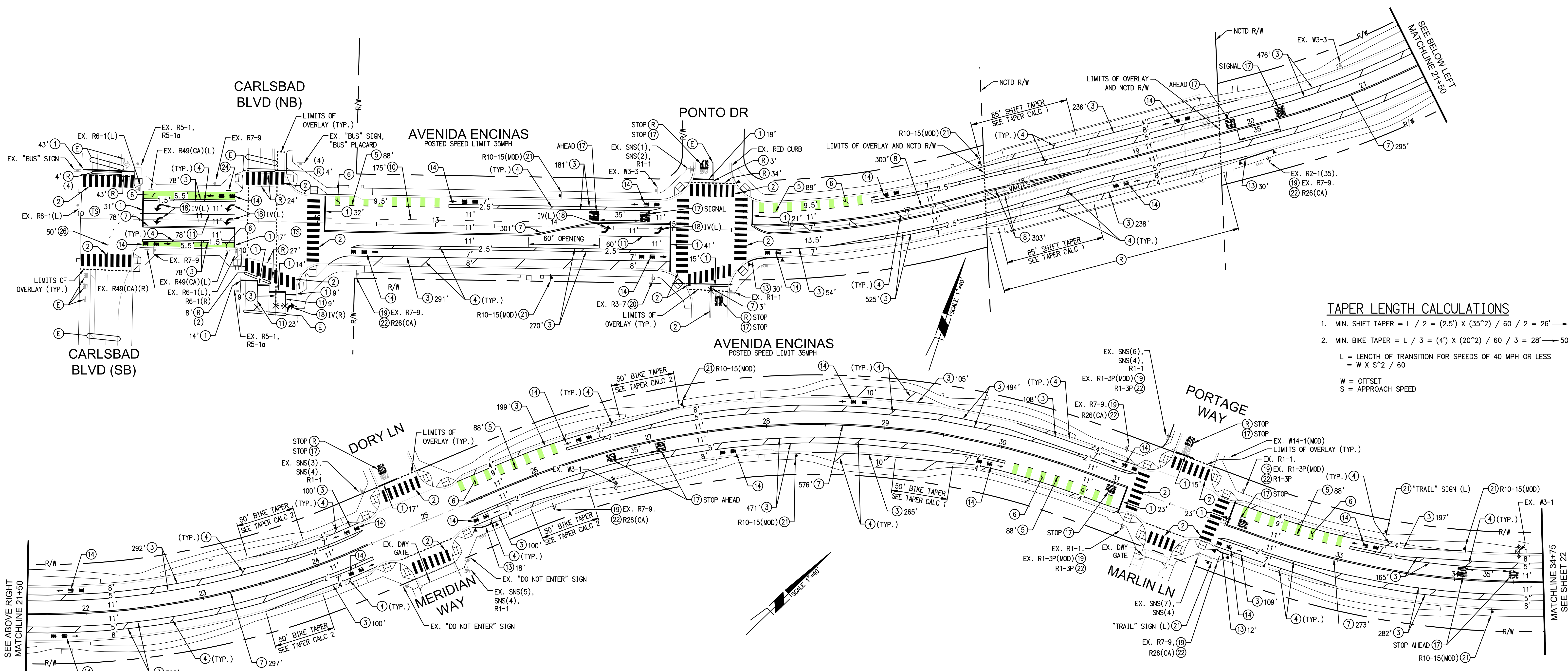
DETAIL "E"
SPLICE CLOSURE
NOT TO SCALE

- NOTES:
1. PROVIDE 50' OF COIL PER TRUNK CABLE AND 25' OF COIL PER BREAKOUT CABLE.
 2. SPLICE CLOSURE SHALL BE HOUSED IN A 36"x36"x36" CITY-APPROVED SPLICED VAULT.



SHEET 20		CITY OF CARLSBAD ENGINEERING DEPARTMENT		SHEETS 29	
IMPROVEMENT PLANS FOR: AVENIDA ENCINAS CRT IMPROVEMENTS DETAIL SHEET					
APPROVED THOMAS FRANK					
TRANSPORTATION DIRECTOR RCE 49070 XX/XX/XX DATE					
DWN BY: GP		PROJECT NO. XXXX		DRAWING NO. 536-6	
CHKD BY: CL					
RVMD BY: CL					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					

C:\Users\jgarcia\OneDrive\Desktop\Projects\Nasim\22-0920-Carlsbad Avenida Encinas CRT\07_Engineering\CADD\DLV\21-0920-DIV_20-DT.dwg Jul 29, 2022 4:14pm R:\Shared\With Me\STC Data\Projects\Nasim\22-0920-Carlsbad Avenida Encinas CRT\07_Engineering\CADD\DLV\21-0920-DIV_20-DT.dwg



TAPER LENGTH CALCULATIONS

- MIN. SHIFT TAPER = $L / 2 = (2.5') \times (35^2) / 60 / 2 = 26' \rightarrow 85'$
- MIN. BIKE TAPER = $L / 3 = (4') \times (20^2) / 60 / 3 = 28' \rightarrow 50'$

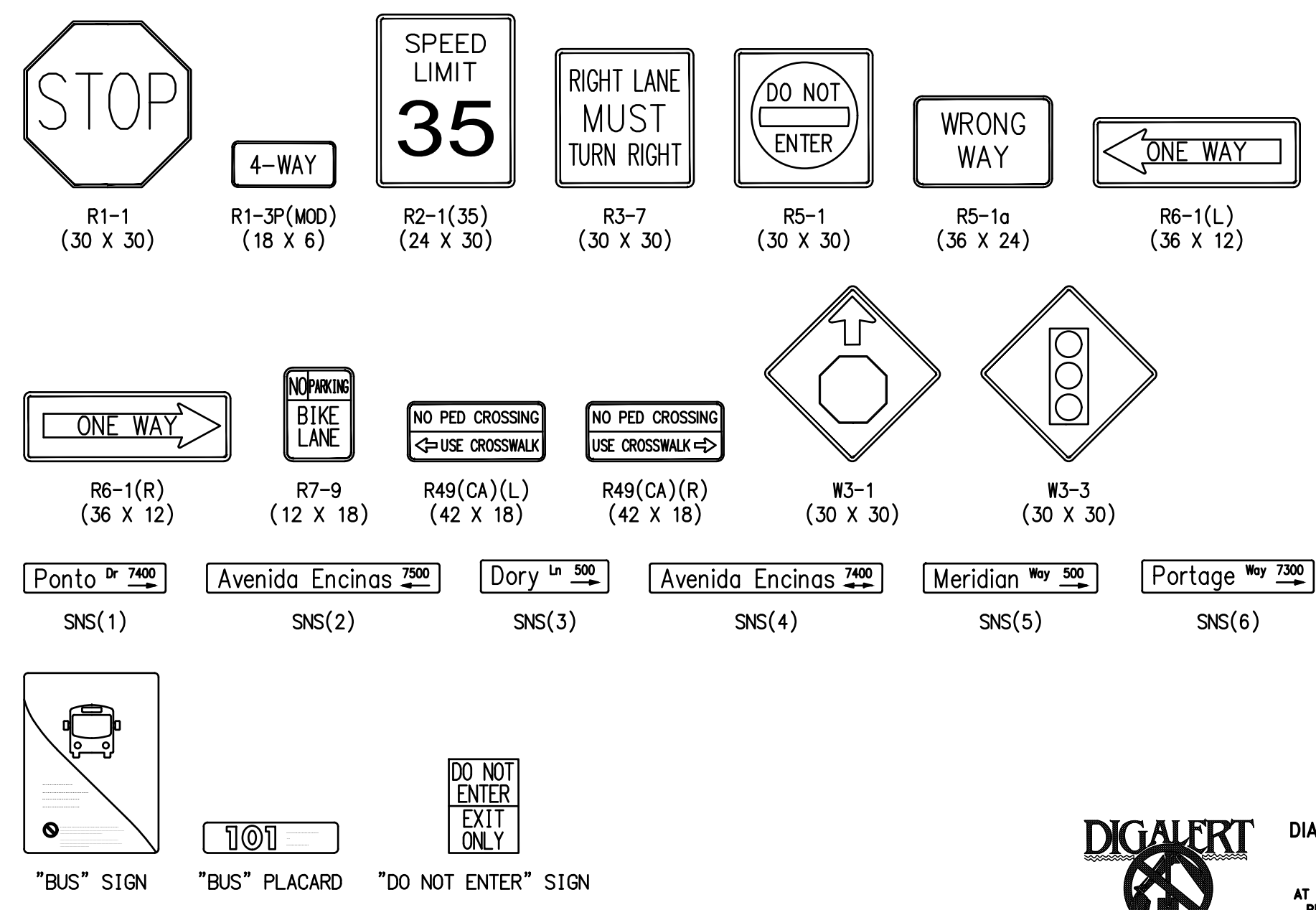
$L =$ LENGTH OF TRANSITION FOR SPEEDS OF 40 MPH OR LESS
 $= W \times S^2 / 60$

$W =$ OFFSET
 $S =$ APPROACH SPEED

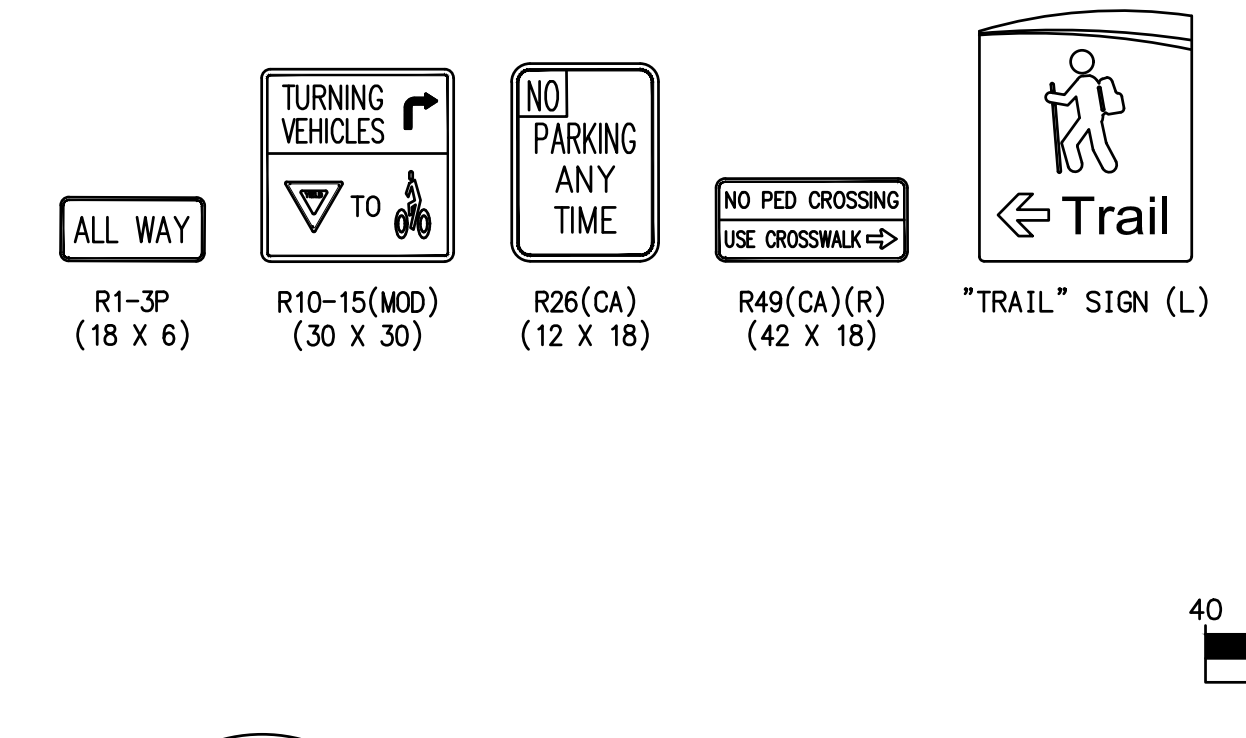
CONSTRUCTION NOTES

- APPLY 12" THERMOPLASTIC LIMIT LINE PER CALTRANS REVISED SP (2018) A24G AND DETAIL "C" ON SHEET 20.
- APPLY WHITE THERMOPLASTIC CONTINENTAL CROSSWALK PER DETAIL "C" ON SHEET 20.
- APPLY 6" WHITE BIKE LANE LINE PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 39.
- APPLY 12" WHITE DIAGONAL HATCHING AT 45 DEGREES AT 40' SPACING. DIAGONAL HATCHING ALONG RED CURB SHALL BE AT 20' SPACING. DIAGONAL HATCHING WITHIN STRIPED MEDIANS SHALL BE YELLOW.
- APPLY 6" WHITE BIKE LANE INTERSECTION LINE PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 39A.
- APPLY GREEN ENNIS-FLINT 985206 GREEN STANDARD FAST DRY WATERBORNE 1952 1/2 TRAFFIC PAINT, PRODUCT CODE 183, PRODUCT COLOR GREEN (34108) WITH REFLECTIVE GLASS BEADS PER DETAIL "A" OR DETAIL "B" ON SHEET 20.
- APPLY 6" DOUBLE YELLOW TWO-DIRECTION NO-PASSING LINE PER CA-MUTCD FIGURE 3A-104 (CA), DETAIL 22.
- APPLY 6" DOUBLE YELLOW MEDIAN ISLAND STRIPING PER CA-MUTCD FIGURE 3A-107 (CA), DETAIL 29.
- APPLY 6" WHITE LANE LINE PER CA-MUTCD FIGURE 3A-102 (CA), DETAIL 8.
- APPLY 8" WHITE CHANNELIZING LINE PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 38.
- APPLY RED PAINT TO CURB.
- APPLY WHITE THERMOPLASTIC "BIKE LANE" AND ARROW PAVEMENT MARKINGS PER CA-MUTCD FIGURE 3B-23 (CA) AND FIGURE 9C-3. MARKING SIZE SHALL BE PER FIGURE 9C-3.
- APPLY WHITE THERMOPLASTIC PAVEMENT MARKING WORD PER CA-MUTCD FIGURE 3B-23. WORD PER PLAN. "STOP" PAVEMENT MARKING AT LIMIT LINES SHALL BE 8' FROM LIMIT LINE.
- APPLY WHITE THERMOPLASTIC PAVEMENT MARKING ARROW PER CA-MUTCD FIGURE 3B-24. ARROW PER PLAN.
- REMOVE EXISTING SIGN FROM POST OR STREET LIGHT POLE. SIGN PER PLAN.
- REMOVE EXISTING SIGN AND POST. SIGN PER PLAN. REPAIR EXISTING SURFACE IN KIND.
- FURNISH AND INSTALL NEW SIGN(S) ON NEW POST. BOTTOM OF LOWEST SIGN SHALL BE MINIMUM 7' FROM FINISHED SURFACE. SIGN(S) PER PLAN.
- FURNISH AND INSTALL NEW SIGN ON EXISTING POST OR STREET LIGHT POLE. BOTTOM OF LOWEST SIGN SHALL BE MINIMUM 7' FROM FINISHED SURFACE. SHIFT SIGNS AS NECESSARY. SIGNS PER PLAN. INSTALL NEW SIGN POST AS NECESSARY.
- FURNISH AND INSTALL NEW TYPE I PEDESTRIAN BARRICADE AND R49(CA)(R) SIGN PER CALTRANS SP (2018) ES-70. REPAIR EXISTING SURFACE IN KIND.
- APPLY 4" WHITE LANE LINE EXTENSION PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 40.
- REMOVE EXISTING CONFLICTING STRIPING OUTSIDE OF LIMITS OF A.C. PAVEMENT OVERLAY.
- EXISTING STRIPING TO REMAIN.

EXISTING SIGN LEGEND

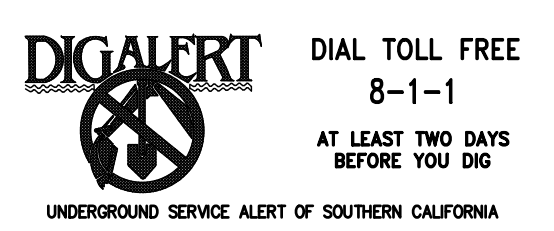
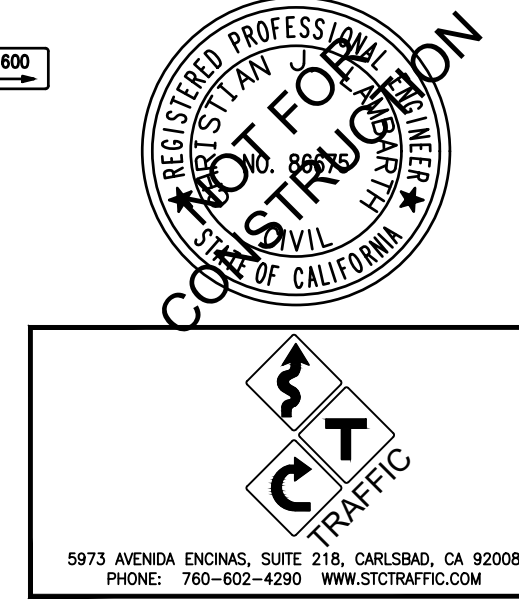
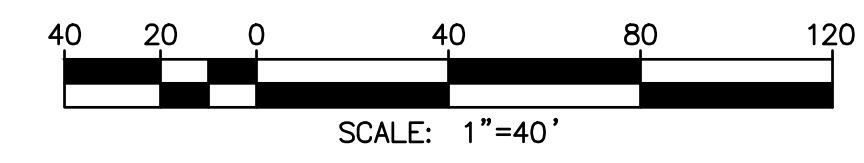


PROPOSED SIGN LEGEND



GENERAL NOTES

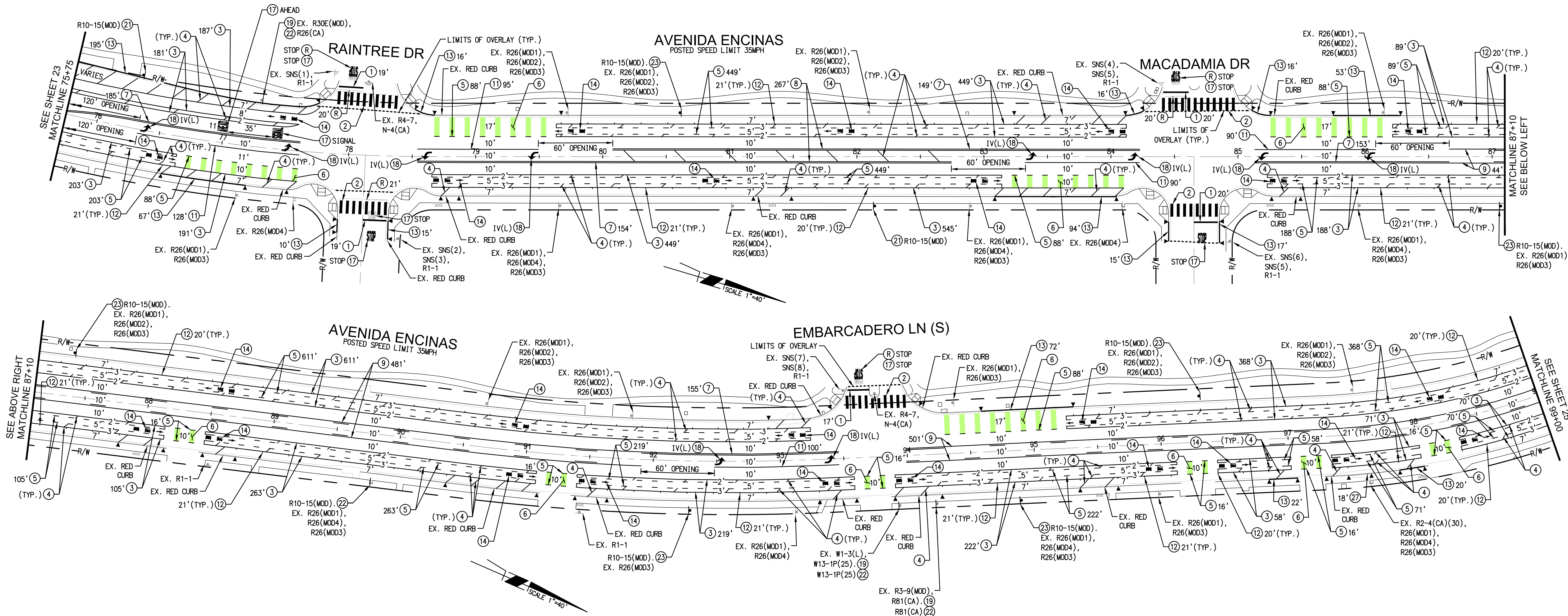
- CONTRACTOR SHALL REFRESH ALL EXISTING PAINTED RED CURB.
- LEGEND**
- X MATCH EXISTING STRIPING INDICATION
 - ▼ START/END CURB PAINT INDICATION



SHEET 21		CITY OF CARLSBAD ENGINEERING DEPARTMENT		SHEETS 29	
IMPROVEMENT PLANS FOR: AVENIDA ENCINAS CRT IMPROVEMENTS SIGNING AND STRIPING PLAN					
APPROVED THOMAS FRANK					
TRANSPORTATION DIRECTOR RCE 49070 XX/XX/XX DATE					
DWN BY: GP		PROJECT NO. XXXX		DRAWING NO. 536-6	
CHKD BY: CL					
RWD BY: CL					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					

Jul 29, 2022 4:14pm R:\Shared\With Me\STC Data\Projects\Nasim\220920-Carlsbad Avenida Encinas CRT\07_Engineering\CADD\DLV\21_0920_DIV_21_LS.dwg
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GENERAL NOTES

- CONTRACTOR SHALL REFRESH ALL EXISTING PAINTED RED CURB.

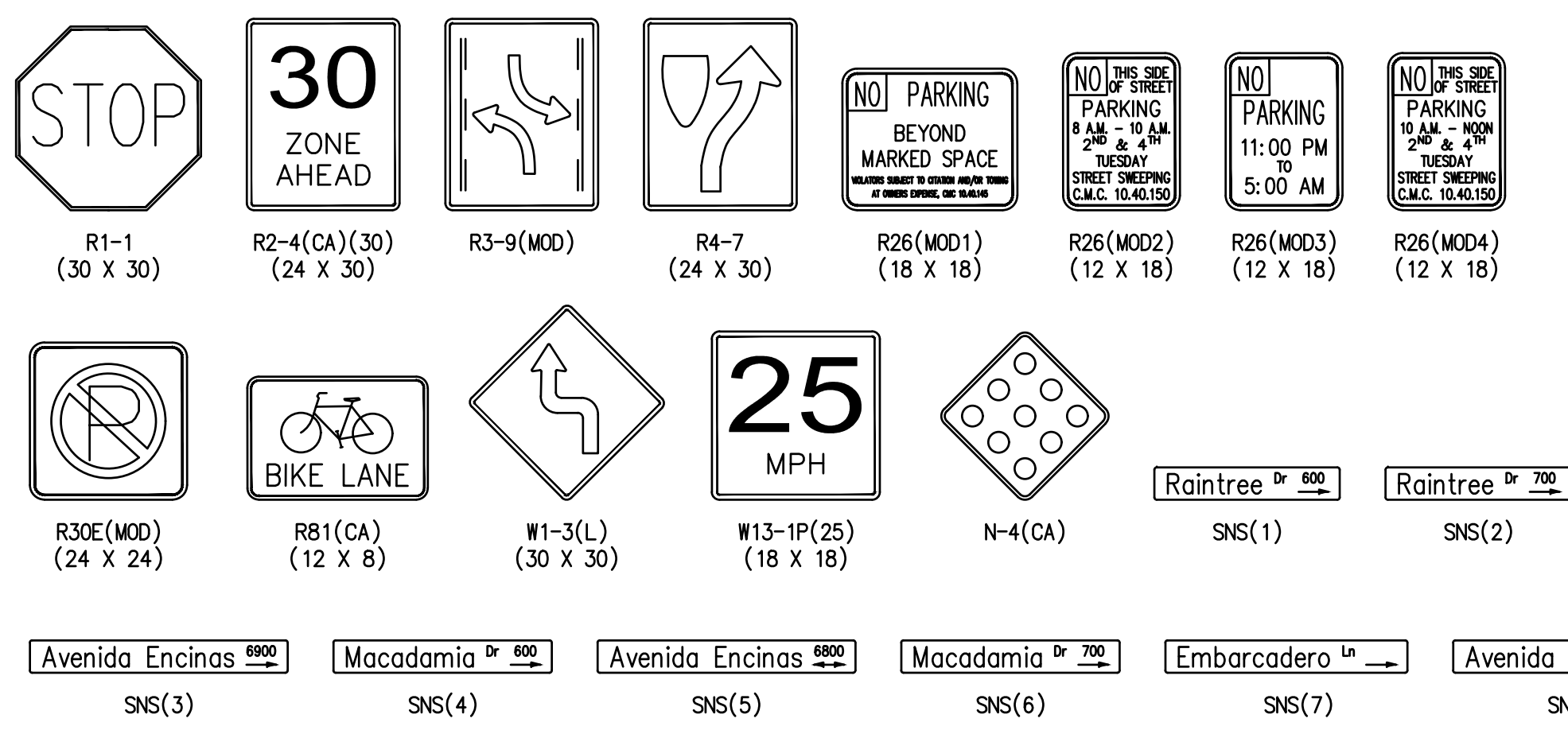
LEGEND

- × MATCH EXISTING STRIPING INDICATION
- ▼ START/END CURB PAINT INDICATION

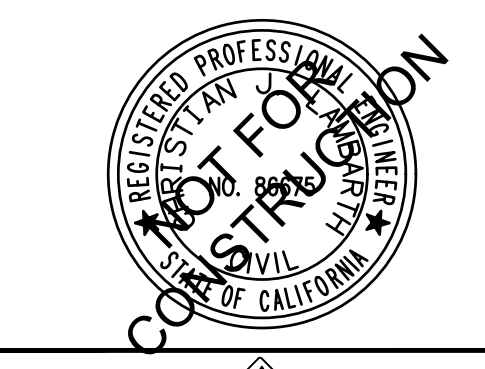
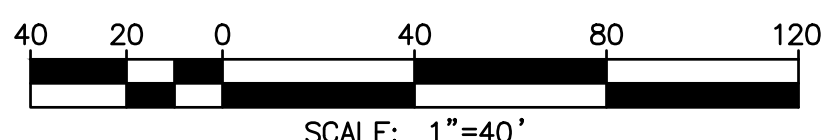
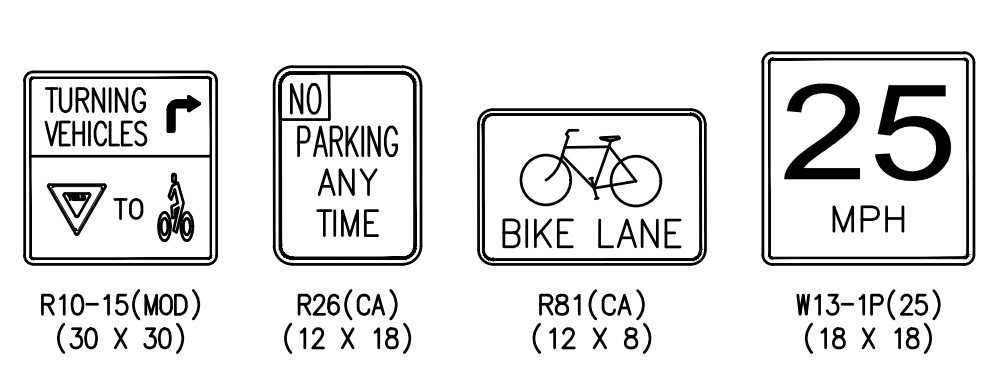
CONSTRUCTION NOTES

- APPLY 12" THERMOPLASTIC LIMIT LINE PER CALTRANS REVISED SP (2018) A246 AND DETAIL "C" ON SHEET 20.
- APPLY WHITE THERMOPLASTIC CONTINENTAL CROSSWALK PER DETAIL "C" ON SHEET 20.
- APPLY 6" WHITE BIKE LANE LINE PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 39.
- APPLY 12" WHITE DIAGONAL HATCHING AT 45 DEGREES AT 40' SPACING. DIAGONAL HATCHING ALONG RED CURB SHALL BE AT 20' SPACING. DIAGONAL HATCHING WITHIN STRIPED MEDIANS SHALL BE YELLOW.
- APPLY 6" WHITE BIKE LANE INTERSECTION LINE PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 39A.
- APPLY GREEN ENNIS-FLINT 985206 GREEN STANDARD FAST DRY WATERBORNE 1552 1/2 TRAFFIC PAINT, PRODUCT CODE 183, PRODUCT COLOR GREEN (34108) WITH REFLECTIVE GLASS BEADS PER DETAIL "A" OR DETAIL "B" ON SHEET 20.
- APPLY 6" DOUBLE YELLOW TWO-DIRECTION NO-PASSING LINE PER CA-MUTCD FIGURE 3A-104 (CA), DETAIL 22.
- APPLY 6" DOUBLE YELLOW MEDIAN ISLAND STRIPING PER CA-MUTCD FIGURE 3A-107 (CA), DETAIL 29.
- APPLY DOUBLE YELLOW TWO-WAY LEFT TURN LANE STRIPING PER CA-MUTCD FIGURE 3A-108 (CA), DETAIL 32.
- APPLY 8" WHITE CHANNELIZING LINE PER CA-MUTCD FIGURE 3A-112 (CA), DETAIL 38.
- APPLY 4" PARKING STALL MARKINGS PER DETAIL "D" ON SHEET 20. SPACING PER PLAN.
- APPLY RED PAINT TO CURB.
- APPLY WHITE THERMOPLASTIC "BIKE LANE" AND ARROW PAVEMENT MARKINGS PER CA-MUTCD FIGURE 3B-23 (CA) AND FIGURE 9C-3. MARKING SIZE SHALL BE PER FIGURE 9C-3.
- APPLY WHITE THERMOPLASTIC PAVEMENT MARKING WORD PER CA-MUTCD FIGURE 3B-23. WORD PER PLAN. "STOP" PAVEMENT MARKING AT LIMIT LINES SHALL BE 8' FROM LIMIT LINE.
- APPLY WHITE THERMOPLASTIC PAVEMENT MARKING ARROW PER CA-MUTCD FIGURE 3B-24. ARROW PER PLAN.
- REMOVE EXISTING SIGN FROM POST OR STREET LIGHT POLE. SIGN PER PLAN.
- FURNISH AND INSTALL NEW SIGN(S) ON NEW POST. BOTTOM OF LOWEST SIGN SHALL BE MINIMUM 7' FROM FINISHED SURFACE. SIGN(S) PER PLAN.
- FURNISH AND INSTALL NEW SIGN ON EXISTING POST OR STREET LIGHT POLE. BOTTOM OF LOWEST SIGN SHALL BE MINIMUM 7' FROM FINISHED SURFACE. SHIFT SIGNS AS NECESSARY. SIGNS PER PLAN. INSTALL NEW SIGN POST AS NECESSARY.
- FURNISH AND INSTALL NEW SIGN AND POST. RELOCATE EXISTING SIGN(S) FROM OLD POST TO NEW. BOTTOM OF LOWEST SIGN SHALL BE MINIMUM 7' FROM FINISHED SURFACE. SIGNS PER PLAN.
- REMOVE EXISTING RED CURB PAINT.
- REMOVE EXISTING CONFLICTING STRIPING OUTSIDE OF LIMITS OF A.C. PAVEMENT OVERLAY.

EXISTING SIGN LEGEND



PROPOSED SIGN LEGEND

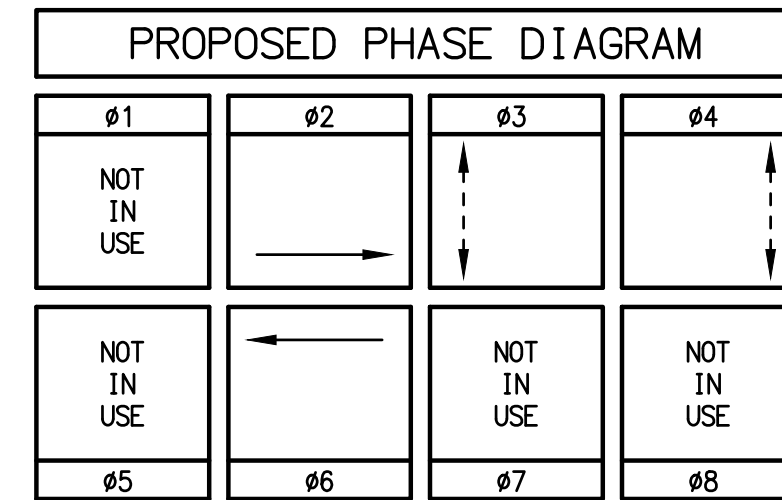


DATE	INITIAL	REVISION DESCRIPTION	DATE	INITIAL	OTHER APPROVAL	DATE	INITIAL	CITY APPROVAL

SHEET 24	CITY OF CARLSBAD ENGINEERING DEPARTMENT	SHEETS 29
IMPROVEMENT PLANS FOR: AVENIDA ENCINAS CRT IMPROVEMENTS SIGNING AND STRIPING PLAN		
APPROVED THOMAS FRANK		
TRANSPORTATION DIRECTOR	RCE 49070 XX/XX/XX	DATE
DWN BY: GP	PROJECT NO. XXXX	DRAWING NO. 536-6
CHKD BY: CL		
RWMD BY: CL		

CONDUCTOR SCHEDULE		CONDUIT RUNS					
AWG OR CABLE SIZE	POLE OR CIRCUIT	1	2	3	4	5	6
12 CSC	POLE A #2, #3P, #3PPB				1	1	1
	POLE B #3P, #3PPB			1	1	1	1
	POLE C #4P, #4PPB			1	1	1	1
	POLE D #6, #4P			1	1	1	1
3 CSC	POLE E #4PPB			1	1	1	1
				1	1	1	1
TOTAL 12 CSC / 3 CSC				3	3	4	4
#10	SAFETY LIGHTING (LTG)		2	2	2	2	
#8	GROUND (GND)	1	1	1	2	2	3
DLC TYPE B	#2					1	1
	#6		3	3	3	3	3
TOTAL DLCS			3	3	3	4	4
EVP DETECTORS (MODEL 138 CABLE)			1	1	1	2	2
CONDUIT SIZE (INCHES)		3"	3"	3"	2-3"	2-3"	3-3"
CONDUIT FILL (%)		8	14	21	16	21	14

● ALL CONDUIT AND CONDUCTORS ARE NEW.



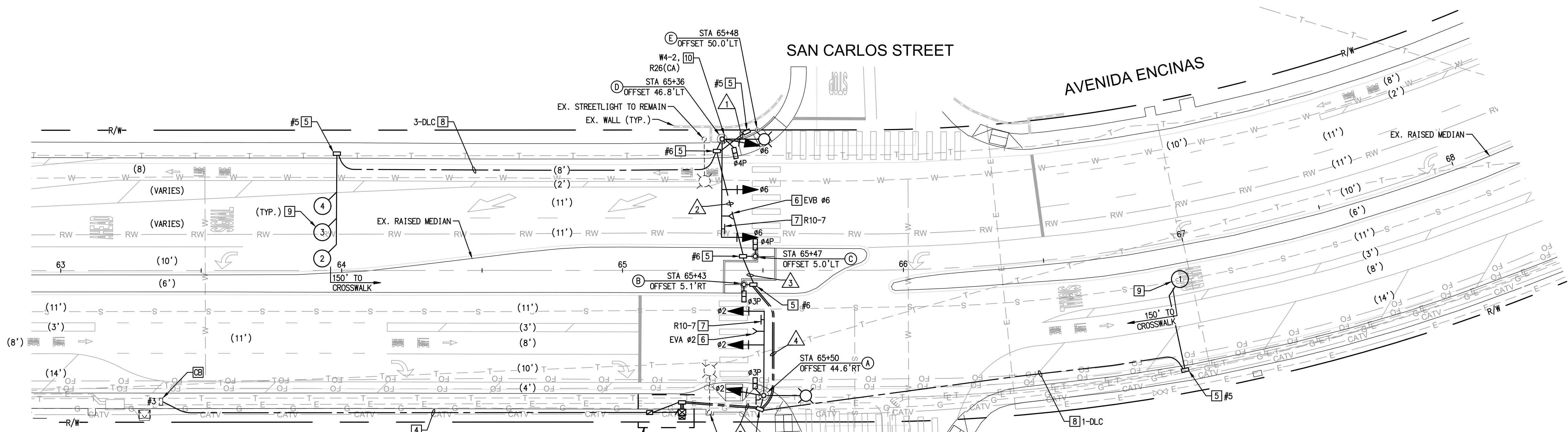
AVENIDA ENCINAS POSTED SPEED = 35 MPH

POLE AND EQUIPMENT SCHEDULE												
No.	STANDARD				LED LUMINAIRE	SIG MTG			POLE LOCATION		SNS	REMARKS
	TYPE	HEIGHT	SIG. M.A.	LUM. M.A.		VEHICLE	PEDESTRIAN	PHASE	QUAD	A		
(A)	19-4-100	30'	30'	15'	120W	2-MAS	SV-1-T	SP-1-T	#3P	4	SEE PLAN	F = 12'
(B)	1-A	10'	-	-	-	-	-	SP-1-T	#3P	4	SEE PLAN	
(C)	1-A	10'	-	-	-	-	-	SP-1-T	#4P	2	SEE PLAN	
(D)	24-4-100	30'	35'	15'	120W	2-MAS	SV-1-T	SP-1-T	-	-	SEE PLAN	
(E)	PPB POST	5'-7"	-	-	-	-	-	-	#4P	2	SEE PLAN	F = 17'

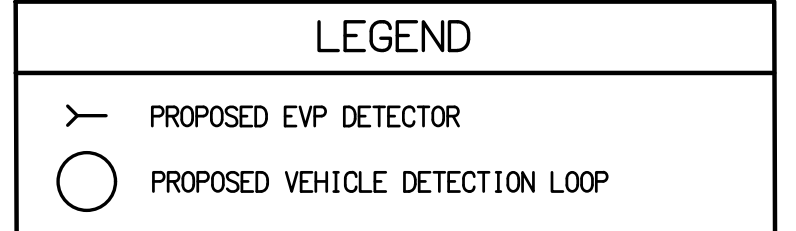
● ALL EQUIPMENT IS NEW.
 (■) = LUMINAIRE MAST ARM TO BE ROTATED 90 DEGREES AS SHOWN.
 (◆) = PEDESTRIAN INDICATIONS SHALL BE LED COUNTDOWN TYPE.
 (▲) = PEDESTRIAN PUSH BUTTONS SHALL BE POLARA APS TYPE.
 (▼) = LED LUMINAIRE SHALL BE GE ERL2-0-16-C3-40-D-GRAY-LY (1600 LUMENS).

AS FIRST ORDER OF WORK, THE CONTRACTOR SHALL POTHOLE POLE LOCATIONS PRIOR TO ORDERING POLES. IF CONFLICTS ARE FOUND DURING POTHOLING, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE CITY. FAILURE TO COMPLY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR FOR ANY LOSS OF TIME, ADDITIONAL COST, AND DAMAGE.

NOTE: CONSTRUCT POLE STANDARDS TO CALTRANS 2018 STANDARDS. CONTRACTOR SHALL SUBMIT MATERIALS CUT SHEETS PRIOR TO ORDERING OF MATERIALS.



DETECTOR ASSIGNMENT				
PHASE	DETECTOR	INPUT ASSEMBLY	FITA	
#2 QUEUE	1	IA1 - I/O 8	FITA1, FIT2, 1-2	
#6 QUEUE	2	IA2 - I/O 9	FITA2, FIT2, 6-7	
#6 QUEUE	3	IA2 - I/O 10	FITA2, FIT3, 1-2	
#6 QUEUE	4	IA2 - I/O 16	FITA2, FIT6, 1-2	
#3P	PPB	IA1 - I/O 26	FITA1, FIT11, 1-2	
#4P	PPB	IA1 - I/O 28	FITA1, FIT12, 1-2	
#2	EVA	IA2 - I/O 24	FITA2, FIT10, 1-2-5	
#6	EVC	IA2 - I/O 25	FITA2, FIT10, 6-7-10	

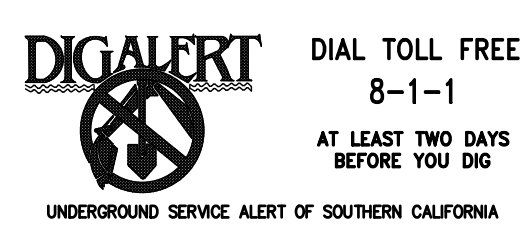
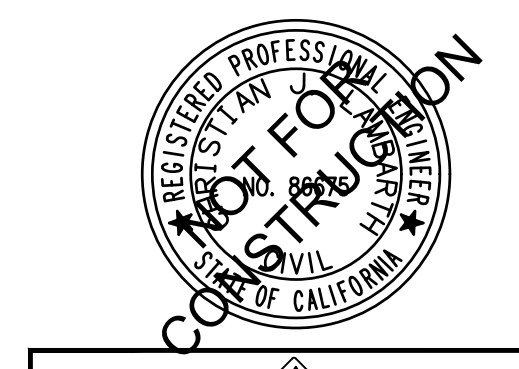
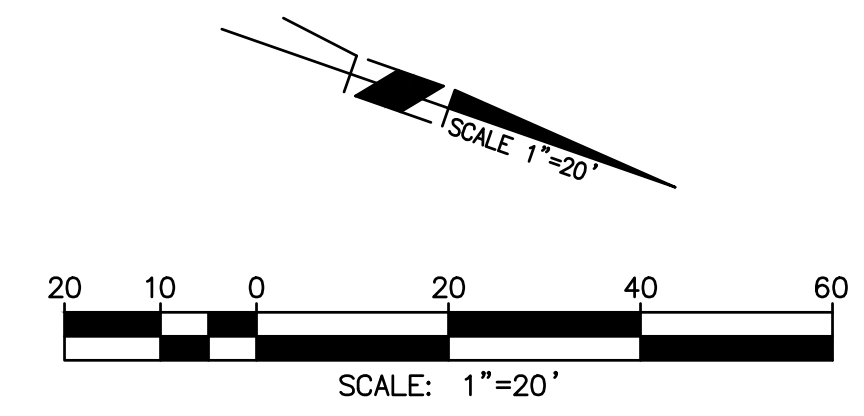
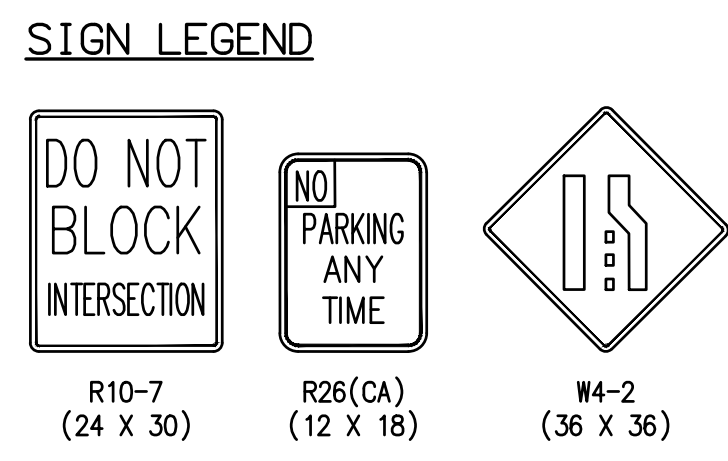


GENERAL NOTES

- ALL EXISTING P.C.C. CURB, GUTTER, AND SIDEWALK IMPACTED BY CONSTRUCTION ASSOCIATED WITH THIS PLAN SHALL BE REPLACED TO THE NEAREST EXISTING JOINTS TO THE SATISFACTION OF THE CITY ENGINEER.
- TRENCHING SHALL BE PERFORMED IN ACCORDANCE WITH CITY OF CARLSBAD SUPPLEMENTAL STANDARD NO. GS-24.
- CONTRACTOR SHALL COORDINATE WITH CITY FOR TREE REMOVAL NECESSARY FOR INSTALLATION OF POLE (D).

CONSTRUCTION NOTES

- FURNISH AND INSTALL 3521 ATC CABINET ON NEW FOUNDATION PER CALTRANS SP (2018) ES-3C. CABINET SHALL HAVE TWO INPUT FILES, A 16 CHANNEL OUTPUT FILE, INTERNAL LED LIGHTS, DOOR SWITCHES, PULL-OUT DOCUMENT DRAWER, AND CITY SPECIFIED LOCKS. CABINET SHALL BE EQUIPPED WITH THE FOLLOWING (OR CITY APPROVED EQUAL):
 - TRAFFICWARE MODEL 2070 ATC CONTROLLER WITH SCOUT FIRMWARE
 - EDI CMU1p-2212HV CONFLICT MONITOR
 - EDI AUXILIARY DISPLAY UNIT FOR ATC CABINET
 - OPTICOM 764 EVP DISCRIMINATOR MODULE
 - POLICE PANEL PICKLESWITCH
 - APS CONTROL MODULE
 - ETHERWAN EX778800 FIELD ETHERNET SWITCH (PER FIBER OPTIC COMMUNICATION PLAN)
- FURNISH AND INSTALL 120/240 VOLT DUAL METER TYPE 111-CF SERVICE PEDESTAL WITH FOUNDATION COMPLETE PER CALTRANS SP (2018) ES-2F. CONTRACTOR SHALL COORDINATE WITH SDG&E FOR SERVICE POINT WELL IN ADVANCE OF NEED. PEDESTAL SHALL BE EQUIPPED WITH TYPE V PHOTOELECTRIC CONTROL UNIT, "PLUG-IN" TYPE BREAKERS AND INCLUDE A 1P-50A TRAFFIC SIGNAL BREAKER (METERED) AND 1P-30A SAFETY LIGHT BREAKER (UN-METERED).
- FURNISH AND INSTALL ALPHA SE48-2216 EXTERNALLY SIDE MOUNTED BATTERY BACK-UP SYSTEM (BBS) WITH GENERATOR PLUG ASSEMBLY, MANUAL BYPASS SWITCH, AND ALPHAGUARD BATTERY CHARGE MANAGEMENT SYSTEM.
- FURNISH AND INSTALL CONDUIT WITH ELECTRICAL CONDUCTORS AND MULE TAPE PER SDG&E REQUIREMENTS. COORDINATE SERVICE POINT WITH SDG&E PLANNER.
- FURNISH AND INSTALL PULL BOX. SIZE PER PLAN.
- FURNISH AND INSTALL OPTICOM MODEL 721 EMERGENCY VEHICLE DETECTOR PER CITY OF CARLSBAD SPECIFICATIONS.
- FURNISH AND INSTALL SIGN ON MAST ARM PER CALTRANS SP (2018) ES-7N, DETAIL "U". SIGN PER PLAN.
- FURNISH AND INSTALL 3" SCHEDULE 80 PVC CONDUIT WITH MULE TAPE. CONDUCTORS PER PLAN.
- FURNISH AND INSTALL TYPE E VEHICLE DETECTOR LOOP PER CALTRANS REVISED SP (2018) ES-5B. LOOPS SHALL BE PLACED IN CENTER OF LANE.
- FURNISH AND INSTALL SIGNS ON TRAFFIC SIGNAL POLE. BOTTOM OF LOWEST SIGN SHALL BE MINIMUM 7' FROM FINISHED SURFACE. SIGN PER PLAN.



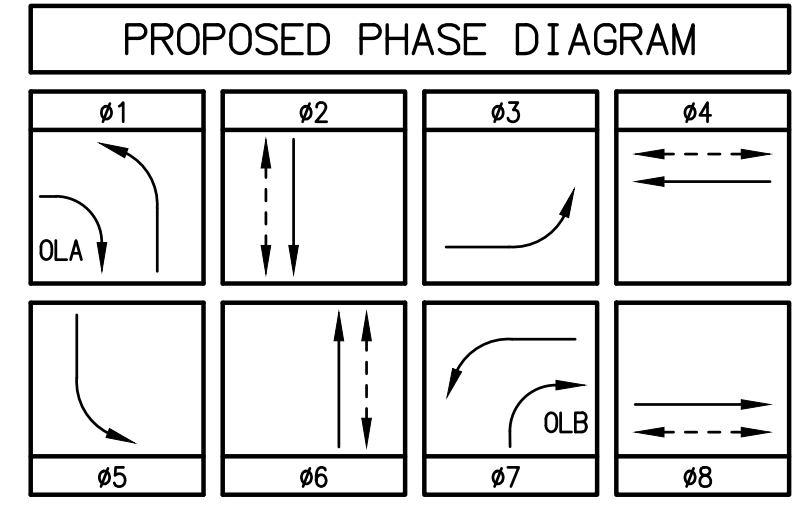
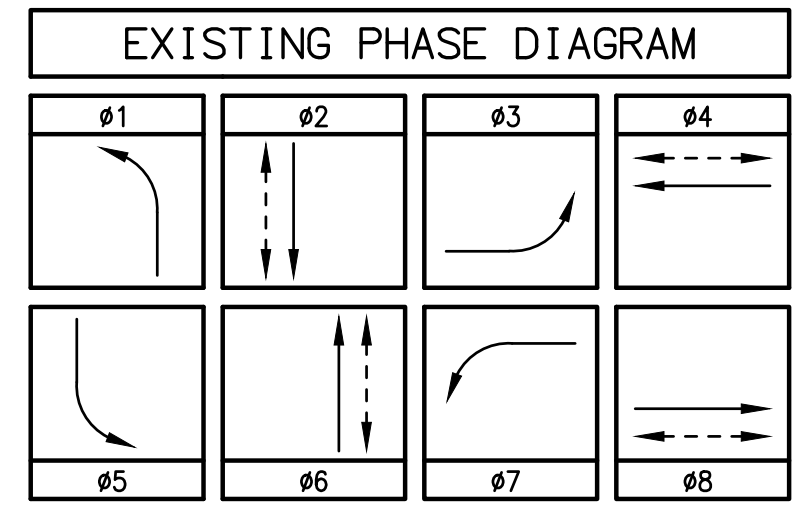
5973 AVENIDA ENCINAS, SUITE 218, CARLSBAD, CA 92008
 PHONE: 760-602-4290 WWW.STCITRAFFIC.COM

SHEET 26		CITY OF CARLSBAD ENGINEERING DEPARTMENT		SHEETS 29	
IMPROVEMENT PLANS FOR: AVENIDA ENCINAS CRT IMPROVEMENTS AVENIDA ENCINAS AND SAN CARLOS STREET PEDESTRIAN SIGNAL PLAN					
APPROVED THOMAS FRANK					
TRANSPORTATION DIRECTOR		RCE 49070 XX/XX/XX		DATE	
DWN BY: GP	PROJECT NO. XXXX	DRAWING NO. 536-6			
CHKD BY: CL					
RWMD BY: CL					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					

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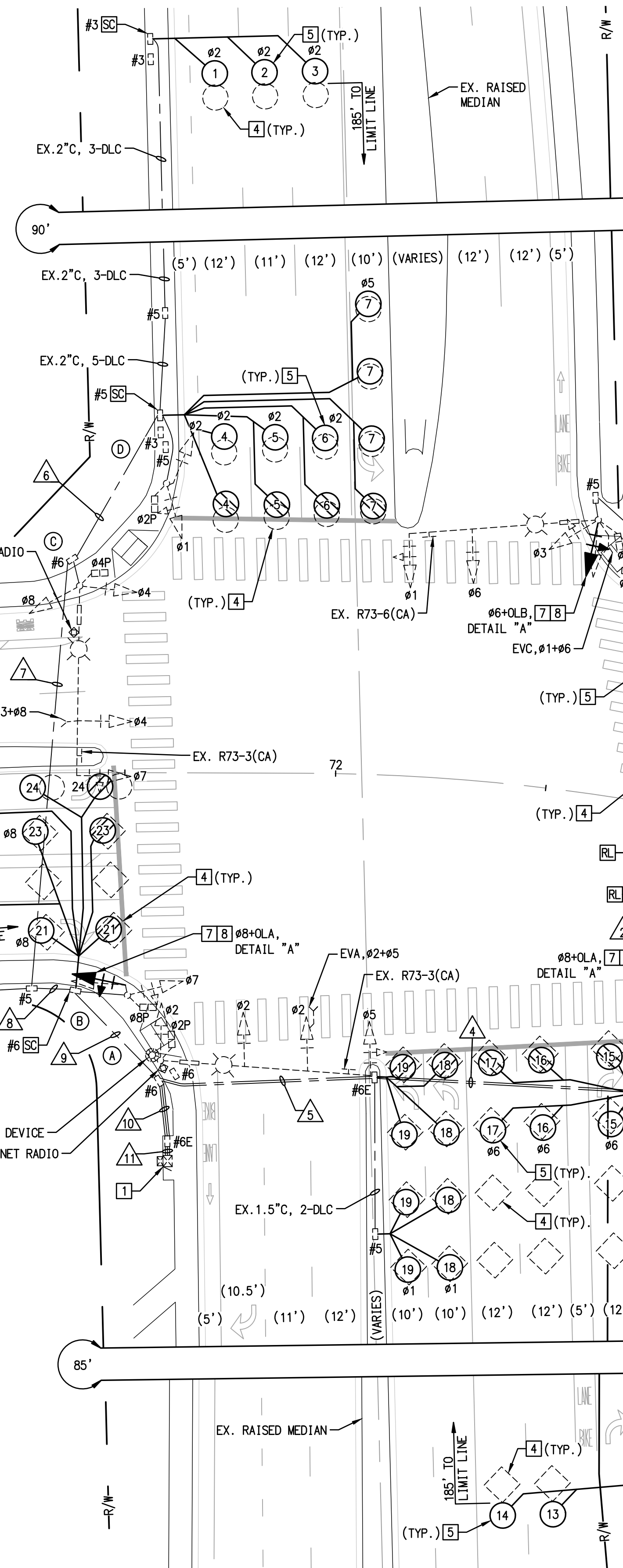
CONDUCTOR SCHEDULE																						
AWG OR CABLE SIZE	POLE OR CIRCUIT	CONDUIT RUNS																				
		1	2	3	4	5	6	7	8	9	10	11										
12 CSC	POLE A	Ø2, Ø5, Ø2P, Ø8PPB																				
	POLE B	Ø7, Ø8+OLA, Ø8P, Ø2PPB																				
	POLE C	Ø4, Ø7, Ø8, Ø4P, Ø2PPB																				
	POLE D	Ø1, Ø2, Ø2P, Ø4PPB																				
	POLE E	Ø1, Ø3, Ø6+OLB, Ø6P, Ø4PPB, Ø6PPB																				
	POLE F	Ø4, Ø4P																				
	POLE G	Ø3, Ø8+OLA, Ø8P, Ø6PPB, Ø8PPB																				
	POLE H	Ø5, Ø6+OLB, Ø6P, Ø8PPB																				
TOTAL 12 CSC / 3 CSC			2	3	4	5	5	1	2	2	3	3	9	9	1							
#8 SAFETY LIGHTING (LTG)			2	2	2	2	2	1	1	1	1	3	3									
#8 GROUND (GND)			1	1	1	2	2	1	1	1	1	3	3									
#6 SERVICE (SVC)																						
DLC TYPE B	Ø1																					
	Ø2							6	6	6	6	6	6									
	Ø3													1	1	1						
	Ø4																					
	Ø5																					
	Ø6																					
	Ø7																					
	Ø8																					
TOTAL DLCS			5	5	10	12	7	7	10	14	26	26										
EVP DETECTORS (MODEL 138 CABLE)			1	1	2	2	2		1	1	1	4	4									
CAT6 (RADIO)																						
CAT6 (TRAFFIC MEASURING DEVICE)																						
ALPR POWER CABLE																						
6PR#22 SIC																						
CONDUIT SIZE (INCHES)			2"	3"	3"	2-3"	2-3"	2"	2"	3"	3"	3-3"	3-3"									
CONDUIT FILL (%)			41	30	46	32	33	35	61	33	45	40	40									

* ALL CONDUIT AND CONDUCTORS ARE EXISTING UNLESS OTHERWISE NOTED.
 (●) = NEW
 (+) = SPARE



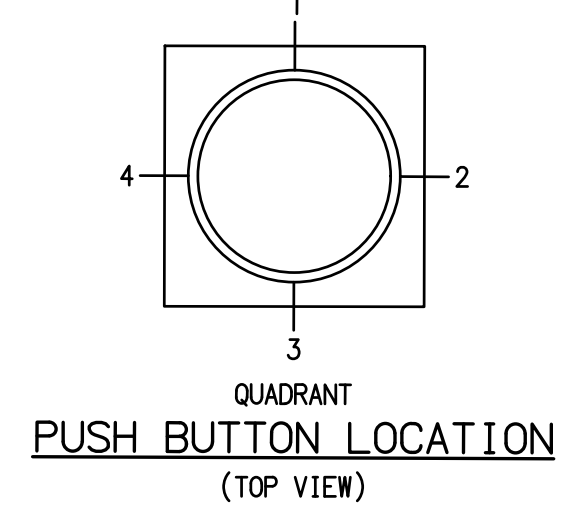
AVENIDA ENCINAS POSTED SPEED = 35 MPH
 POINSETTIA LANE POSTED SPEED = 35 MPH

POINSETTIA LANE



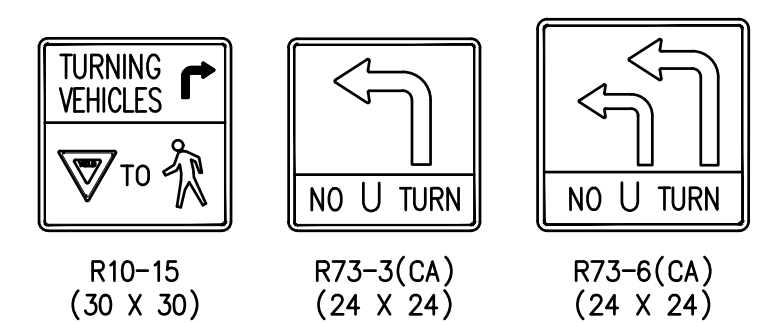
POLE AND EQUIPMENT SCHEDULE													
No.	TYPE	STANDARD			LED LUMINAIRE	SIG MTG VEHICLE	PED	PPB		POLE LOCATION		SNS	REMARKS
		HEIGHT	SIG. M.A.	LUM. M.A.				PHASE	QUAD	A	B		
(A)	29-5-80	30'	50'	15'	120W	3-MAS	SV-1-T	SP-1-T	Ø8P	1	EXISTING	Avenida Encinas	
(B)	1-A	10'	-	-	-	-	TV-2-T	SP-1-T	Ø2P	2	EXISTING	-	
(C)	26-4-80	30'	45'	15'	120W	2-MAS	SV-1-T	SP-1-T	Ø2P	2	EXISTING	Poinsettia Ln	
(D)	1-A	10'	-	-	-	-	TV-2-T	SP-1-T	Ø4P	3	EXISTING	-	
(E)	26-4-80	30'	45'	15'	120W	2-MAS	SV-2-T	SP-1-T	Ø4P Ø6P	3	EXISTING	Avenida Encinas	
(F)	1-A	10'	-	-	-	-	TV-1-T	SP-1-T	-	-	EXISTING	-	
(G)	26-4-80	30'	45'	15'	120W	1-MAS 2-MAS	SV-1-T	SP-1-T	Ø6P Ø8P	4	EXISTING	Poinsettia Ln	F = 20'
(H)	1-A	10'	-	-	-	-	TV-2-T	SP-1-T	Ø8P	1	EXISTING	-	

* ALL EQUIPMENT IS EXISTING UNLESS OTHERWISE NOTED.
 (●) = NEW
 (▲) = RELOCATED



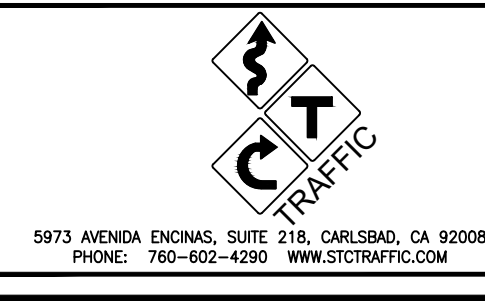
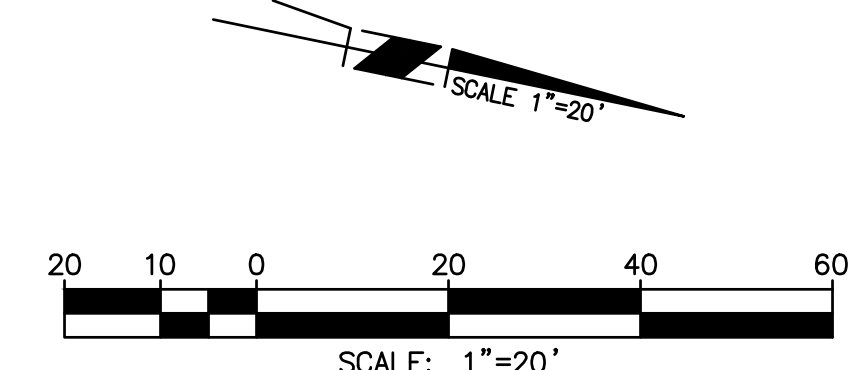
DETECTOR ASSIGNMENT			
DETECTOR	PHASE	SLOT	FIELD TERMINAL
1	2 ADV	J2U	T2 - 5k6
2	2 ADV	J2L	T2 - 7k8
3	2 ADV	J3U	T2 - 9k10
4	2	J3L	T2 - 11k12
5	2	J4U	T4 - 1k2
6	2	J4L	T4 - 3k4
7	5	J1U	T3 - 1k2
8	4 ADV	J6U	T4 - 9k10
9	4	J7L	T6 - 3k4
10	4 BIKE	J8U	T6 - 5k6
11	4	J8L	T6 - 7k8
12	7	J5U	T5 - 5k6
13	6 ADV	J2U	T3 - 5k6
14	6 ADV	J2L	T3 - 7k8
15	6	J3L	T3 - 11k12
16	6	J4U	T5 - 1k2
17	6	J4L	T5 - 3k4
18	1	J1U	T2 - 1k2
19	1	J1L	T2 - 3k4
20	8 ADV	J6U	T5 - 9k10
21	8	J7L	T7 - 3k4
22	8 BIKE	J8U	T7 - 5k6
23	8	J8L	T7 - 7k8
24	3	J5U	T4 - 5k6
PPB	2P	J12U	T8 - 4 & COM 6
PPB	4P	J12L	T8 - 5 & COM 6
PPB	6P	J13U	T8 - 7 & COM 9
PPB	8P	J13L	T8 - 8 & COM 9
EVA	2+5	J12U	T9 - 4 & COM 6
EVb	4+7	J13U	T9 - 7 & COM 9
EVC	1+6	J12L	T9 - 5 & COM 6
EVD	3+8	J13L	T9 - 8 & COM 9
FLASH SENSE	-	J14U	T8 - 10 & COM 12

SIGN LEGEND



CONSTRUCTION NOTES

- EXISTING TYPE 332 CONTROLLER CABINET TO REMAIN WITH THE FOLLOWING EQUIPMENT:
 EXISTING TRAFFICWARE MODEL 2070 TRAFFIC SIGNAL CONTROLLER
 EXISTING SAFETRAN TYPE III PDA
 EXISTING EDI MODEL 2010EOLIP CMU
 EXISTING PoE INJECTOR (WIRELESS ETHERNET RADIO)
 EXISTING PoE INJECTOR (TRAFFIC MEASURING DEVICE)
 ETHERMAN EX778800 FIELD ETHERNET SWITCH (PER FIBER OPTIC COMMUNICATION PLAN)
- EXISTING BATTERY BACK-UP CABINET WITH DIMENSIONS BBS TO REMAIN.
- EXISTING TYPE III SERVICE PEDESTAL TO REMAIN.
- EXISTING LOOPS.
- INSTALL TYPE E VEHICLE DETECTION LOOPS PER CALTRANS REVISED SP (2018) ES-5B. LIMIT LINE LOOPS SHALL BE TYPE F. BIKE LOOPS SHALL BE MODIFIED 4'X6' TYPE O.
- EXISTING 40' TRAFFIC SIGNAL MAST ARM COMPLETE. FURNISH AND INSTALL NEW 45' TRAFFIC SIGNAL MAST ARM ON EXISTING TRAFFIC SIGNAL POLE. EXISTING TRAFFIC SIGNAL EQUIPMENT TO NEW MAST ARM.
- EXISTING VEHICLE HEAD AND MOUNTING COMPLETE.
- FURNISH AND INSTALL NEW VEHICLE HEAD PER PLAN. MOUNTING SHALL BE PER POLE AND EQUIPMENT SCHEDULE.

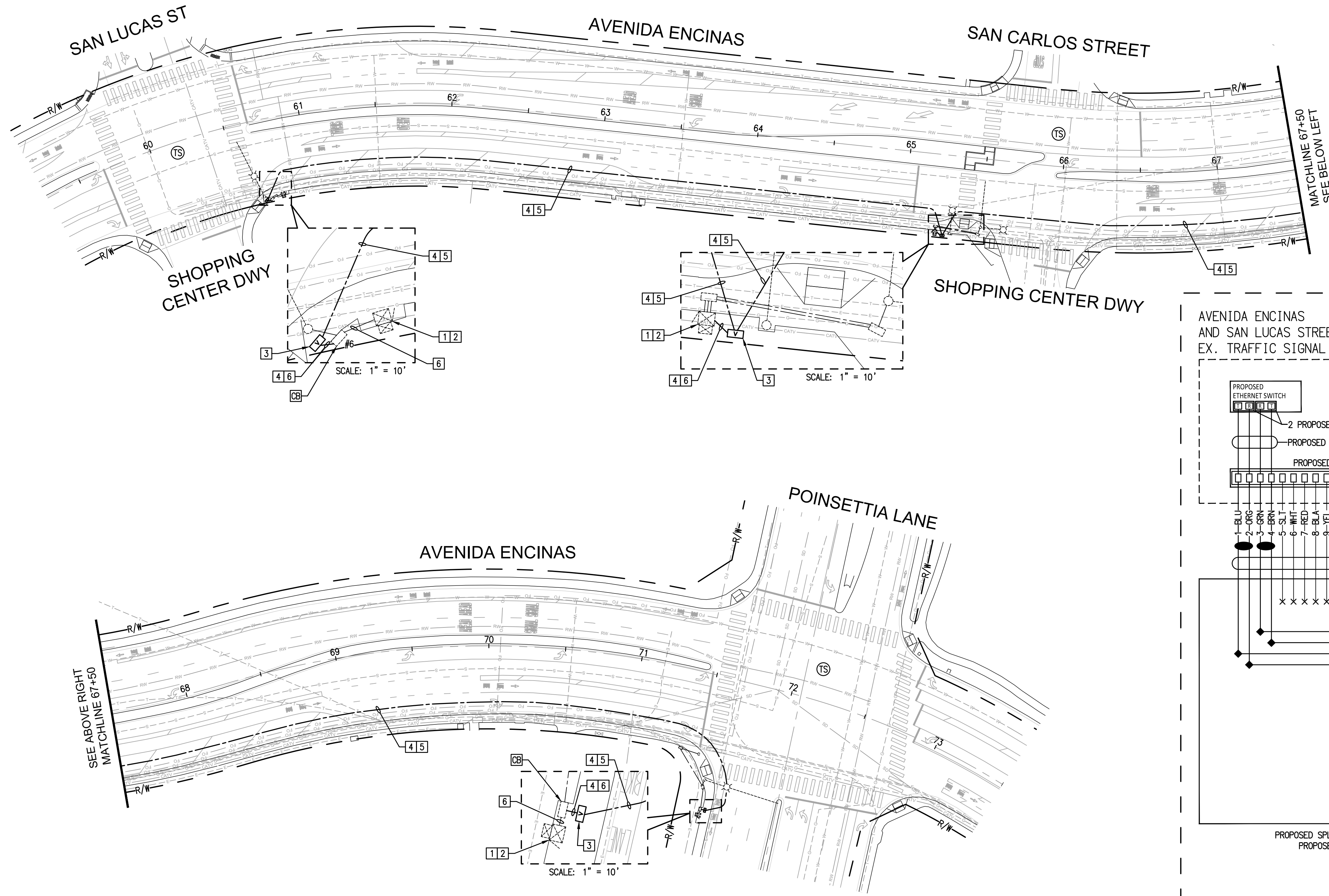


SHEET 27		CITY OF CARLSBAD ENGINEERING DEPARTMENT		SHEETS 29	
IMPROVEMENT PLANS FOR: AVENIDA ENCINAS CRT IMPROVEMENTS AVENIDA ENCINAS AND POINSETTIA LANE TRAFFIC SIGNAL MODIFICATION PLAN					
APPROVED THOMAS FRANK					
TRANSPORTATION DIRECTOR		RCE 49070 XX/XX/XX		DATE	
DWN BY: GP	PROJECT NO. XXXX	DRAWING NO. 536-6			
CHKD BY: CL					
RVWD BY: CL					
DATE	INITIAL	DATE	INITIAL	DATE	INITIAL
ENGINEER OF WORK		OTHER APPROVAL	CITY APPROVAL		
REVISION DESCRIPTION					

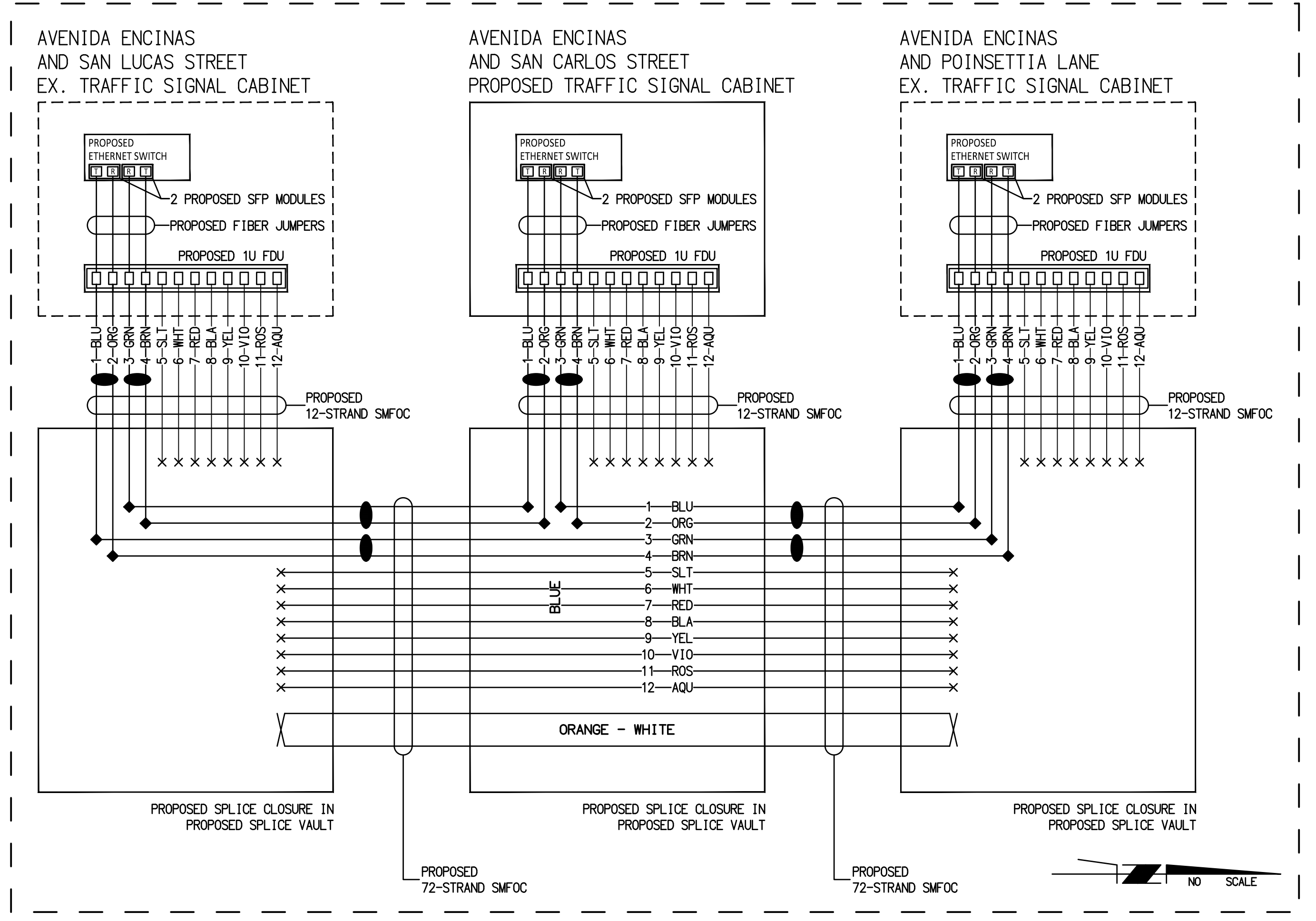
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 UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA



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PROPOSED SPLICE DIAGRAM



GENERAL NOTES

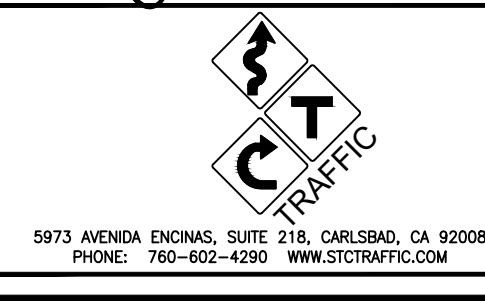
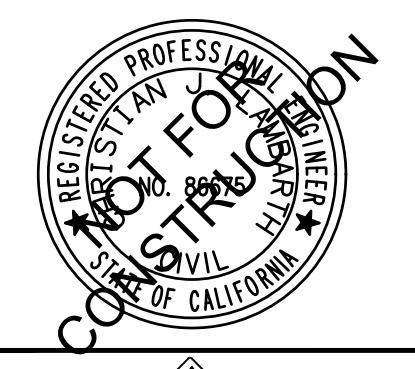
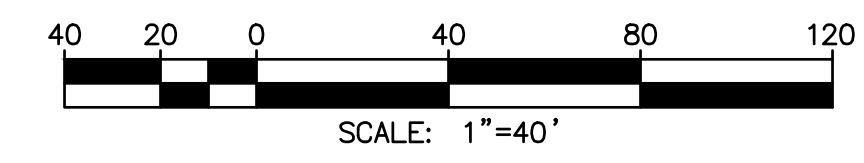
1. ALL EXISTING P.C.C. CURB, GUTTER, AND SIDEWALK IMPACTED BY CONSTRUCTION ASSOCIATED WITH THIS PLAN SHALL BE REPLACED TO THE NEAREST EXISTING JOINTS TO THE SATISFACTION OF THE CITY ENGINEER.
2. TRENCHING SHALL BE PERFORMED IN ACCORDANCE WITH CITY OF CARLSBAD SUPPLEMENTAL STANDARD NO. GS-24.

CONSTRUCTION NOTES

1. EXISTING FIELD ETHERNET SWITCH IF APPLICABLE. FURNISH AND INSTALL ETHERWAN EX7800 FIELD ETHERNET SWITCH OR APPROVED EQUAL WITH 2-DUAL FIBER-TRANSCIVER PORTS AND 2-DUAL SFP MODULES. FURNISH CAT6 CABLES AND JUMPER AS NECESSARY TO CONNECT DEVICES. CONNECTED DEVICES INCLUDE:
 - a. TRAFFIC SIGNAL CONTROLLERS
 - b. CONFLICT MONITOR UNITS
 - c. EVP DISCRIMINATORS
 - d. TRAFFIC MEASURING DEVICE
 - e. BATTERY BACKUP SYSTEMS
 - f. WIRELESS ETHERNET RADIOS
2. FURNISH AND INSTALL FIBER DISTRIBUTION UNIT WITH 1U RACK MOUNT HOUSING AND 12 PORT SLICE CASSETTE. PROVIDE ALL FIBER JUMPERS AND CONNECTORS NECESSARY FOR COMMUNICATION.
3. FURNISH AND INSTALL 36"x36"x36" SPLICE VAULT WITH FIBER OPTIC SPLICE CLOSURE CAPABLE OF HOUSING A MINIMUM OF 72 SPLICES PER DETAIL "E" ON SHEET 20.
4. FURNISH AND INSTALL 3" SCHEDULE 80 PVC CONDUIT WITH 45° CONDUIT SWEEPS FOR FIBER OPTIC CABLE.
5. FURNISH AND INSTALL 72 SMFOC WITH #8 TRACER WIRE AND MULE TAPE.
6. FURNISH AND INSTALL 12 SMFOC WITH #8 TRACER WIRE AND MULE TAPE.

LEGEND

- ◆ - PROPOSED FIBER SPLICE
- × - CUT END OF FIBER, NO SPLICE
- - PROPOSED SIGNAL CARRYING FIBER PAIR



DATE	INITIAL	REVISION DESCRIPTION	DATE	INITIAL	OTHER APPROVAL	DATE	INITIAL	CITY APPROVAL

SHEET 29	CITY OF CARLSBAD ENGINEERING DEPARTMENT	SHEETS 29
IMPROVEMENT PLANS FOR: AVENIDA ENCINAS CRT IMPROVEMENTS FIBER OPTIC COMMUNICATION PLAN		
APPROVED THOMAS FRANK		
TRANSPORTATION DIRECTOR	RCE 49070 XX/XX/XX	DATE
DWN BY: GP	PROJECT NO. XXXX	DRAWING NO. 536-6
CHKD BY: CL		
RVMD BY: CL		



Staff Report

Meeting Date: Aug. 18, 2022
To: Traffic and Mobility Commission
Staff Contact: Brandon Miles, Associate Engineer
Brandon.Miles@carlsbadca.gov, 442-339-2745
Subject: Intersection Improvements at Melrose Drive and Palomar Airport Road

Recommended Action

Support staff's recommendation to approve the plans and specifications of the intersection improvements at Melrose Drive and Palomar Airport Road, Capital Improvement Project No. 6034.

Background

The project is located at the intersection of Melrose Drive and Palomar Airport Road as shown in Exhibit 1. Currently, southbound Melrose Drive has two left-turn lanes, two through travel lanes, a bike lane and two right-turn lanes as the roadway approaches Palomar Airport Road.

On Dec. 17, 2019, the City Council expedited the project's roadway improvements to improve traffic flow of southbound Melrose Drive and waived any right-of-way open space boundary adjustment requirements under General Plan Open Space Element Policy 4-P.6 applicable to Capital Improvement Project, or CIP, Project 6034.

On May 4, 2020, Traffic and Mobility Commission unanimously supported staff's recommendation to implement the project as it provided operational improvements consistent with the city's General Plan Mobility Element.

The project will widen southbound Melrose Drive to include a third southbound through travel lane approaching the intersection with Palomar Airport Road, as shown in Exhibit 2. The ultimate lane configuration for southbound Melrose Drive will include two left-turn lanes, three through travel lanes, enhanced bike lane and two right-turn lanes as the roadway approaches Palomar Airport Road.

The project will be widened into the adjacent ornamental private park area. However, no right-of-way acquisition is needed because the developer who widened Melrose Drive in 2010 recorded an irrevocable offer of dedication in favor of the city for public street and utility purposes pursuant to Government Code section 7050. Later this year, staff will therefore present the City Council with a resolution accepting this irrevocable offer of dedication to accommodate the intersection improvements featured in CIP Project No. 6034.

Additionally, the project will realign the striping through the intersection of Melrose Drive and Palomar Airport Road to provide a smoother transition in the northbound direction. These improvements are expected to improve traffic circulation and operations.

Next Steps

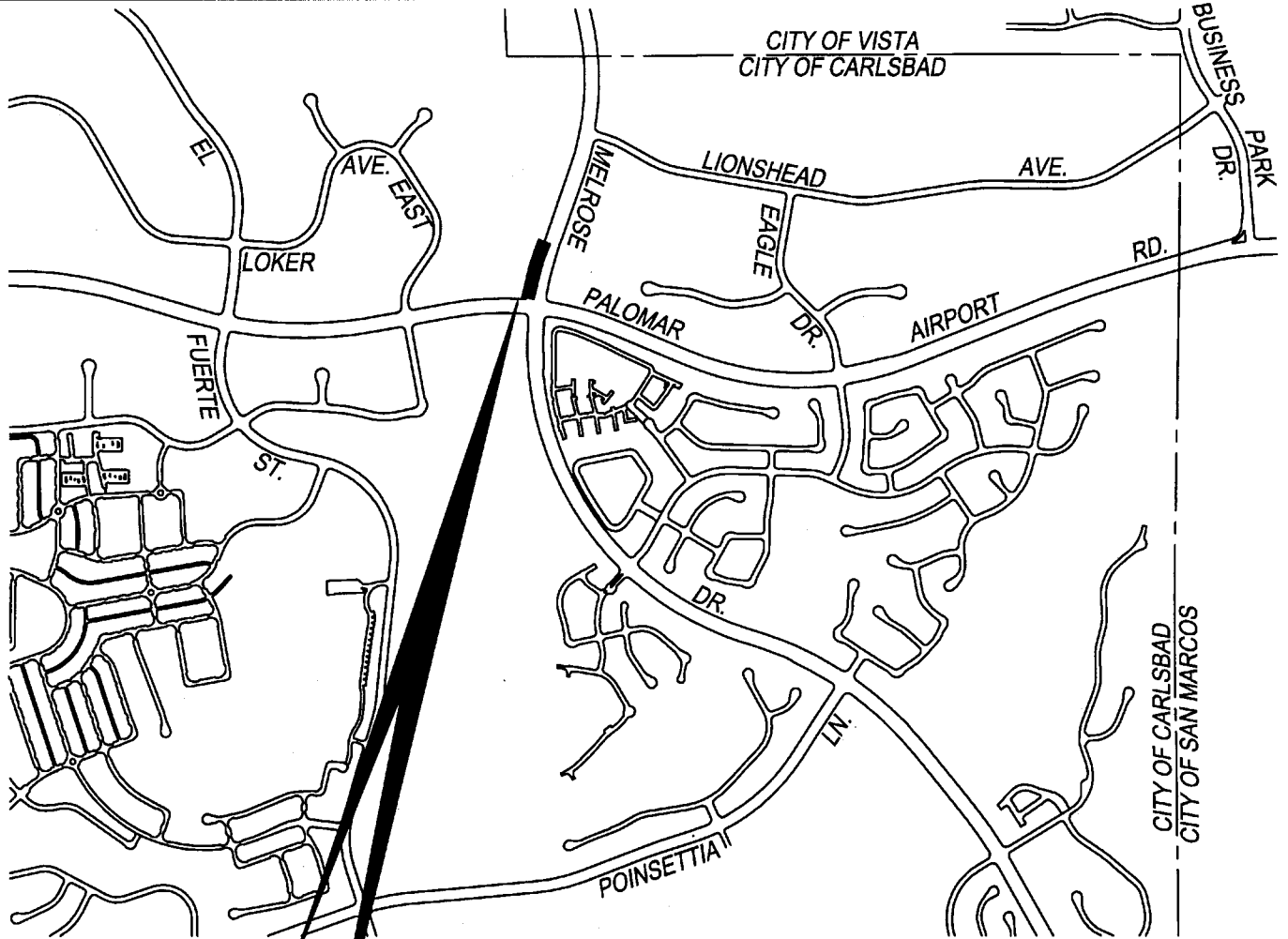
Following the Traffic and Mobility Commission's review, staff will finalize the construction plans for the project and will bring a staff report to the City Council in late 2022 with a recommendation to (1) accept the irrevocable offer of dedication for public street and utility purposes, and (2) approve the plans, specifications and contract documents and authorize the City Clerk to advertise for construction bids for the project.

After the bidding process is complete, staff will return to the City Council to request award a construction contract to the lowest responsive and responsible bidder. The construction is targeted to begin in early 2023.

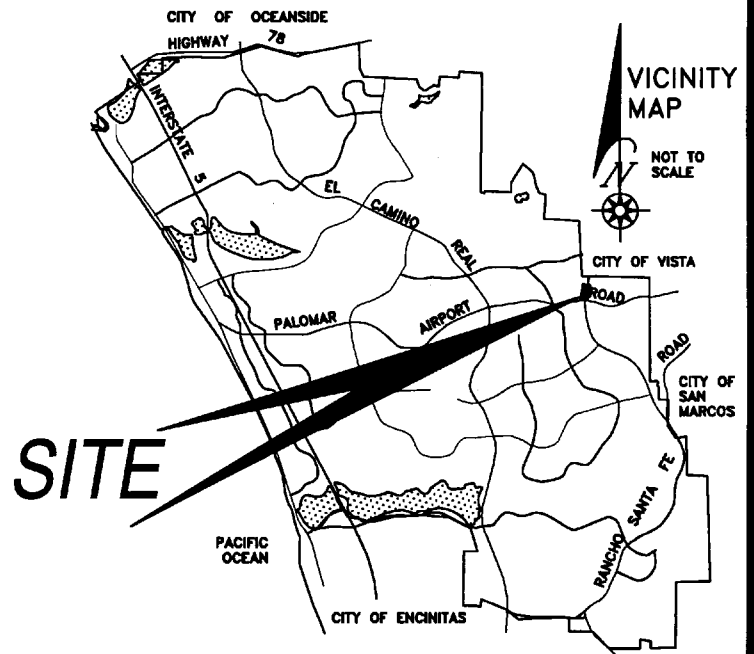
Exhibits

1. Location Map
2. Signing and Striping Plans

LOCATION MAP



SITE



VICINITY MAP

NOT TO SCALE

SITE



NOT TO SCALE

PROJECT NAME
PALOMAR AIRPORT ROAD AND MELROSE DRIVE IMPORVEMENTS

PROJECT NUMBER
6034

EXHIBIT
1

Aug. 18, 2022

Item #2

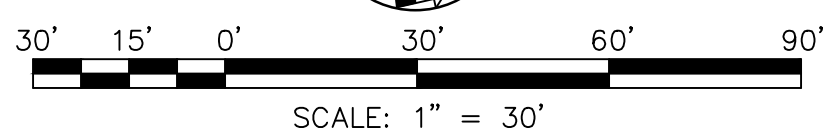
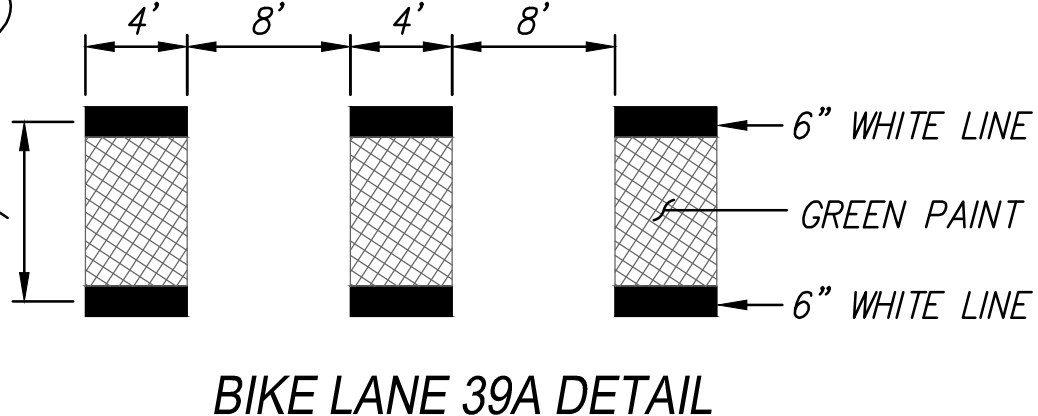
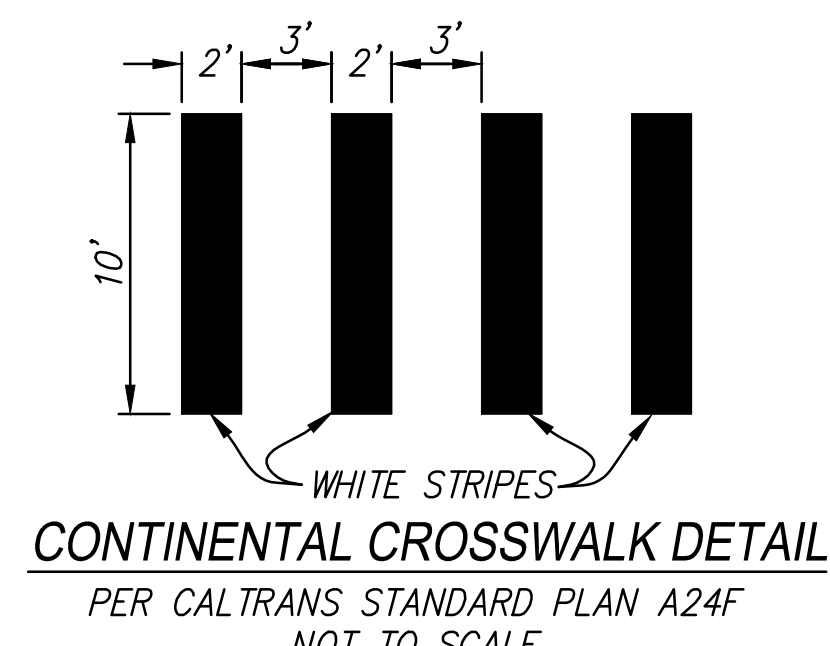
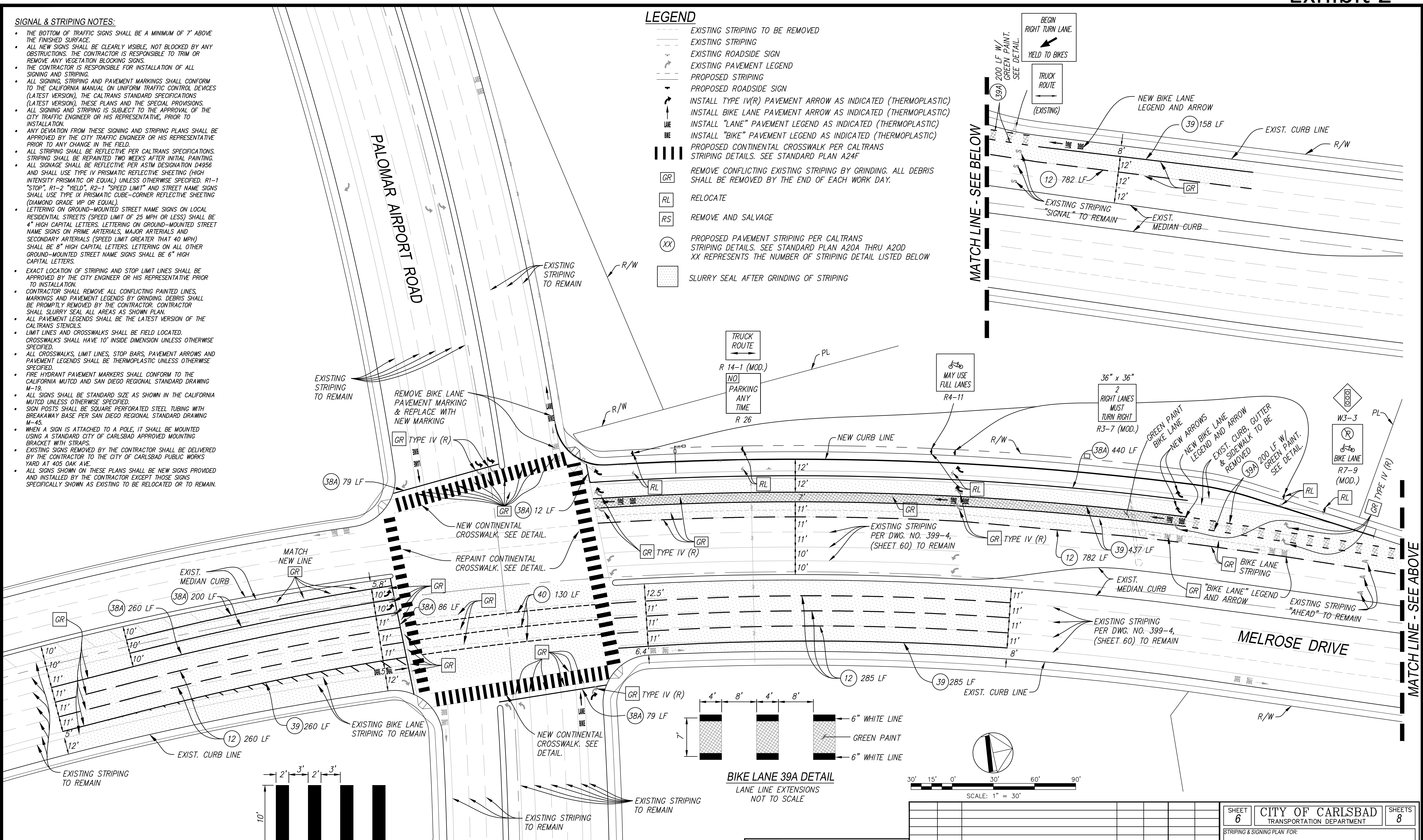
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SIGNAL & STRIPING NOTES:

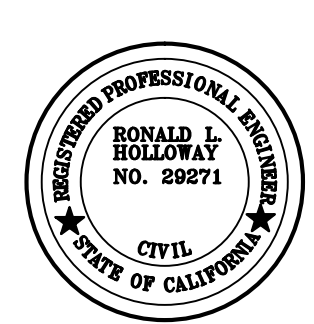
- THE BOTTOM OF TRAFFIC SIGNS SHALL BE A MINIMUM OF 7' ABOVE THE FINISHED SURFACE.
- ALL NEW SIGNS SHALL BE CLEARLY VISIBLE, NOT BLOCKED BY ANY OBSTRUCTIONS. THE CONTRACTOR IS RESPONSIBLE TO TRIM OR REMOVE ANY VEGETATION BLOCKING SIGNS.
- THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ALL SIGNING AND STRIPING.
- ALL SIGNING, STRIPING AND PAVEMENT MARKINGS SHALL CONFORM TO THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (LATEST VERSION), THE CALTRANS STANDARD SPECIFICATIONS (LATEST VERSION), THESE PLANS AND THE SPECIAL PROVISIONS.
- ALL SIGNING AND STRIPING IS SUBJECT TO THE APPROVAL OF THE CITY TRAFFIC ENGINEER OR HIS REPRESENTATIVE, PRIOR TO INSTALLATION.
- ANY DEVIATION FROM THESE SIGNING AND STRIPING PLANS SHALL BE APPROVED BY THE CITY TRAFFIC ENGINEER OR HIS REPRESENTATIVE PRIOR TO ANY CHANGE IN THE FIELD.
- ALL STRIPING SHALL BE REFLECTIVE PER CALTRANS SPECIFICATIONS. STRIPING SHALL BE REPAINTED TWO WEEKS AFTER INITIAL PAINTING.
- ALL SIGNAGE SHALL BE REFLECTIVE PER ASTM DESIGNATION D4956 AND SHALL USE TYPE IV PRISMATIC REFLECTIVE SHEETING (HIGH INTENSITY PRISMATIC OR EQUAL) UNLESS OTHERWISE SPECIFIED. R1-1 "STOP", R1-2 "YIELD", R2-1 "SPEED LIMIT" AND STREET NAME SIGNS SHALL USE TYPE IV PRISMATIC CUBE-CORNER REFLECTIVE SHEETING (DIAMOND GRADE WP OR EQUAL).
- LETTERING ON GROUND-MOUNTED STREET NAME SIGNS ON LOCAL RESIDENTIAL STREETS (SPEED LIMIT OF 25 MPH OR LESS) SHALL BE 4" HIGH CAPITAL LETTERS. LETTERING ON GROUND-MOUNTED STREET NAME SIGNS ON PRIME ARTERIALS, MAJOR ARTERIALS AND SECONDARY ARTERIALS (SPEED LIMIT GREATER THAN 40 MPH) SHALL BE 8" HIGH CAPITAL LETTERS. LETTERING ON ALL OTHER GROUND-MOUNTED STREET NAME SIGNS SHALL BE 6" HIGH CAPITAL LETTERS.
- EXACT LOCATION OF STRIPING AND STOP LIMIT LINES SHALL BE APPROVED BY THE CITY ENGINEER OR HIS REPRESENTATIVE PRIOR TO INSTALLATION.
- CONTRACTOR SHALL REMOVE ALL CONFLICTING PAINTED LINES, MARKINGS AND PAVEMENT LEGENDS BY GRINDING. DEBRIS SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR. CONTRACTOR SHALL SLURRY SEAL ALL AREAS AS SHOWN PLAN.
- ALL PAVEMENT LEGENDS SHALL BE THE LATEST VERSION OF THE CALTRANS STENCILS.
- LIMIT LINES AND CROSSWALKS SHALL BE FIELD LOCATED. CROSSWALKS SHALL HAVE 10' INSIDE DIMENSION UNLESS OTHERWISE SPECIFIED.
- ALL CROSSWALKS, LIMIT LINES, STOP BARS, PAVEMENT ARROWS AND PAVEMENT LEGENDS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.
- FIRE HYDRANT PAVEMENT MARKERS SHALL CONFORM TO THE CALIFORNIA MUTCD AND SAN DIEGO REGIONAL STANDARD DRAWING M-19.
- ALL SIGNS SHALL BE STANDARD SIZE AS SHOWN IN THE CALIFORNIA MUTCD UNLESS OTHERWISE SPECIFIED.
- SIGN POSTS SHALL BE SQUARE PERFORATED STEEL TUBING WITH BREAKAWAY BASE PER SAN DIEGO REGIONAL STANDARD DRAWING M-45.
- WHEN A SIGN IS ATTACHED TO A POLE, IT SHALL BE MOUNTED USING A STANDARD CITY OF CARLSBAD APPROVED MOUNTING BRACKET WITH STRAPS.
- EXISTING SIGNS REMOVED BY THE CONTRACTOR SHALL BE DELIVERED BY THE CONTRACTOR TO THE CITY OF CARLSBAD PUBLIC WORKS YARD AT 405 OAK AVE.
- ALL SIGNS SHOWN ON THESE PLANS SHALL BE NEW SIGNS PROVIDED AND INSTALLED BY THE CONTRACTOR EXCEPT THOSE SIGNS SPECIFICALLY SHOWN AS EXISTING TO BE RELOCATED OR TO REMAIN.

LEGEND

- EXISTING STRIPING TO BE REMOVED
- EXISTING STRIPING
- EXISTING ROADSIDE SIGN
- EXISTING PAVEMENT LEGEND
- PROPOSED STRIPING
- PROPOSED ROADSIDE SIGN
- INSTALL TYPE IV(R) PAVEMENT ARROW AS INDICATED (THERMOPLASTIC)
- INSTALL BIKE LANE PAVEMENT ARROW AS INDICATED (THERMOPLASTIC)
- INSTALL "LANE" PAVEMENT LEGEND AS INDICATED (THERMOPLASTIC)
- INSTALL "BIKE" PAVEMENT LEGEND AS INDICATED (THERMOPLASTIC)
- PROPOSED CONTINENTAL CROSSWALK PER CALTRANS STRIPING DETAILS. SEE STANDARD PLAN A24F
- REMOVE CONFLICTING EXISTING STRIPING BY GRINDING. ALL DEBRIS SHALL BE REMOVED BY THE END OF EACH WORK DAY.
- RELOCATE
- REMOVE AND SALVAGE
- PROPOSED PAVEMENT STRIPING PER CALTRANS STRIPING DETAILS. SEE STANDARD PLAN A20A THRU A20D
XX REPRESENTS THE NUMBER OF STRIPING DETAIL LISTED BELOW
- SLURRY SEAL AFTER GRINDING OF STRIPING



RONALD L. HOLLOWAY DATE _____
bha, inc.
 land planning, civil engineering, surveying
 5115 AVENIDA ENGINAS
 SUITE "L"
 CARLSBAD, CA. 92008-4387
 (760) 931-8700



NOTE:
 CONTRACTOR SHALL LOCATE ALL EXISTING UTILITY VALVE BOXES, MANHOLES, VAULTS, SEWER CLEANOUTS, STORM DRAIN GRATES, INLETS AND SURVEY MONUMENT BOXES AND PROTECT IN PLACE, UNLESS OTHERWISE NOTED IN WORK IN TO BE DONE.

"AS BUILT"

RCE _____	EXP. _____	DATE _____
REVIEWED BY: _____		
INSPECTOR _____	DATE _____	

DATE	INITIAL	REVISION DESCRIPTION	DATE	INITIAL	DATE	INITIAL

SHEET 6	CITY OF CARLSBAD	SHEETS 8
TRANSPORTATION DEPARTMENT		
STRIPING & SIGNING PLAN FOR:		
MELROSE DRIVE		
PALOMAR AIRPORT RD. AT MELROSE DRIVE IMPROVEMENTS		
APPROVED	HOSSEIN AJDEH	DATE _____
ENGINEERING MANAGER PE 75991	EXPIRES 6/30/22	DATE _____
DWN BY: CH	PROJECT NO. 6034	DRAWING NO. 472-4
CHKD BY: RH		
RVWD BY: RH		



TRAFFIC AND MOBILITY COMMISSION

Staff Report

Meeting Date: Aug. 18, 2022

To: Traffic and Mobility Commission

Staff Contact: Miriam Jim, Senior Engineer
Miriam.Jim@carlsbadca.gov, 442-339-5796

John Kim, City Traffic Engineer
John.Kim@carlsbadca.gov, 442-339-2757

Subject: Revise Prima Facie Speed Limits for Gateway Road between El Camino Real and El Fuerte Street and Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street

Recommended Action

Support staff's recommendation to City Council to revise prima facie speed limits on:

- 1) Gateway Road between El Camino Real and El Fuerte Street, from 40 miles per hour to 35 miles per hour
- 2) Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street, from 40 miles per hour to 35 miles per hour

Background

Setting speed limits requires a rational and defensible procedure to maintain the public's confidence in public agency traffic and legal systems. By following a uniform procedure, public agencies can establish uniform speed limits throughout the state that avoid influence from political pressure or emotional perceptions. The California State Legislature establishes authority for California speed limits through the California Vehicle Code, or CVC. The CVC describes speed limit authority in language used primarily for enforcement purposes. The California Department of Transportation, or Caltrans, publishes the California Manual for Setting Speed Limits that establishes the standard procedure for setting legally defensible speed limits. This manual provides guidance on how speed limits may be established in compliance with the CVC and how appropriate signage may be installed according to the California Manual on Uniform Traffic Control Devices, or CAMUTCD.

Speed limit determinations rely on the premise that a reasonable speed limit is one that conforms to the actual behavior of the majority of drivers. Speed limits are normally set near the 85th percentile speed. The 85th percentile speed, also known as the "critical speed", is the speed at or below which 85% of the traffic is moving, and statistically represents one standard deviation above the average speed. According to the California Manual for Setting Speed Limits,

Federal Highway Administration studies have demonstrated that the most effective attribute in establishing the speed limit is to determine the 85th percentile speed and set the posted speed limit close to that value. The empirical data in these studies demonstrates that setting the speed limit too high or too low can increase collisions.

In addition, setting the speed limit arbitrarily low often results in a disproportionate number of drivers who become violators of the speed limit, which does not facilitate the orderly movement of traffic and requires constant enforcement to maintain compliance. With this in mind, speed limits that are set near the 85th percentile speed are safer and produce less variance in vehicle speeds. Therefore, the 85th percentile speed is used to establish the upper limit of operating speeds that are considered reasonable and prudent.

CVC Section 22352 sets two prima facie speed limits in California, 15 miles per hour, or mph, and 25 mph. These limits cover six specific classes of location. No Engineering and Traffic Survey, or E&TS, or speed limit signs are required for these classes of location. These six classes of location are as follow:

- Uncontrolled railroad crossings (15 mph)
- Blind, uncontrolled intersections (15 mph)
- Alleyways (15 mph)
- Locations that meet business and residential district requirements (25 mph)
- School zones (25 mph)
- Areas immediately around senior centers (25 mph)

For locations that do not meet the six classes listed above, CVC Section 22358 allows local jurisdictions to establish speed limits on roadways based on the findings of an E&TS. An E&TS is a survey that consists of engineering measurements of the 85th percentile speed, a review of the collision history and a review of roadway conditions. The CAMUTCD provides detailed guidelines and procedures on conducting an E&TS in compliance with CVC Section 627.

A comprehensive review of the collision history of the roadway is an important element in the process to establish a speed limit on that roadway. A collision review reveals whether a high incidence of speed-related collisions exists on the roadway at specific locations. If no significant collision history exists, public agencies may conclude that most drivers operate their vehicles in a prudent manner, which supports using the 85th percentile speed to establish the speed limit.

CVC Section 22358.5 specifically prohibits lowering the speed limit for conditions that are readily apparent to drivers, such as roadway width, curvature, grade and surface conditions. Conditions that are not readily apparent to drivers may be used to lower the speed limit up to an additional 5 mph in certain cases, but those speed limits are sometimes defeated in court. For unusual conditions, or conditions not readily apparent to drivers, appropriate warning devices should be considered in lieu of lowering the speed limit.

Since roadway conditions are integral to the appropriate setting of speed limits, changes to roadway conditions can create opportunities to re-evaluate existing speed zones and recommend changes, if appropriate.

Discussion

Gateway Road between El Camino Real and El Fuerte Street

Gateway Road between El Camino Real and El Fuerte Street is classified as a Local/Neighborhood Street in the Mobility Element of the General Plan. This segment of Gateway Road features one travel lane and one bike lane in each direction. A contiguous sidewalk is provided on both sides of the roadway. The road has a curb-to-curb width of 52 feet. It follows a curvilinear alignment with roadway grades that vary from 1% to 6%. Adjacent land uses include light industrial, single- and multi-family residential and two local shopping centers.

Gateway Road is currently posted at 40 mph based on the E&TS dated Nov. 20, 2015, which showed the 85th percentile speed to be 43 mph.

In 2019, two sets of chicanes, approximately 300' long each, were installed on Gateway Road between Alicante Road and Innovation Way, as well as between Innovation Way and Village Green Drive, as a traffic calming measure to address speeding concerns from residents. A marked crosswalk with pedestrian-activated Rectangular Rapid Flashing Beacons, or RRFBs, was installed at the west leg of the intersection at Innovation Way. This crosswalk was installed in 2020 as part of the off-site improvements by the mixed-used development located at the northeast quadrant of the intersection of Gateway Road and Innovation Way. There is another raised and marked crosswalk with RRFBs located 630' east of El Camino Real. Based on these changes to the roadway, staff conducted the appropriate studies to re-evaluate the existing 40 mph speed zone on Gateway Road.

Staff reviewed the Police Department traffic collision summary report for a two-year period from April 1, 2020 through March 31, 2022 for this segment of Gateway Road. There were two reported collisions, one of which was speed-related. None of the collisions involved pedestrians or bicyclists.

A speed survey was conducted on Gateway Road on June 16, 2021. The 85th percentile speed of 36 mph was calculated based on a sample of 100 vehicles (50 vehicles in each direction of travel). The 85th percentile speeds from previous speed surveys and this recent survey are summarized in Table 1.

Based on the foregoing CVC-related authority, and the findings of the recently performed E&TS, a staff recommends posting a speed limit of 35 mph for Gateway Road between El Camino Real and El Fuerte Street.

Table 1 – Speed Survey Data

Survey Date	Existing 85 th Percentile Speed (mph)	Existing Speed Limit (mph)	Survey Date	New 85 th Percentile Speed (mph)	Proposed Posted Speed Limit (mph)
9/11/2014	43	40	6/16/2021	36	35

Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street

Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street is classified as a Local/Neighborhood Street in the Mobility Element of the General Plan. This segment of Cadencia Street has one travel lane in each direction. Sidewalk is provided on each side of the road except for a 600-foot portion on the west side of the road north of Piragua Street where sidewalk is not constructed. The road has a curb-to-curb width of 40 feet. It follows a curvilinear alignment with roadway grades that vary from 1.5% to 13.7%. Adjacent land uses include single- and multi-family residential and open space.

This segment of Cadencia Street is currently posted at 40 mph based on the E&TS dated Oct. 22, 2014, which showed the 85th percentile speed of 41 mph.

A recent traffic calming project, approved by the Traffic and Mobility Commission at its Aug. 3, 2020 meeting, installed three radar speed feedback signs on Cadencia Street (two in southbound and one in the northbound direction) and a marked crosswalk with RRFBs on the north side of the intersection at Venado Street. Construction of the project was completed in Winter 2021. Based on these changes to the roadway, staff conducted the appropriate studies to re-evaluate the existing 40 mph speed zone on Cadencia Street.

Staff reviewed the Police Department traffic collision summary report for a two-year period from May 1, 2020, through April 30, 2022 for this segment of Cadencia Street. There was one collision that was not speed-related and did not involve pedestrians or bicyclists.

A speed survey was conducted on this segment of Cadencia Street on May 19, 2022. The 85th percentile speed of 37 mph was calculated based on a sample of 100 vehicles (50 vehicles in each direction of travel). The 85th percentile speeds from previous speed surveys and this recent survey are summarized in Table 2.

Based on the foregoing CVC-related authority, and the findings of the recently performed E&TS, staff recommend posting a speed limit of 35 mph for Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street.

Table 2 – Speed Survey Data

Survey Date	Existing 85 th Percentile Speed (mph)	Existing Speed Limit (mph)	Survey Date	New 85 th Percentile Speed (mph)	Proposed Posted Speed Limit (mph)
3/9/2015	41	40	5/19/2022	37	35

Necessary City Council Action

The City Council must adopt an ordinance to establish the new prima facie speed limits on:

- 1) Gateway Road between El Camino Real and El Fuerte Street as 35 mph; and
- 2) Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street as 35 mph.

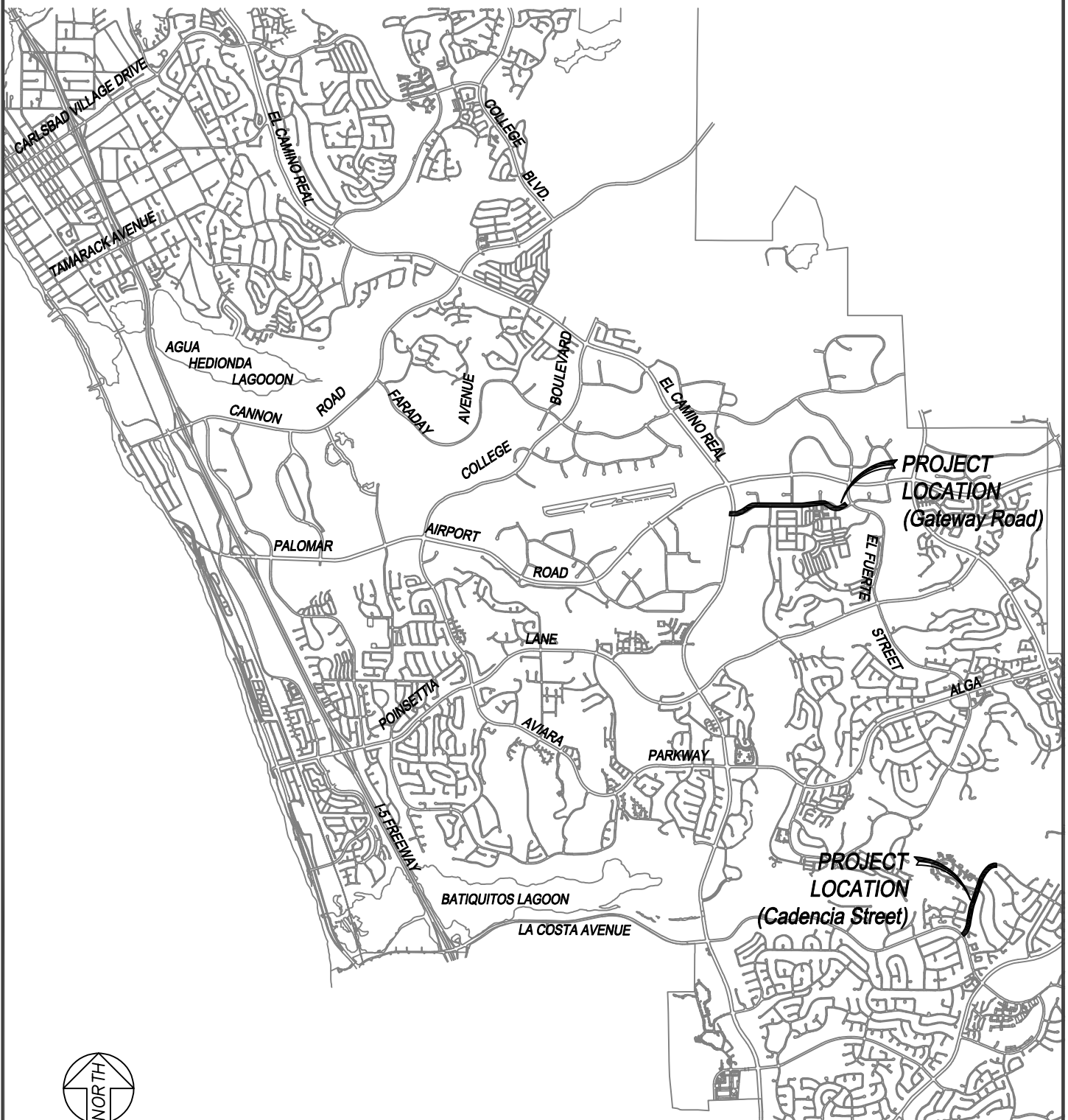
Next Steps

Upon receiving support from the Traffic and Mobility Commission, staff will introduce an ordinance for City Council adoption. Once the ordinance is adopted, appropriate regulatory speed limit signs will be posted in compliance with the CAMUTCD. Once the signs are installed, the Police Department can enforce the new speed limits on these two roadway segments.

Exhibits

1. Location Map
2. Engineering and Traffic Survey (Gateway Road)
3. Engineering and Traffic Survey (Cadencia Street)

LOCATION MAP




NOT TO SCALE

REVISE PRIMA FACIE SPEED LIMITS

*Gateway Road between El Camino Real and El Fuerte Street
Cadencia Street from Del Rey Avenue to 0.15 mile north of Piragua Street*

EXHIBIT

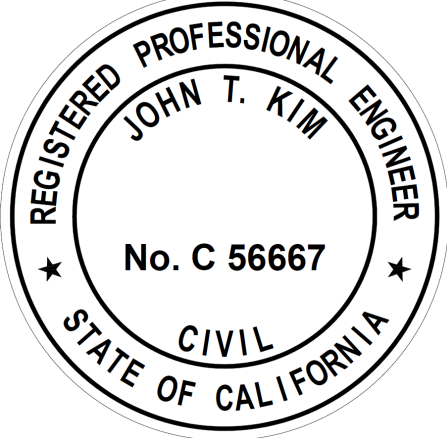
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CITY OF CARLSBAD ENGINEERING AND TRAFFIC SURVEY

Exhibit 2

STREET: Gateway Road

LIMITS: El Camino Real to El Fuerte Street

FACTORS		
A. Prevailing Speed Data: Date of Speed Survey Location of Speed Survey 85th Percentile 10 MPH Pace Percent in Pace	Direction: Eastbound/Westbound 6/16/21 300 Feet w/o Village Green Drive 36 MPH 27 to 37 MPH 85	
B. Accident History: (4/01/20 through 3/31/22) Speed-Related Accidents Pedestrian and/or Bicycle Accidents Total Accidents	1 None 2	
C. Traffic Factors: Average Daily Traffic Traffic Controls Pedestrian/Bicycle Traffic Bicycle Lanes On-Street Parking	5,373 – West of Innovation Way (2016) Traffic Signals at El Fuerte Street, Finnila Place and El Camino Real Light Bicycle Lane on Each Side Parking Prohibited on Each Side	
D. Roadway Factors: Street Classification Length of Segment Roadway Width Number of Lanes Vertical Alignment Horizontal Alignment Sidewalks Driveways Street Lighting	Circulation Element – Unclassified. Mobility Element – Local/Neighborhood Street. 0.83 Mi. 52 Feet (Curb-to-Curb) One Lane Each Direction Roadway Grades Vary from 1.00% to 5.89% Curvilinear Sidewalk on Each Side Five Driveways Street Lights on Each Side	
E. Special Conditions: Travel lanes are separated by either a painted median, two-way left-turn lane, or chicanes.		
F. Adjacent Land Uses: Light Industrial, Single-Family Residential, Multi-Family Residential, Two Local Shopping Centers including Several Restaurants.		
G. Remarks/Conditions Not Readily Apparent: Marked crosswalks with Rectangular Rapid Flashing Beacons (RRFBs) are located on the west side of the intersection at Innovation Way and also 630 feet east of El Camino Real. The latter crosswalk is a raised crosswalk. Chicanes are located both east and west of Innovation Way. Warning signs for the chicanes and RRFBs are posted for each direction of travel.		
H. Traffic Engineer's Recommendation (Explanation): This speed zone satisfies the conditions of Section 627 of the California Vehicle Code and has been prepared and evaluated in accordance with the California Manual on Uniform Traffic Control Devices, effective March 30, 2021, with respect to design and prevailing speeds, accident records, pedestrian and bicyclist safety, intersection and driveway spacing, and roadside and traffic conditions not readily apparent to the driver. A speed limit posting of 35 MPH is found to be appropriate and justified.		
I. Approvals: <input type="checkbox"/> Recertification of existing speed zone per Sections 22357, 22358 and 40802 of the California Vehicle Code. <input type="checkbox"/> Establishment of new speed zone.		
Approved _____ John T. Kim City Traffic Engineer CA RCE 56667		
_____ Date		


NOTE: Appropriate speed survey is attached hereto and made a part hereof.

CITY OF CARLSBAD ENGINEERING AND TRAFFIC SURVEY

Exhibit 3

STREET: Cadencia Street

LIMITS: Del Rey Avenue to 0.15 Mile north of Piragua Street

FACTORS		
<p>A. <u>Prevailing Speed Data:</u></p> <p>Date of Speed Survey Location of Speed Survey 85th Percentile 10 MPH Pace Percent in Pace</p>	<p><u>Direction:</u> Northbound/Southbound</p> <p>5/19/22 At Venado Street 37 MPH 28 to 38 MPH 74%</p>	
<p>B. <u>Accident History:</u> (5/01/20 through 4/30/22)</p> <p>Speed-Related Accidents Pedestrian and/or Bicyclist Accidents Total Accidents</p>	<p>None None 1</p>	
<p>C. <u>Traffic Factors:</u></p> <p>Average Daily Traffic Pedestrian/Bicycle Traffic Bicycle Lanes On-Street Parking</p>	<p>1,257 – n/o Piragua Street (2016); 2,360 – n/o Del Rey Avenue (2016) Light No Designated Bicycle Lanes Have Been Striped on the Roadway Parking Allowed on Each Side except n/o Piragua Street Where 383 Feet of Parking is Prohibited on Each Side</p>	
<p>D. <u>Roadway Factors:</u></p> <p>Street Classification Length of Segment Roadway Width Number of Lanes Vertical Alignment Horizontal Alignment Sidewalks Driveways Street Lighting</p>	<p>Circulation Element – Unclassified. Mobility Element – Local/Neighborhood Street 0.43 Mi. 40 Feet (Curb-to-Curb) 1 Lane Each Direction Roadway Grades Vary from 1.5% to 13.7% Curvilinear 600 Feet of Sidewalk Not Constructed on West Side North of Piragua Street Residential Driveways on East Side Limited Street Lighting</p>	
<p>E. <u>Special Conditions:</u> School bus stop located 400 feet north of Del Rey Avenue. A marked crosswalk with Rectangular Rapid Flashing Beacons (RRFBs) is located on the north side of the intersection at Venado Street. Warning signs for the crosswalk and RRFBs are posted for each direction of travel. Three speed feedback signs, one northbound and two southbound, inform motorists of the speed they are traveling. Speed cushions are located at the south end of the subject roadway segment just north of Del Rey Avenue where a 25 mph speed zone begins.</p>		
<p>F. <u>Adjacent Land Uses:</u> Single and Multi-Family Residential, Open Space</p>		
<p>G. <u>Remarks/Conditions Not Readily Apparent:</u> The school bus stop may result in children crossing the roadway.</p>		
<p>H. <u>Traffic Engineer's Recommendation (Explanation):</u> This speed zone satisfies the conditions of Section 627 of the California Vehicle Code and has been prepared and evaluated in accordance with the California Manual on Uniform Traffic Control Devices, effective March 30, 2021, with respect to design and prevailing speeds, accident records, pedestrian and bicyclist safety, intersection and driveway spacing, and roadside and traffic conditions not readily apparent to the driver. A speed limit posting of 35 MPH is found to be appropriate and justified.</p>		
<p>I. <u>Approvals:</u></p> <p><input type="checkbox"/> Recertification of existing speed zone per Sections 22357, 22358 and 40802 of the California Vehicle Code. <input type="checkbox"/> Establishment of new speed zone.</p>		
<p>Approved _____ Date _____</p> <p style="margin-left: 150px;">John T. Kim City Traffic Engineer CA RCE 56667</p>		

NOTE: Appropriate speed survey is attached hereto and made a part hereof.

Aug. 18, 2022

Item #3

8



TRAFFIC AND MOBILITY COMMISSION

Staff Report

Meeting Date: Aug. 18, 2022

To: Traffic and Mobility Commission

Staff Contact: Scott Meritt, Sergeant
scott.meritt@carlsbadca.gov, 442-339-2107

Allegra Frost, Deputy City Attorney
Allegra.frost@carlsbadca.gov, 760-434-2891

Subject: Amend the Carlsbad Municipal Code by Adding Section 10.40.302 to Restrict Overnight Parking on Surfside Lane and Island Way

Recommended Action

Support staff's recommendation that City Council introduce an ordinance amending the Carlsbad Municipal Code by adding Section 10.40.302 to restrict overnight parking from 11 p.m. to 5 a.m. on both sides of Surfside Lane, north of Island Way and both sides of Island Way, east of Carlsbad Boulevard.

Executive Summary

On March 3, 2022, the Carlsbad Police Department received a letter from Grand Pacific Resorts, concerning the area of Surfside Lane in front of the business being utilized for long term parking. (Exhibit 2.)

Police Department staff have verified complaints that the street in front of the resort is being used for long term, overnight parking by numerous passenger vehicles, recreational vehicles, buses, and camper vans. Area residents have reported similar concerns to the Police Department. Resort staff and area residents have also reported that subjects are using the resort's outdoor rinse shower to bathe.

The recommended action would amend the Carlsbad Municipal Code by adding Section 10.40.302 to establish no parking zones between the hours of 11 p.m. and 5 a.m. on both sides of Surfside Lane, north of Island Way and both sides of Island Way, east of Carlsbad Boulevard.

Discussion

The City of Carlsbad has received several complaints regarding recreational vehicles and passenger vehicles parking overnight and for extended periods on Surfside Lane and Island Way. The Police Department conducted an increased number of enforcement contacts in this area in response. Oversized vehicles parked on the affected streets have been cited pursuant to the Carlsbad Municipal Code 10.40.180. However, Police Department staff have noted many of the vehicles parked in the area do not qualify as oversized vehicles as defined in the Carlsbad Municipal Code. There are currently no parking time restrictions on the affected streets and the

Police Department has only been able to mark passenger vehicles with a notice of 72-hour violation (California Vehicle Code § 22651(k)). Upon rechecking the vehicles after 72 hours, they generally move to new parking spaces in the same area and cannot be towed or cited.

The use of city streets for long term and overnight parking of vehicles often increases the calls for police services to address noise complaints, littering, public urination and concerns of suspicious or unwanted activity in the adjacent areas. In addition, the overnight parking restriction would not decrease access to the coast because beach activities do not commonly occur during the 11 p.m. to 5 a.m. time period. Beach activity is generally limited to daytime and early dusk use and the beach areas in the vicinity of the proposed parking restriction lack lighting to allow safe access at night.

From June 1, 2021, to June 30, 2022, the Police Department responded to the following calls for service on Surfside Lane and Island Way, abutting Seapointe Resort.

CALL TYPE	
Parking Complaints from community or initiated by Police staff	29
Extra Patrols initiated by Police staff to address parking issues	11
Municipal Code complaints for oversize RV parking	10
Public Relations contacts regarding illegally parked vehicles	3
Noise Disturbance related to parked RV	1
Special Details focusing specifically on oversize vehicle enforcement	7
TOTAL	61

On March 24, 2022, Carlsbad Police Department staff distributed flyers to all homes in the residential neighborhood of Surfside Lane, south of Island Way and all residences on Franciscan Road. The Police Department solicited feedback from area residents to determine the impact of requesting no overnight parking in the affected area. Lieutenant Jason Jackowski spoke with approximately ten households. All of the households agreed that the area immediately surrounding Seapointe Resort warranted some form of restricted parking to better regulate problems in the area. Several residents expressed concerns that the parking issues would migrate into their residential neighborhood.

To address the concerns of the community, a meeting was held at Seapointe Resort on June 30, 2022. Flyers were distributed to all residences on Surfside Lane, south of Island Way and all residences on Franciscan Road. Approximately ten households were represented as well as resort ownership and management. All attendees agreed that regulating overnight parking on Surfside Lane, north of Island Way and Island Way, east of Carlsbad Boulevard was necessary. All attendees also agreed to move forward with a request to the Traffic and Mobility Commission and the Carlsbad City Council to request no overnight parking from 11 p.m. to 5 a.m. for the affected streets, directly adjacent to the resort.

Community concerns were addressed regarding the problem possibly moving into the residential neighborhood. Police Department staff advised they would monitor the area for the parking issues migrating into the residential neighborhood, conduct increased enforcement and

evaluate if the parking restrictions put in place adjacent to Seapointe Resort would need to be modified or expanded onto the residential streets.

On July 11, 2022, a recommendation was made to the Traffic and Mobility Division and the city Traffic Engineer, to install marked spaces and signage in the affected area to restrict parking of oversized vehicles. The request was made pursuant to Carlsbad Municipal Code 10.40.145, which authorizes the Traffic Engineer to determine locations for parking space markings.

Next Steps

Following the Traffic and Mobility Commissions review, staff will present the proposed ordinance to City Council and recommend that the City Council introduce the ordinance for adoption.

Exhibits

1. Ordinance
2. Letter to the Carlsbad Police Department from Seapointe Resort

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF CARLSBAD, CALIFORNIA, ADDING SECTION 10.40.302 OF THE CARLSBAD MUNICIPAL CODE TO ESTABLISH NO PARKING ZONES ALONG BOTH SIDES OF SURFSIDE LANE, NORTH OF ISLAND WAY AND BOTH SIDES OF ISLAND WAY, EAST OF CARLSBAD BOULEVARD BETWEEN THE HOURS OF 11 P.M. AND 5 A.M.

WHEREAS, the City of Carlsbad has received numerous reports of individuals parking recreational and passenger vehicles overnight along Surfside Lane and Island Way for extended periods; and

WHEREAS, the Carlsbad Police Department received a letter from Grand Pacific Resorts, concerning the area of Surfside Lane in front of the business being utilized for long term parking; and

WHEREAS, Police Department staff has verified complaints that the street in front of the resort is being used for long term, overnight parking by numerous passenger vehicles, recreational vehicles, buses, and camper vans; and

WHEREAS, the Carlsbad Police Department distributed flyers to all homes in the residential neighborhood of Surfside Lane, south of Island Way and all residences on Franciscan Road and solicited feedback to determine the impact of overnight parking restrictions in the affected areas; and

WHEREAS, residents agreed that the area immediately surrounding Seapointe Resort warrants some form of restricted parking to better regulate the problems in the area; and

WHEREAS, the use of city streets for long term and overnight parking of vehicles often increases the calls for police services to address noise complaints, littering, public urination and concerns of suspicious or unwanted activity in the adjacent areas; and

WHEREAS, the overnight parking restriction would not decrease access to the coast because beach activities do not commonly occur during the 11 p.m. to 5 a.m. time period, beach activity is generally limited to daytime and early dusk use, and the beach areas in the vicinity of the proposed parking restriction lack lighting to allow safe access at night; and

WHEREAS, the City Council of the City of Carlsbad has determined that it is in the interest of public safety that a parking restriction be implemented along both sides of Surfside Lane, north of Island Way and both sides of Island Way, east of Carlsbad Boulevard between the hours of 11 p.m. and 5 a.m.; and

WHEREAS, the City Planner has determined that the project is categorically exempt from the California Environmental Quality Act (CEQA) per state CEQA Guidelines section 15301 (c) and that no exception to the exemption as set forth in CEQA Guidelines section 15300.2 applies;

NOW, THEREFORE, the City Council of the City of Carlsbad, California, ordains as follows that:

1. The above recitations are true and correct.
2. That Title 10, Chapter 10.40, of the Carlsbad Municipal Code is amended by the

addition of Section 10.40.302 to read as follows:

10.40.301 Parking restricted on Surfside Lane and Island Way.

Parking of vehicles is prohibited between 11:00 p.m. and 5:00 a.m. on both sides of Surfside Lane, north of Island Way and both sides of Island Way, east of Carlsbad Boulevard.

EFFECTIVE DATE: This ordinance shall be effective thirty days after its adoption; and the City Clerk shall certify the adoption of this ordinance and cause the full text of the ordinance or a summary of the ordinance prepared by the City Attorney to be published at least once in a newspaper of general circulation in the City of Carlsbad within fifteen days after its adoption.

INTRODUCED AND FIRST READ at a Regular Meeting of the Carlsbad City Council on the ____ day of _____, 2022, and thereafter

PASSED, APPROVED AND ADOPTED at a Regular Meeting of the City Council of the City of Carlsbad on the __ day of _____, 2022, by the following vote, to wit:

AYES:

NAYS:

ABSENT:

APPROVED AS TO FORM AND LEGALITY:

CINDIE K. McMAHON, City Attorney

MATT HALL, Mayor

FAVIOLA MEDINA,
City Clerk Services Manager

(SEAL)

March 3, 2022

Lieutenant Jason Jackowski
Carlsbad Police Department
2560 Orion Way
Carlsbad, CA 92008

Dear Lieutenant Jackowski,

Thanks for reaching out and assisting us in managing the street parking on Surfside Lane.

Given the close proximity of Seapointe Resort to the beaches, Surfside Lane provides parking for many who want to enjoy our beaches. Unfortunately, what is occurring is that people are not just day parking, but using Surfside Lane for overnight and long-term parking. We have had some people set up home base and dispose of their waste inappropriately in the marsh area in front of the property.

We understand that Surfside Lane is open for public parking and are in full support of this. What we would appreciate is your assistance in managing the long term and overnight vehicles. We have a couple ideas on how to manage this:

1. Post no parking signs during the hours of 11:00 PM to 5:00 AM, which I believe is consistent with beach parking.
2. Stripe the roadway to accommodate a regular vehicle parking, but puts the larger RV's outside the parking stripe

Of course, we would appreciate any suggestions you might have as to better manage this situation.

I have enclosed a few pictures (we have lots!) of vehicles parking overnight that our staff has been taking daily since November 2021 at around 2:00 am each morning, as well as videos.

Thank you for your partnership in this matter. Over the years, we are grateful for the support and relationship with the Carlsbad Police Department. Please let me know if you need any additional information.

Sincerely,



Tim Stripe







TRAFFIC AND MOBILITY COMMISSION

Staff Report

Meeting Date: Aug. 18, 2022

To: Traffic and Mobility Commission

Staff Contact: Jason Jackowski, Police Lieutenant
Jason.jackowski@carlsbadca.gov, 442-339-2172

Subject: Police Report Regarding Traffic & Mobility-Related Matters During the Month of July 2022

Recommended Action

Receive a presentation from a representative of the City of Carlsbad’s Police Department that will provide an overview of traffic and mobility-related police matters during the month of July 2022.

Fiscal Analysis

This action has no fiscal impact.

Environmental Evaluation

In keeping with California Public Resources Code Section 21065, this action does not constitute a “project” within the meaning of the California Environmental Quality Act in that it has no potential to cause either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment. Therefore, it does not require environmental review.

Public Notification and Outreach

This item was noticed in keeping with the Ralph M. Brown Act and it was available for public viewing and review at least 72 hours before the scheduled meeting date.

Exhibits

None



TRAFFIC AND MOBILITY COMMISSION

Staff Report

Meeting Date: Aug. 18, 2022

To: Traffic and Mobility Commission

Staff Contact: Tom Frank, Transportation Director/City Engineer
Tom.frank@carlsbadca.gov, 442-339-2766

Katie Hentrich, Senior Program Manager
Katie.hentrich@carlsbadca.gov, 442-339-2623

Subject: Update on the South Carlsbad Boulevard Climate Adaptation Project

Recommended Action

Receive an update on the traffic impact analysis and roadway design options studies for the grant-funded South Carlsbad Boulevard Climate Adaptation Project.

Background

On July 5, 2022, staff presented preliminary conceptual designs for the South Carlsbad Boulevard Climate Adaptation Project to the Traffic and Mobility Commission (Exhibit 1). To make a recommendation on a preferred option prior to presenting the options to City Council, the Commission approved two motions as follows:

1. The first motion approved the following comments:
 - Recommended that sharrows be provided within the inscribed circle of the roundabouts
 - The traffic impact analysis results should be provided to the Traffic & Mobility Commission for review prior to going to City Council
 - The shared use paths around each roundabout shall be constructed of concrete and not decomposed granite
 - Recommended that City Council form an advisory committee for further review of the South Carlsbad Boulevard project
2. The second motion approved a recommendation that a special Traffic and Mobility Commission meeting be convened on Aug. 18, 2022, to review the results of a traffic impact and options analysis or traffic study.

The draft meeting minutes for the July 5, 2022, Traffic and Mobility Commission meeting are provided as Exhibit 2 and this staff report serves as an update on the second approved motion.

During the July 5, 2022 Traffic and Mobility Commission meeting, staff had stated a traffic study would be presented later in the process and prior to the completion of the 30% design being presented to City Council. However, upon reviewing the grant's work plan, a traffic study is not included in the project deliverables at this stage. This grant does not include funding to conduct environmental analysis or to build the new road, but rather funding to complete a 30% design centered around climate adaptation.

To address the need for a traffic study, staff have initiated a study that is targeted to be completed by early 2023. Following the completion of the traffic study, staff will present the study and option analyses to the Traffic and Mobility Commission for consideration prior to presenting the item to the City Council with a recommended preferred option for the preliminary conceptual design of the project study area.

The traffic study methodology will conform with the City's General Plan and follow similar practices used for other traffic studies or other projects that require a coastal development permit. The study will include multi-modal data and analysis.

Next Steps

- Staff will present a project update to City Council on Sept. 13, 2022.
- The City's consultant will conduct the traffic study and provide results to staff.
- Once the traffic study is completed, the analysis and results will be incorporated into other grant-funded deliverables as appropriate.
- Staff will return to the Traffic and Mobility Commission with this updated analysis and present the traffic study, the project options, and request the commissioners to provide comments on the project options and consider supporting staff's recommendation for a preferred project option to City Council.
- At a later date, the City Council will provide direction on next steps for the 1-mile section of roadway, Las Encinas Creek habitat restoration and phased adaptation, and the full length of south Carlsbad Boulevard.
- While the grant's funding deadline is Feb. 2023, staff are exploring a time extension with the State Coastal Conservancy to support additional analysis and public outreach.

Exhibits

1. July 5, 2022 Traffic and Mobility Commission Staff Report
2. July 5, 2022 Traffic and Mobility Commission Summary Memo



TRAFFIC AND MOBILITY COMMISSION

Staff Report

Meeting Date: July 5, 2022

To: Traffic and Mobility Commission

Staff Contact: Tom Frank, Transportation Director
Tom.Frank@carlsbadca.gov, 442-339-2766

Katie Hentrich, Senior Program Manager
Katie.Hentrich@carlsbadca.gov, 442-339-2623

Subject: Preliminary Conceptual Design of the South Carlsbad Boulevard Climate Adaptation Project

Recommended Action

1. Receive a report from staff regarding the Preliminary Conceptual Design of the South Carlsbad Climate Adaptation Project and the results of the public outreach effort; and, Receive public comments
2. Discuss and provide comments to staff on the three design options identified in the presentation
3. Consider supporting staff's recommendation to City Council to approve Option 2 as the preferred option for the preliminary conceptual design of the South Carlsbad Boulevard Climate Adaptation Project

Background

On May 5, 2020, the City Council adopted Resolution No. 2020-077, accepting a \$498,075 grant from the State Coastal Conservancy, or SCC, and entering into a research agreement with the University of California, San Diego, or UCSD, for the South Carlsbad Climate Adaptation Project. On June 16, 2020, the City Council adopted Resolution No. 2020-109, authorizing a Professional Services Agreement, or PSA, with GHD, Inc., for consulting services related to the Project. On Sept. 14, 2021, the City Council adopted Resolution No. 2021-209, authorizing agreement amendments to receive an additional \$35,100 from SCC to add analysis of restoration options for Las Encinas Creek into the Project scope. The SCC funding expires in February 2023.

The project's intent focuses on the eastward relocation of the southbound lanes of South Carlsbad Boulevard from approximately Manzano Drive to 400 feet south of Island Way to maximize the roadway's resiliency to coastal flooding and cliff erosion (Figure 1). Near Palomar Airport Road and Las Encinas Creek, southbound Carlsbad Boulevard is currently exposed to bluff erosion hazards. Periodic flooding of the southbound lanes is already occurring, resulting in beach cobble on the roadway surface and lane closures.



Figure 1. South Carlsbad Boulevard Project Area

As indicated in the city’s Sea Level Rise Vulnerability Assessment (December 2017), by year 2050 there is a high risk of damage to Carlsbad Boulevard due to sea level rise. The sensitivity of the potential sea level rise impacts to Carlsbad Boulevard is high because of the significant disruption caused by bluff erosion and flooding. The Assessment also found that *“Adaptation to sea level rise, and other results of climate change involves taking appropriate actions to prevent or minimize the adverse effects of climate-induced impacts.”*

The project involves several phases, including development of preliminary conceptual designs, design options, and a 30% design of the preferred project option, culminating in a final report in early 2023. Each option will also include public access, recreation amenities (such as bike paths, walking paths, open space, and/or trails), as well as ecosystem enhancements. The project builds upon roadway realignment and stakeholder outreach work conducted in 2012 and 2013 as part of the Carlsbad Boulevard Realignment Capital Improvement Program, or CIP, Project No. 6031. The core project team consists of staff from the city, SCC, UCSD (specifically, the Scripps Institution of Oceanography’s Center for Climate Change Impacts and Adaptation), and GHD, Inc.; staff from California State Parks, and the California Coastal Commission are a part of the larger project team.

Work Completed to Date

The project’s first task involved assessing physical and regulatory constraints, such as estimating and mapping sea level rise and coastal erosion impacts along with constraints such as public rights-of-way, private property lines, water and sewer utilities, and habitat areas; this first task also assessed existing roadway capacity to incorporate into the proposed project options. From fall 2020 through early 2021, the project team conducted listening sessions with

city staff and key stakeholders; the listening sessions covered topics such as process and communication, road use and realignment, coastal access, recreational spaces, and natural open space. A cross-cutting themes matrix was developed to better connect input to design considerations and components.

The community has already helped shape the policies in the city's General Plan, including the guiding principles for south Carlsbad Boulevard. On Sept. 14, 2021, the City Council provided direction to staff about public outreach for the project. Beginning in early 2022, the city sought input from the Carlsbad community about their priorities, needs, and values related to the future use of the entirety of south Carlsbad Boulevard (extending beyond the segment being analyzed in the project), within the parameters of the city's General Plan. The public input report, shown in Exhibit 1, summarizes the key themes and feedback received from this outreach, which will be incorporated into the final project deliverables.

Simultaneously, UCSD prepared a Cliff Erosion Assessment Report, shown in Exhibit 2, to help inform the analysis and other deliverables for the Project. Due to the area's importance to the overall project, GHD, Inc. prepared a restoration analysis of Las Encinas Creek. This restoration analysis explored two options. The first involves leaving the existing bridge, roadway, and revetment¹ intact until they experience significant deterioration from ocean impacts. Another option, which is preferred by the California Coastal Commission and SCC staff, is to remove the existing bridge, roadway, and revetment and allow the creek mouth and the surrounding coastline to transition into a natural system. Both options would involve wetland and upland restoration along the creek. A draft of the restoration analysis has been shared with the Project team and a final version is expected by Spring of 2023

Project Options

This section summarizes the existing conditions of the project area, describes the proposed coastal roadway and active transportation options, summarizes the results of the public outreach effort, and analyzes the roadway and active transportation options. What this report will not address is recreation amenities, coastal access designs, and parking area designs. Once the city has approved the preferred option for the preliminary roadway conceptual design, the preferred option will be developed to a 30% level of conceptual engineering design. Opportunities for public access and recreational amenities within the Project area will be outlined within a future memorandum

Existing Conditions

In its current configuration, Carlsbad Boulevard within the project area operates as a four-vehicle lane section, with two vehicle lanes in each direction and is split by a large, landscaped median. Both northbound and southbound directions have buffered bike lanes. Sidewalks are provided sparsely throughout the project area near Island Way and Solamar Drive. Carlsbad

¹ Revetments are sloping structures placed on riverbanks or bluffs to absorb the energy of incoming water.

Boulevard is classified as a Coastal Street², meaning its primary purpose is to move people along the city’s ocean waterfront and connect people to the beach, recreation, businesses, and residences in close proximity to the waterfront.

Counts were collected as a part of the Carlsbad Active Transportation Monitoring program in September 2021 along Carlsbad Boulevard between Avenida Encinas and Ponto Road. This included 24-hour vehicular counts, 13-hour pedestrian counts and 13-hour bicycle counts. The counts are summarized below:

- Weekday Vehicular Count: 10,833 vehicles
- Weekend Vehicular Count: 14,069 vehicles
- Weekday Bicycle Count: 797 bikes
- Weekend Bicycle Count: 2,509 bikes
- Weekday Pedestrian Count: 147 pedestrians
- Weekend Pedestrian Count: 408 pedestrians

As shown by these counts, this section of Carlsbad Boulevard not only moves vehicular traffic, but also high volumes of pedestrians and bikes. As noted within the Carlsbad Active Transportation Monitoring report, this section saw the largest weekend bike count and the second largest weekday bike count out of all 26 study segments throughout Carlsbad.

Description of Project Options

The project includes the following four options:

- Current Conditions (No action)
- Option 1: 4-lanes with traffic signals
- Option 2: 2-lanes with roundabouts
- Option 3: 2-lanes with roundabouts and an enhanced pedestrian crossing at Solamar Drive

The preliminary conceptual design options are provided in Exhibit 3. All three options show a majority of the Class-I and pedestrian pathway within the depicted coastal hazard zone and along the existing southbound lanes of Carlsbad Boulevard. The Class-I facility is shared pathway for exclusive use by pedestrians, bicyclists and other non-motorized modes of travel including strollers, and skateboards. The Class-I path will be intended for use by slower moving users with a maximum speed of 20 mph.

This is a cost-effective approach to achieve the planned separated bike and pedestrian pathways, although, this alignment is only viable as long as the area is available for use. There may be several iterations of Class-I and pedestrian pathway alignments as the coastline erodes and changes in the future. The final deliverables for this project will include information on how to “phase” the adaptation of all infrastructure in the coastal hazard zone, including this Class I

² Per the City of Carlsbad General Plan Mobility Element

pathway, likely based on the frequency and intensity of climate impacts experienced in the Project area.

The plans also depict a right of way line with adjacent Class-I and sidewalk or pedestrian pathway. This area will be planned and reserved for the most easterly alignment of a Class-I and pedestrian pathway on the west side of the roadway when needed due to the changing coastline. This easterly alignment of the Class-I and pedestrian pathway is mostly outside of the identified coastal hazard zone.

The no action option includes keeping Carlsbad Boulevard in its current configuration. This includes maintaining the northbound and southbound split Carlsbad Boulevard configuration with two vehicle lanes in each direction and very few pedestrian pathways.

The traffic signal with four vehicle lanes option (Option 1) would shift southbound Carlsbad Boulevard to the eastern portion of the existing northbound configuration. Option 1 proposes Carlsbad Boulevard as having two vehicle lanes in each direction, with signals controlling major intersections, on street Class-II buffered bike lanes and a detached Class-I path and pedestrian trail.

The roundabout with two vehicle lanes option (Option 2) would shift southbound Carlsbad Boulevard to the eastern portion of the existing northbound configuration. Carlsbad Boulevard would operate with a single vehicle lane in each direction. Option 2 would propose to have roundabouts controlling major intersections, on street Class-II buffered bike lanes and a detached Class-I path and pedestrian trail.

Option 3 would be the same configuration as Option 2; however, Option 3 would include a side street stop control at Solamar Drive with an enhanced pedestrian crossing and different configuration of crosswalks.

Public Outreach Summary

In April 2022, a public input report was created by the City of Carlsbad to summarize the input that was received regarding the entire three-mile stretch of Carlsbad Boulevard (inclusive of the one-mile portion studied by this project). The online survey and workshops posed open-ended questions designed to identify characteristics of Carlsbad's coastline that were most important to participants and how to make it better. The latest public input report and outreach efforts are provided at the webpage here - <https://www.carlsbadca.gov/departments/parks-recreation/coastal-corridor/south-carlsbad-coastline-project> and provided as Exhibit 1.

Options Analysis

To assist with the options analysis, the city's design team used the study's objectives, the city's General Plan's Coastal Streets Guiding Principles, and input received by the public input process

to determine the priorities to include the analysis. The analysis resulted in completing an options analysis matrix to help determine a preferred option.

The matrix weighting was determined by the city's design team and considered the priorities overall contribution to the project objective and the comments received during the public input process. Cost was also included as a priority and considers both construction and long-term maintenance costs.

The options analysis is provided as Exhibit 4. As you can see, the rankings show option 2 ranked number one with option 3 as a close second.

Considering the above, staff's approach is to recommend option 2 as the preferred option, with a long-term plan to convert the infrastructure to option 3 when the coastline no longer will facilitate a roundabout at Carlsbad Boulevard and Solamar Drive.

Funding Opportunities

The project team has been made aware of future funding opportunities to support project implementation, such as engineering or environmental analysis, described below:

- California Ocean Protection Council (OPC)
 - Funds available through Prop 68 could support pre-construction tasks, such as final engineering and environmental analysis, related to the Project.
 - While calls for 2022 applications are currently open with applications due in summer 2022, this funding source could be available in future years.
- Federal Emergency Management Agency (FEMA)
 - Building Resilient Infrastructure and Communities, or BRIC, could support construction or other "shovel ready" components of the Project.
 - Fiscal Year 2022 funding is expected to open in fall 2022, with applications due late 2022/early 2023 and funding awarded in summer 2023.
 - The Hazard Mitigation Grant Program could support construction or other "shovel ready" components of the Project.
 - Funding is made available on a similar schedule as BRIC. The Hazard Mitigation Grant Program is supported by presidential major disaster declarations; depending on the year, calls for funding may be made available more than once.

Not only would these funding opportunities could support implementation of the final design option selected for the project, but they would also support findings from the city's Sea Level Rise Vulnerability Assessment and support Resolution No. 2021-215, which declared a climate emergency and that *"any meaningful action that stands a chance of success at mitigating and adapting to the effects of climate change requires mobilization without delay."*

Next Steps

- Public input and feedback from the city's boards and commissions will be shared with the City Council later this summer to assist in selecting the preferred option.
- The following additional city commissions will have an opportunity to review the options and provide feedback: Beach Preservation Commission, Parks & Recreation Commission, Beach Preservation Commission and Planning Commission.
- A primary final deliverable for this project will be a conceptual design of how southbound Carlsbad Boulevard from Manzano Drive to Island Way could be moved to the east. The grant does not include funding to build the new road, just to complete a 30% design.
- Other final deliverables include the final Las Encinas Creek restoration analysis, a final design report, and a long-term master plan that discusses phased adaptation over time.
- Beyond completing the study, the City Council will determine next steps for the 1-mile section of roadway and the full length of south Carlsbad Boulevard.
- At this time, no additional work has been included in the city's FY 2022-23 budget for either project.

Exhibits

1. Public Input Report
2. South Carlsbad Boulevard Cliff Erosion Assessment Report
3. Preliminary Conceptual Design Plan Options
4. Options Analysis

Council Memorandum

July 7, 2022

To: Honorable Mayor Hall and Members of the City Council
From: Nathan Schmidt, Transportation Planning & Mobility Manager
Re: **Summary of Traffic & Mobility Commission Meeting of July 5, 2022**

APPROVAL OF MINUTES: By a 5/0/0/2 (Absent: Commissioner Diane Proulx and Commissioner William Fowler) vote, the Traffic and Mobility Commission approved the minutes of the May 2, and June 6, 2022, meeting as presented.

1. POLICE REPORT REGARDING TRAFFIC & MOBILITY RELATED MATTERS DURING THE MONTH OF June 2022– Received a report on Traffic & Mobility related matters during the month of June.
2. CARLSBAD BOULEVARD RESTRIPING BETWEEN MANZANO DRIVE AND ISLAND WAY, BIKE LANEIMPROVEMENTS AT FIVE LOCATIONS ON CARLSBAD BOULEVARD AND THREE SUSTAINABLE MOBILITY PLAN BIKE ENHANCEMENT PROJECTS – By a 5/0/0/2 (Absent: Commissioner Diane Proulx and Commissioner William Fowler) vote, the Traffic & Mobility Commission provided the following suggestions:
 - Staff shall provide notification to residents on Jefferson Street north of Las Flores informing them of the removal of on-street parking on the east side of Jefferson Street
 - Staff shall ensure that the traffic study for the improvements on Carlsbad Boulevard include analysis of pre-COVID traffic conditions
 - After implementation of the proposed improvements on Cannon Road at Paseo Del Norte, staff shall review the project for potential implementation of a bike box at the westbound approach of Cannon Road
 - After implementation of the proposed improvements on Carlsbad Boulevard staff shall evaluate options to reconfigure the parking lot north of Island Way to accommodate the maximum number of vehicles
3. PRELIMINARY CONCEPTUAL DESIGN OF THE SOUTH CARLSBAD BOULEVARD CLIMATE ADAPTATION PROJECT – By a 5/0/0/2 (Absent: Commissioner Diane Proulx and Commissioner William Fowler) vote, the Traffic & Mobility Commission provided the following suggestions:
 - Recommended that sharrows be provided within the inscribed circle of the roundabouts
 - The traffic impact analysis results should be provided to the Traffic & Mobility Commission for review prior to going to City Council
 - The shared use paths around each roundabout shall be constructed of concrete and not decomposed granite
 - Recommended that City Council form an advisory committee for further review of the South Carlsbad Boulevard project

Public Works Branch

Transportation Department

1895 Paradise Avenue | Carlsbad, CA 92008 | 442-339-2746 t

Item #6

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By a 5/0/0/2 (Absent: Commissioner Diane Proulx and Commissioner William Fowler) vote, the Traffic & Mobility Commission recommended that a special Traffic & Mobility Commission meeting shall be convened on Aug. 18, 2022, to review the results of the traffic impact analysis and alternatives analysis.

4. UPDATE ON TAMARACK AVENUE AND VALLEY STREET PEDESTRIAN SIGNAL PROJECT – By a 5/0/0/2 (Absent: Commissioner Diane Proulx and Commissioner William Fowler) vote, the Traffic & Mobility Commission made a recommendation to staff to request the City Council to expedite a traffic calming program at Tamarack Avenue from Skyline Drive to Adams Street and to address the resident’s concerns on the newly installed Bulb-Outs at the intersection of Tamarack Avenue and Valley Street.
5. TRAFFIC AND MOBILITY COMMISSION FISCAL YEAR 2022-23 WORKPLAN – By a 5/0/0/2 (Absent: Commissioner Diane Proulx and Commissioner William Fowler) vote, the Traffic & Mobility Commission approved the FY 2022-23 Traffic & Mobility Commission Work Plan and nominated Chair Brandon Perez to represent the commission at the City Council meeting on Aug. 16, 2022.

cc: Scott Chadwick, City Manager
Celia Brewer, City Attorney
Geoff Patnoe, Assistant City Manager
Paz Gomez, Deputy City Manager, Public Works
Gary Barberio, Deputy City Manager, Community Services
Laura Rocha, Deputy City Manager, Administrative Services
Mickey Williams, Police Chief
Robby Contreras, Assistant City Attorney
Jeff Murphy, Community Development Director
Jason Geldert, Engineering Manager
Tom Frank, Transportation Director/City Engineer
John Kim, City Traffic Engineer
Hossein Ajideh, Engineering Manager
Sheila Cobian, Legislative and Constituent Services Director
Faviola Medina, City Clerk Services Manager
Jason Jackowski, Lieutenant, Police Department
Nikki Matosian, Communication & Engagement
James Wood, Environmental Sustainability Director
Katie Hentrich, Senior Program Manager